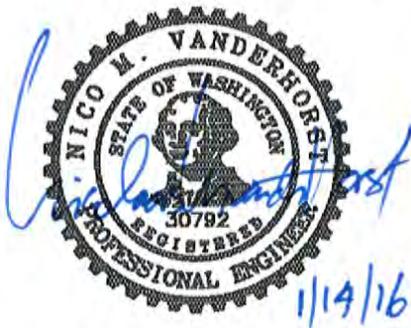




**Specifications, Proposal and Contract Documents**  
for  
**Redmond Central Connector**  
**Phase II**  
**Project No. 20021314**  
**Federal Aid No. CM1065(011)**

*January 2016*



Nico M. Vanderhorst, P.E.



Touta J. Phengsavath, P.E.

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## Appendix A — Standard Plans and Details

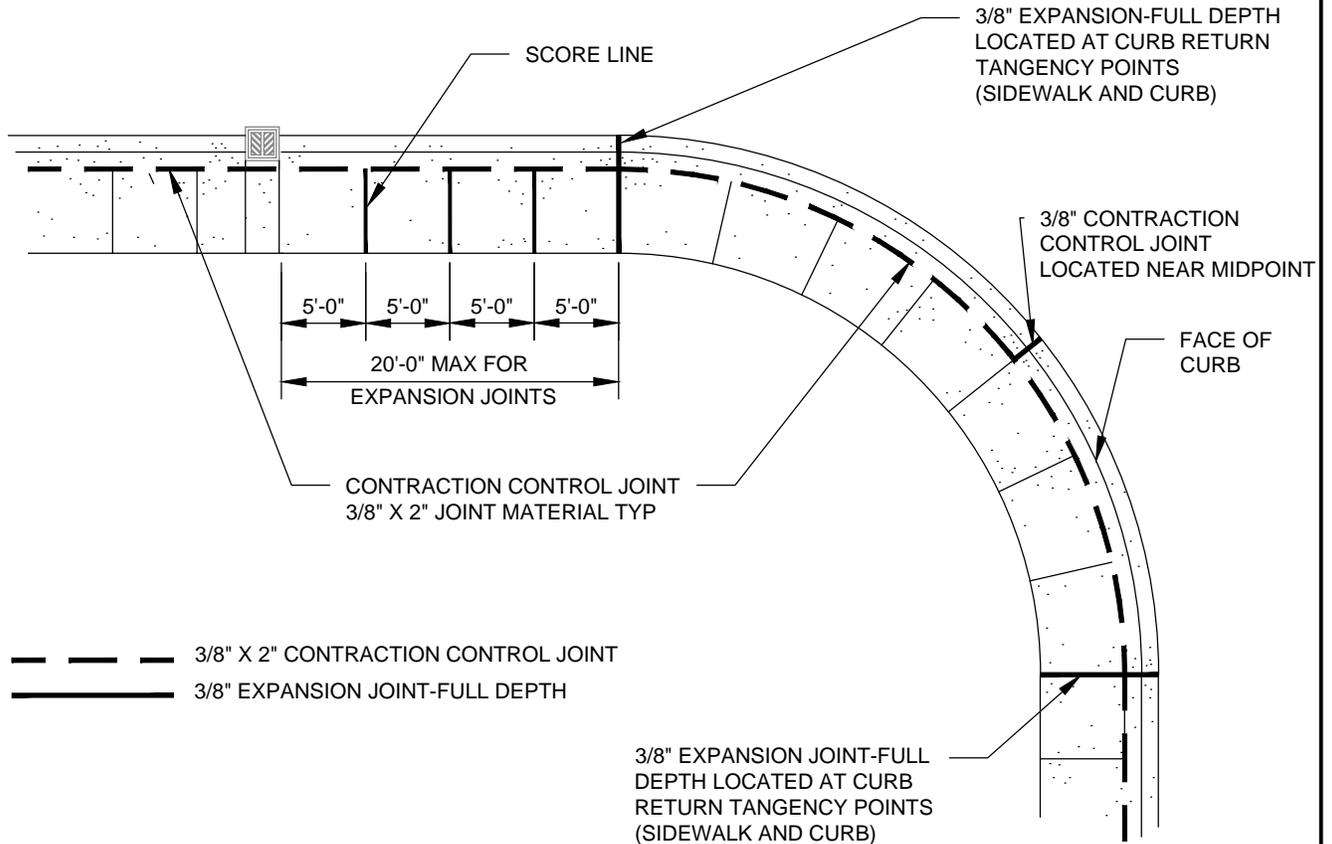
Note: These standard plans/details are included in the Contract Provisions for Contractor's convenience. Contractor shall keep a copy of the applicable edition of the WSDOT/APWA Standard Plans for Road, Bridge and Municipal Construction and the City of Redmond Standard Details on the jobsite. Any detail cited in the Contract Documents, or required for construction of the Work, shall apply regardless of whether or not it is included in this Appendix, unless superseded by special details on the Plans.

## City of Redmond Standard Details

|      |  |
|------|--|
| 303  | Curb and Sidewalk Joints                           |
| 304  | Typical Curb and Gutter Details                    |
| 304A | Extruded and Barrier Curb Sections                 |
| 307  | Parallel Curb Ramp                                 |
| 309  | Perpendicular Curb Ramp                            |
| 311  | Crosswalk Markings & Signs                         |
| 311A | Crosswalk & Stop Bar                               |
| 311B | Crosswalk & Stop Bar                               |
| 312  | Channelization Lane Line Types                     |
| 415  | Speed Hump (1 of 2) xxxx                           |
| 416  | Speed Hump (2 of 2)                                |
| 416A | Bump Legend  |
| 419  | Lane-Use Pavement Markings                         |
| 420A | Typical Luminaire Locations                        |
| 421  | Uniform Luminaire Wiring                           |
| 426A | Sign and Post Detail                               |
| 426B | Street Light Pole Mounting Sign                    |
| 426C | Sign and Post in Concrete/Asphalt                  |
| 426D | Sign and Post in Ground                            |
| 430B | Pedestrian Scale Pole zxxxx                        |
| 453  | Pedestrian Push Button Orientation                 |
| 454  | Approved Bicycle Racks                             |
| 454A | Loop Detector Numbering and Spacing                |
| 455  | Loop Detector Sawcut                               |
| 456  | Sawcut and Conduit Under Curb                      |
| 457  | Loop Detector Splice and Wiring Labeling           |
| 458  | Loop Terminal Strip                                |
| 459A | Cabinet Foundation                                 |
| 459C | Service Cabinet Foundation                         |
| 461  | Service Cabinet                                    |
| 464  | Junction Box on Slope or Soft Areas                |
| 465  | CCTV Camera Mounting                               |
| 465A | CCTV Video Cable Connection                        |
| 467A | Junction Box Details 1 of 2                        |
| 467B | Junction Box Details 2 of 2                        |
| 471  | Pedestrian Luminaire and Pole                      |
| 474  | Pedestrian Luminaire Location and Pavement Scoring |
| 479  | Raised Crosswalk                                   |
| 481  | Raised Crosswalk Signing                           |
| 502  | Filter Fabric Fence                                |
| 503  | Stabilized Construction Entrance                   |
| 504  | Temporary Swale                                    |
| 505  | Earth Dike   |

|      |   |
|------|---|
| 506  | Perimeter Dike/Swale                                    |
| 609A | Typical Orientation of Type 2 CB in City Street Section |
| 609B | Typical Catch Basin Placement                           |
| 615  | Stormwater Manhole/Catch Basin Adjustment Detail        |
| 622  | Stenciling Detail                                       |
| 631  | Replace Frame & Grate with Ring & Cover                 |
| 903  | Survey Monument Case and Cover                          |
| 911  | Parks Bollard   |
|      |   |
|      |   |
|      |   |
|      |   |

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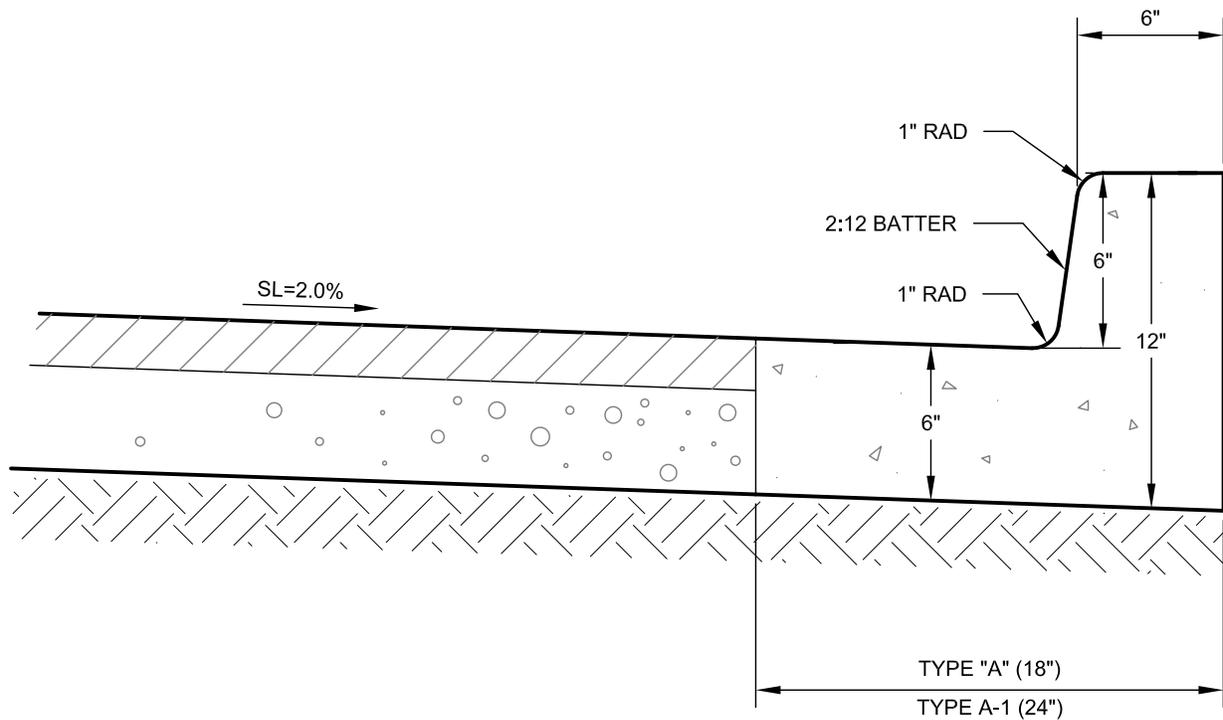


**PLAN VIEW**  
NTS

NOTES:

1. SCORE LINE SHALL BE 1/4 INCH DEEP V-GROVE.
2. JOINTS SHALL MATCH THE CURB.
3. SIDEWALK SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 8-14 OF THE STANDARD SPECIFICATIONS AND AS SHOWN ON THE STANDARD DETAILS
4. SIDEWALK SHALL BE 6 INCH MINIMUM THICKNESS WHERE ADJACENT TO ROLLED CURB SECTION, OTHERWISE MINIMUM THICKNESS SHALL BE 4 INCH.
5. THE CONCRETE MIX FOR SIDEWALKS SHALL BE AIR ENTRAINED CONCRETE CLASS 3000 IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 6-02.
6. PLACING AND FINISHING OF SIDEWALKS SHALL BE PER SECTION 8-14 OF THE STANDARD SPECIFICATION. THE SURFACES ARE TO BE STRUCK OFF, TROWELED, LIGHTLY BRUSHED IN TRANSVERSE DIRECTION, THEN JOINED AND EDGED. THE FINISH REQUIREMENTS INCLUDE:
  - A. JOINTS SHALL BE TOOLED WITH 1/4 INCH RADIUS EDGER.
  - B. SIDEWALK EDGES TOOLED WITH A 1/2 INCH EDGER.
  - C. WHEN REPLACING SECTIONS OF EXISTING SIDEWALK OR WHEN NEW SIDEWALK ADJOINS EXISTING, NEW CONCRETE SHALL BE FINISHED TO MATCH THE EXISTING CONCRETE OR AS DIRECTED BY THE CITY ENGINEER. COLORING AGENT SHALL BE USED IN NEW CONCRETE TO MATCH EXISTING.
  - D. WHEN CASTINGS ARE LOCATED IN THE SIDEWALK, JOINTS SHALL BE INSTALLED TO CONTROL CRACKING. REBAR SHALL BE INSTALLED AS DIRECTED BY THE CITY TO STRENGTHEN SIDEWALK TO PREVENT CRACKING AT CORNERS OF CASTINGS AND OTHER BLOCKOUTS.

|  |   |   |
|--|---|---|
| <br>APPROVED BY: PAULETTE NORMAN<br>DEVELOPMENT ENGINEERING MANAGER | <br>City of Redmond<br>WASHINGTON | STANDARD DETAILS<br><b>CURB AND SIDEWALK JOINTS</b><br>FILE NAME: SD303.DWG    DETAIL NUMBER: 303 |
| REVISION DATE: JULY 01, 2015   |   |   |



## TYPE A AND A-1 CURB AND GUTTER SECTION

NTS

### NOTES:

1. CONCRETE SHALL BE AIR ENTRAINED CLASS 3000 PER WSDOT STANDARD SPECIFICATIONS 6-02 CLASS 4000 REQUIRED ALONG DRIVEWAY ENTRANCE.
2. TYPE "A" AND ROLLED CURB AND GUTTER SECTIONS MAY BE PERMITTED FOR NEW PRIVATE STREETS.
3. TYPE "A" OR ROLLED CURB AND GUTTER MAY BE PERMITTED ON PUBLIC STREETS ONLY TO MATCH EXISTING.
4. CURB AND GUTTER SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 8-04 OF THE STANDARD SPECIFICATIONS.
5. AFTER THE CONCRETE HAS SET SUFFICIENTLY, THE ROADWAY FACE OF THE CURB FORMS SHALL BE REMOVED AND THE TOP AND FACE OF THE CURB SHALL RECEIVE A LIGHT BRUSH FINISH, AND THE TOP OF THE GUTTER SHALL RECEIVE A BROOM FINISH.

*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2014



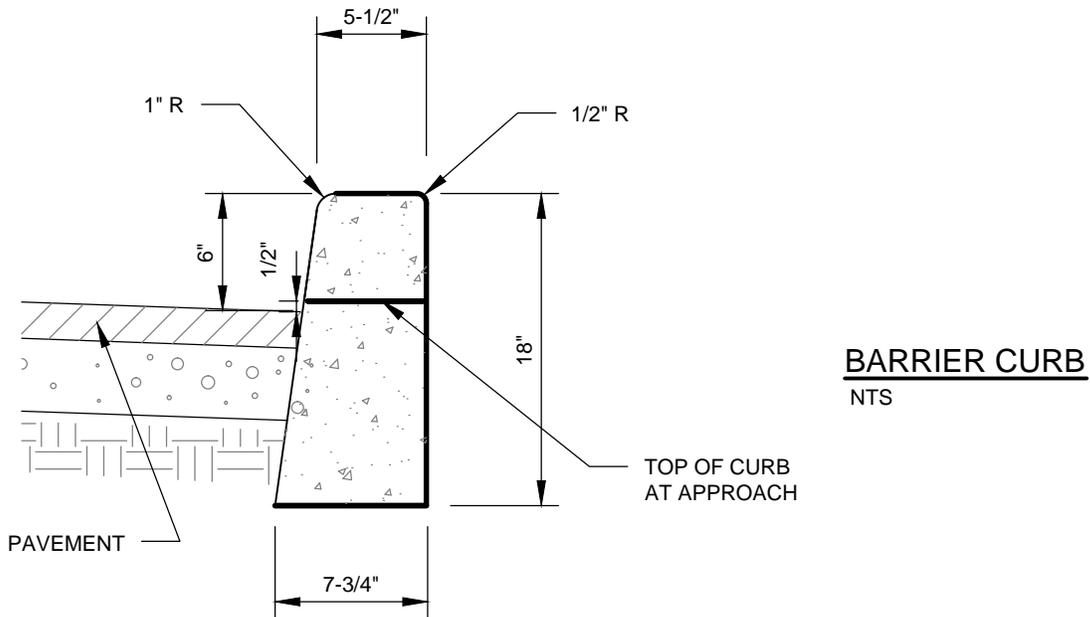
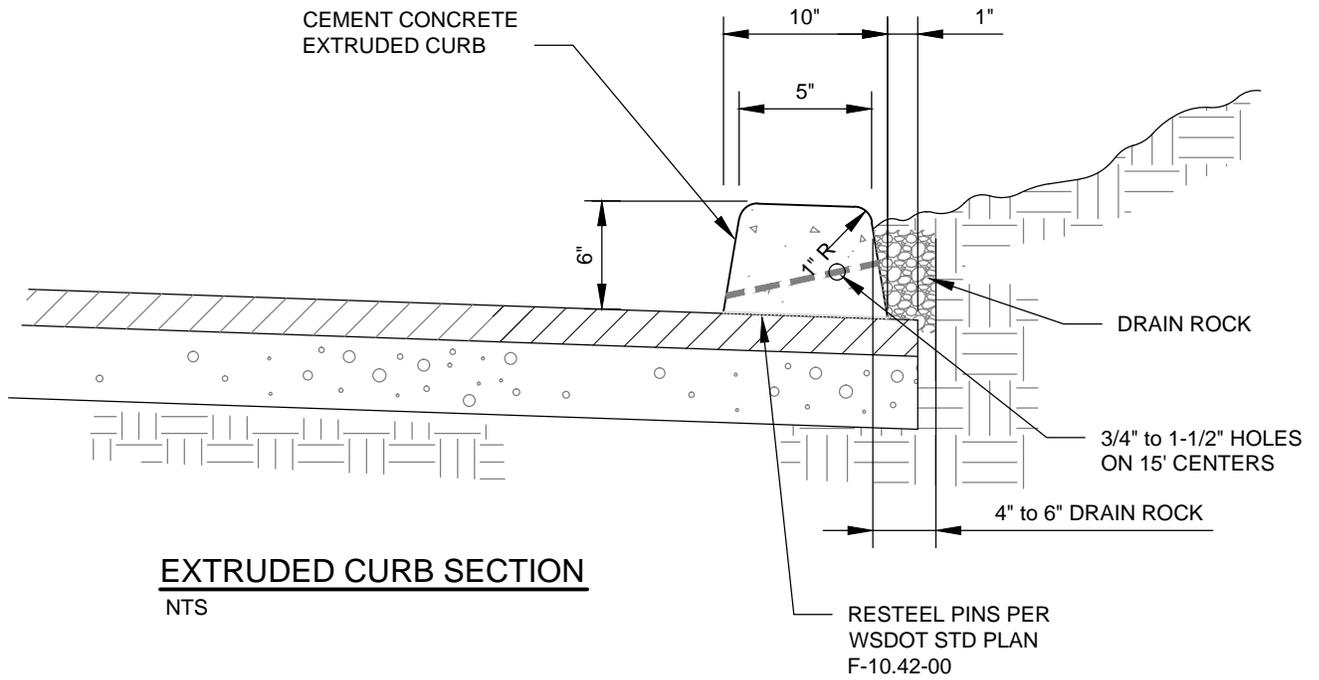
City of Redmond  
WASHINGTON

STANDARD DETAILS

### **TYPICAL CURB AND GUTTER DETAILS**

FILE NAME: SD304.DWG

DETAIL NUMBER: **304**



NOTES:

1. CURBS SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 8-04 OF THE STANDARD SPECIFICATIONS.
2. THE CONCRETE MIX FOR CURB SHALL BE AIR ENTRAINED CONCRETE CLASS 3000 IN ACCORDANCE WITH REQUIREMENTS OF SECTION 6-02 OF THE STANDARD SPECIFICATIONS. USE CLASS 4000 ALONG DRIVEWAYS.
3. AFTER THE CONCRETE HAS SET SUFFICIENTLY, THE ROADWAY FACE OF THE CURB FORMS SHALL BE REMOVED AND THE TOP AND FACE OF THE CURB SHALL RECEIVE A LIGHT BRUSH FINISH.

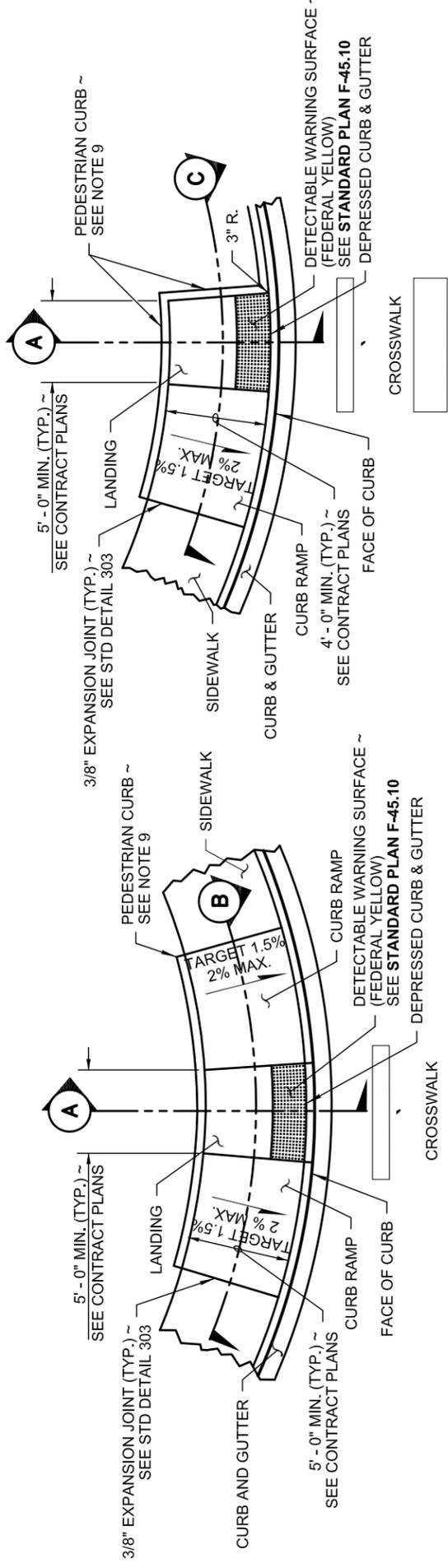
*Rob Crittenden*  
 APPROVED BY: ROB CRITTENDEN  
 TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER  
 REVISION DATE: JULY 01, 2015



STANDARD DETAILS  
**EXTRUDED AND BARRIER CURB SECTIONS**  
 FILE NAME: SD304A.DWG | DETAIL NUMBER: 304A

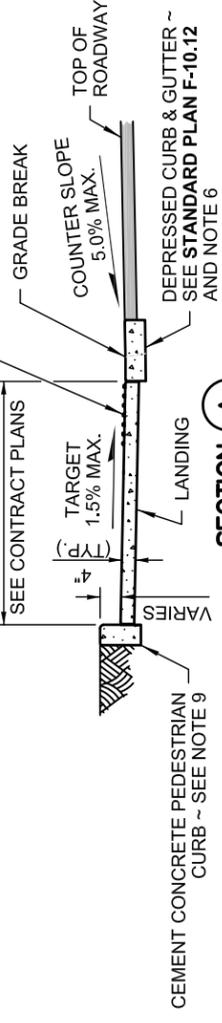
**NOTES**

1. Provide a separate Curb Ramp for each marked or unmarked crosswalk. Curb Ramp location shall be placed within the width of the associated crosswalk, or as shown in the Contract Plans.
2. Where "GRADE BREAK" is called out, the entire length of the grade break between the two adjacent surface planes shall be flush.
3. Do not place Gratings, Junction Boxes, Access Covers, or other appurtenances in front of the Curb Ramp or on any part of the Curb Ramp or Landing.
4. See Contract Plans for the curb design specified. See STD Detail 304 and 304A for Curb, Curb and Gutter, Depressed Curb and Gutter, and Pedestrian Curb details.
5. See **Standard Plan F-30.10** for Cement Concrete Sidewalk Details. See Contract Plans for width and placement of sidewalk.
6. (Intentionally left BLANK)
7. Ramp slope must be Target 7.5 percent (8.3 percent Max) or ramp length must exceed 15 feet.
8. Curb Ramp, Landing, and Flares shall receive broom finish. See **Standard Specifications 8-14**.
9. Pedestrian Curb may be omitted if the ground surface at the back of the Curb Ramp and/or Landing will be at the same elevation as the Curb Ramp or Landing and there will be no material to retain.

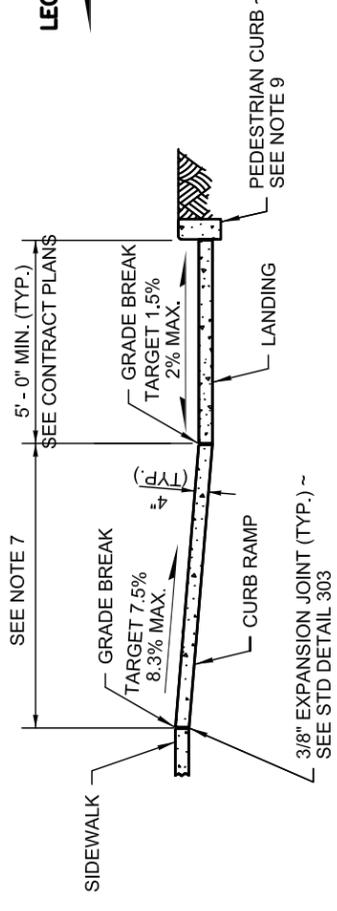


**PLAN VIEW TYPE PARALLELE B**

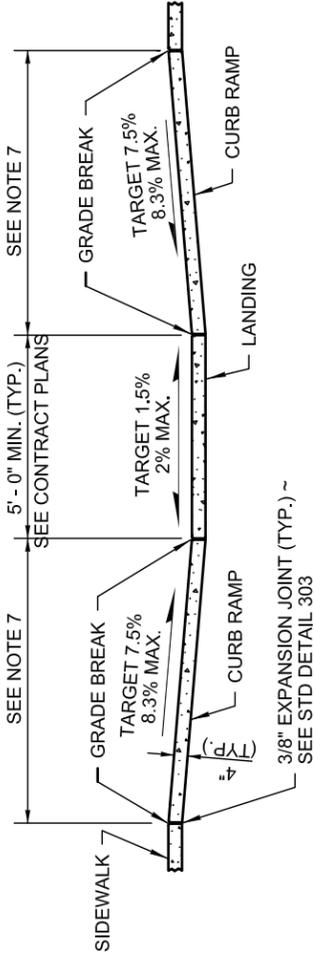
**PLAN VIEW TYPE PARALLELE A**



**SECTION A**

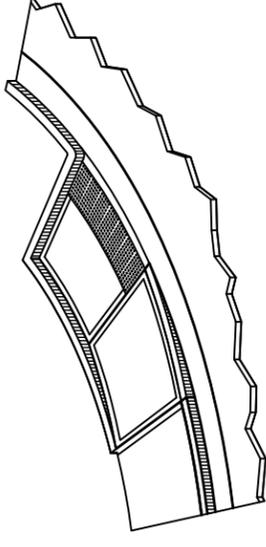
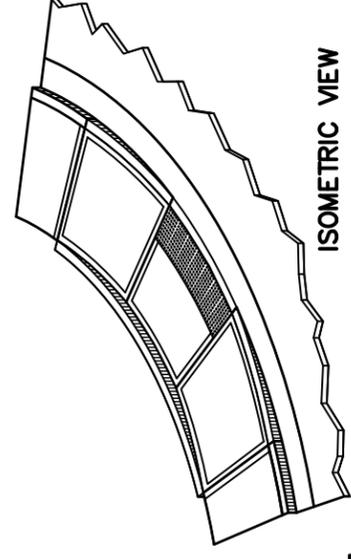


**LEGEND**



**SECTION B**

**SECTION C**



**MODIFIED STANDARD PLAN F-40.12-02**  
**WSDOT 6/20/13**  
 SHEET 1 OF 1 SHEET

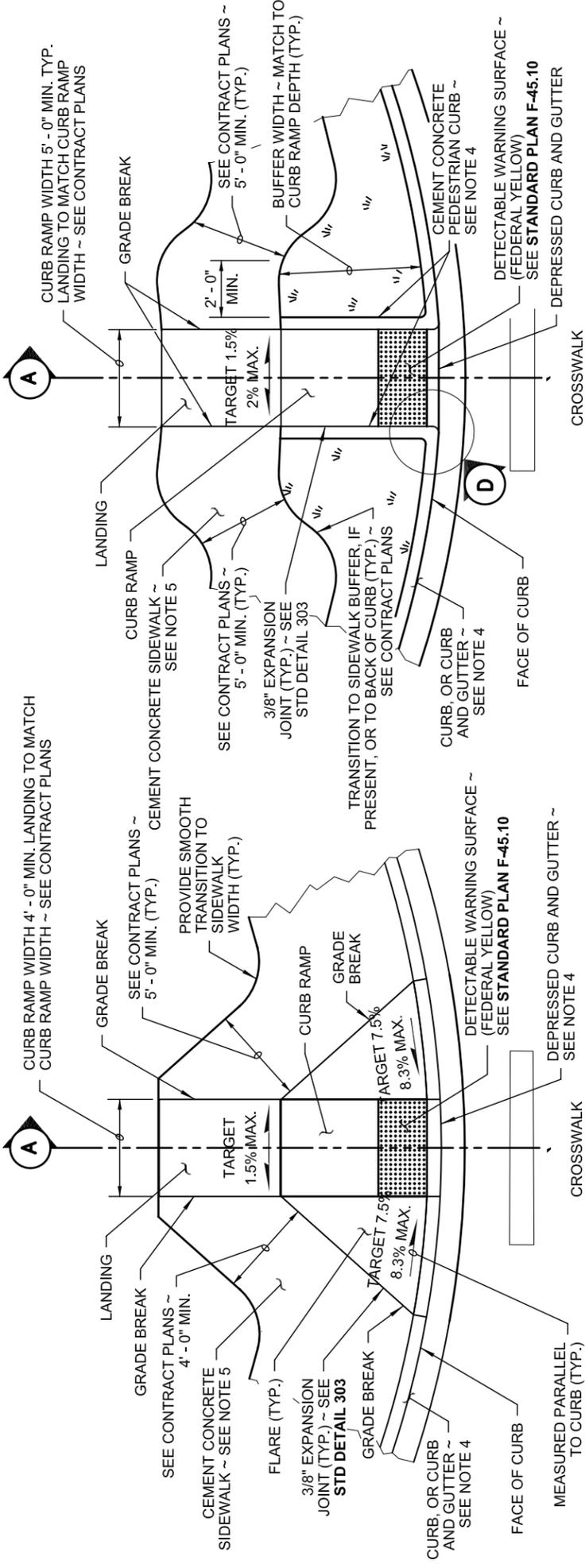


*Rob Crittenden*  
 APPROVED BY: ROB CRITTENDEN  
 TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER  
 REVISION DATE: JULY 01, 2015

STANDARD DETAILS

**PARALLELE CURB RAMP**

FILE NAME: SD307.DWG DETAIL NUMBER: **307**



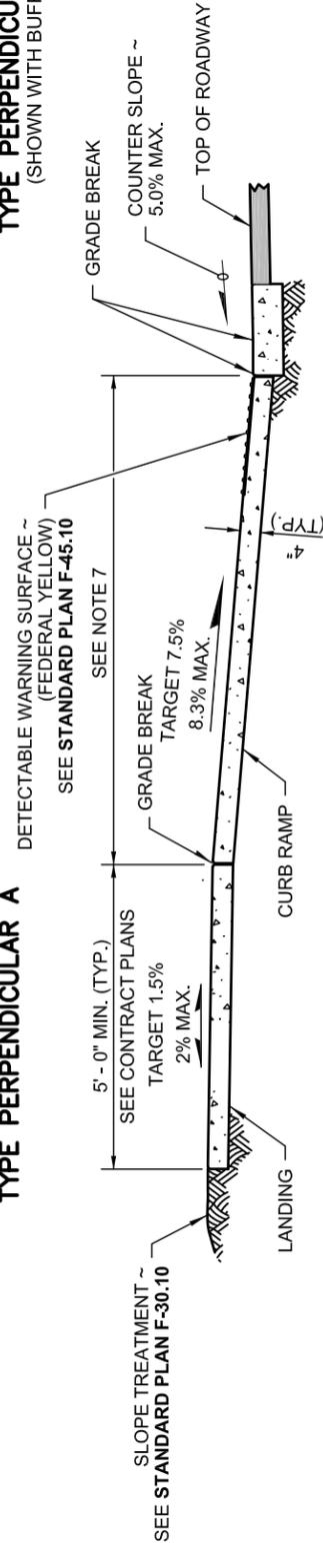
**NOTES**

1. Provide a separate Curb Ramp for each marked or unmarked crosswalk. Curb Ramp location shall be placed within the width of the associated crosswalk or as shown in the Contract Plans.
2. Where "GRADE BREAK" is called out, the entire length of the grade break between the two adjacent surface planes shall be flush.
3. Do not place Gratings, Junction Boxes, Access Covers, or other appurtenances in front of the Curb Ramp or on any part of the Curb Ramp or Landing.
4. See the Contract Plans for the curb design specified. See STD Detail 304 and 304A for Curb, Curb and Gutter, Depressed Curb and Gutter, and Pedestrian Curb details.
5. See STD Detail 303 for Cement Concrete Sidewalk details. See Contract Plans for width and placement of sidewalk.
6. (Intentionally left BLANK)
7. Ramp slope must be Target 7.5 percent (8.3 percent Max) or ramp length must exceed 15 feet.
8. Curb Ramp, Landing, and Flares shall receive broom finish. See **Standard Specifications 8-14.**

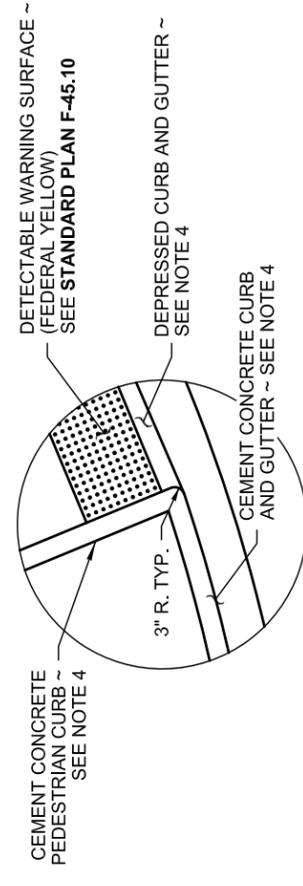


**PLAN VIEW  
TYPE PERPENDICULAR A**

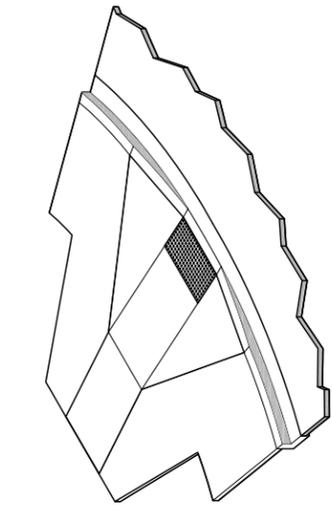
**PLAN VIEW  
TYPE PERPENDICULAR B  
(SHOWN WITH BUFFER)**



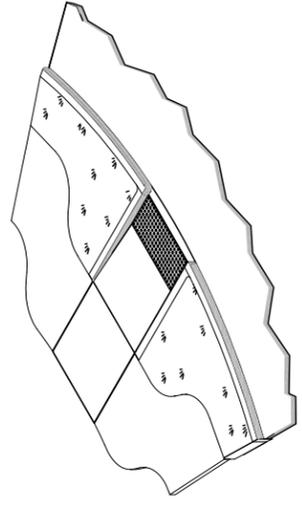
**SECTION A**



**CURB RADIUS DETAIL D**



**ISOMETRIC VIEW**



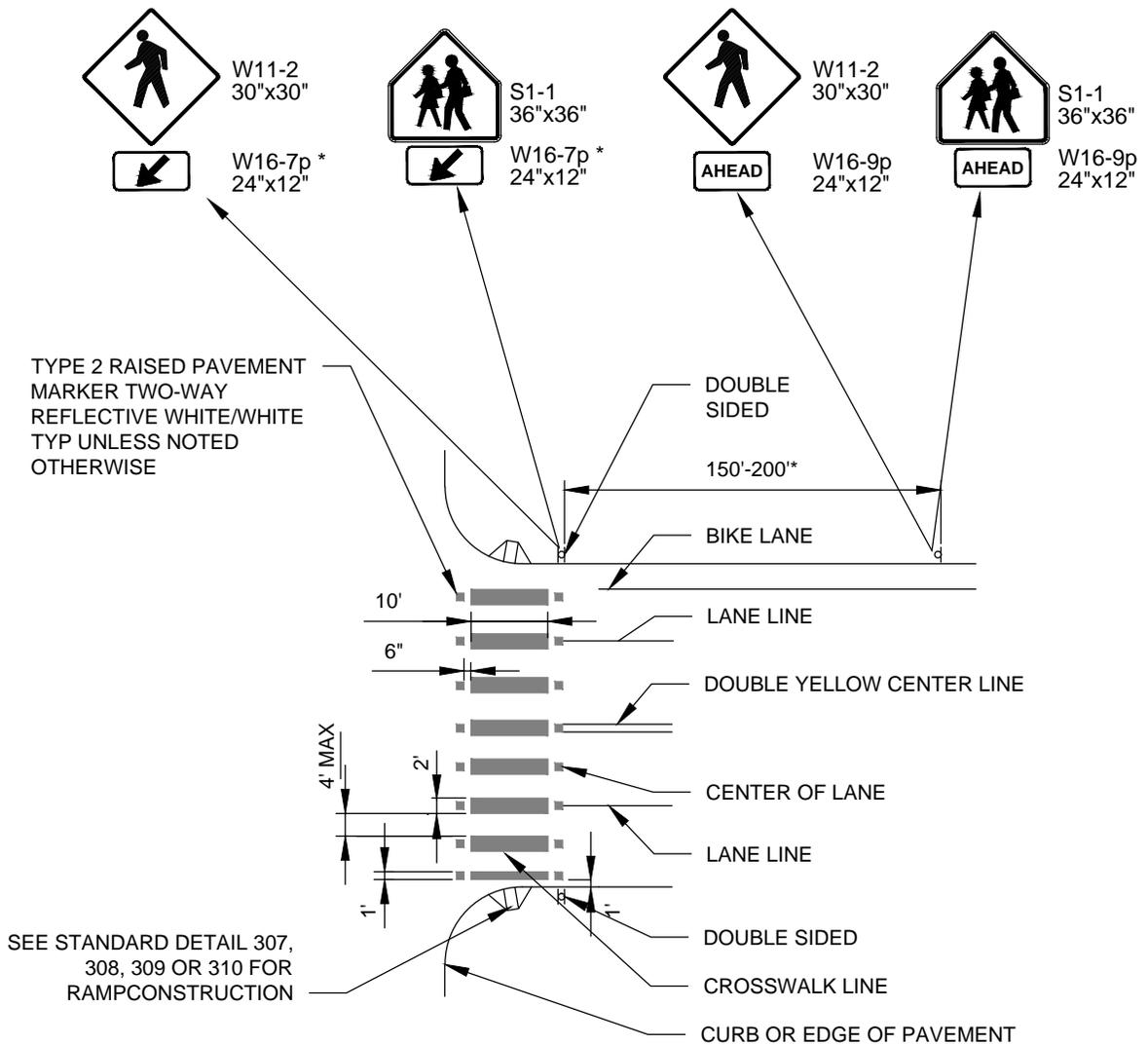
**ISOMETRIC VIEW**



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 TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER  
 REVISION DATE: JULY 01, 2015

**MODIFIED  
 STANDARD PLAN F-40.15-02**  
 WSDOT 6/20/13  
 SHEET 1 OF 1 SHEET

|                                    |                           |
|------------------------------------|---------------------------|
| STANDARD DETAILS                   |                           |
| <b>PERPENDICULAR<br/>CURB RAMP</b> |                           |
| FILE NAME: SD309.DWG               | DETAIL NUMBER: <b>309</b> |



\*TYPICAL

**UNPROTECTED CROSSWALK OR  
UNSIGNALIZED SCHOOL CROSSWALK**

NTS

NOTES:

1. USE WHITE THERMOPLASTIC UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
2. SIGNS HAVE FLUORESCENT YELLOW GREEN BACKGROUND WITH BLACK LEGEND AND BORDER. SIGN MATERIAL: 3M DIAMOND GRADE DG3.
3. UNPROTECTED NON-SCHOOL CROSSWALK: USE W11-2 WITH W16-7p AND W11-2 WITH W16-9P.
4. UNPROTECTED SCHOOL CROSSWALK USE S1-1 WITH W16-7p AND S1-1 WITH W16-9P.
5. PROTECTED CROSSWALK OMIT S1-1 WITH W16-7P AND S1-1 WITH W16-9P.

*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



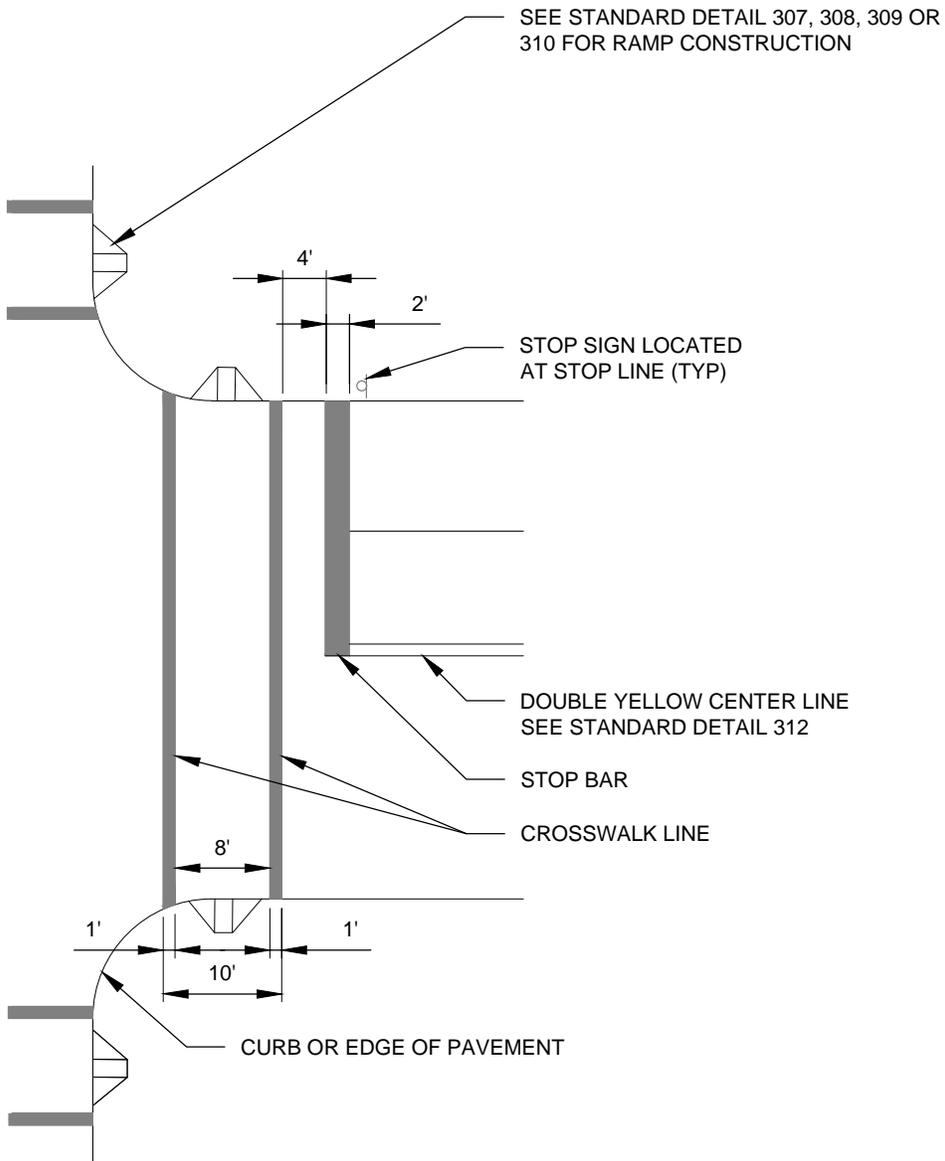
City of Redmond  
WASHINGTON

STANDARD DETAILS

CROSSWALK MARKINGS & SIGNS

FILE NAME: SD311.DWG

DETAIL NUMBER: 311



**PLAN VIEW**

NTS

NOTES:

1. USE WHITE THERMOPLASTIC UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
2. INSTALLATION OF OFFSET STOP BAR SHALL BE ENGINEER-APPROVED

*Rob Crittenden*

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TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



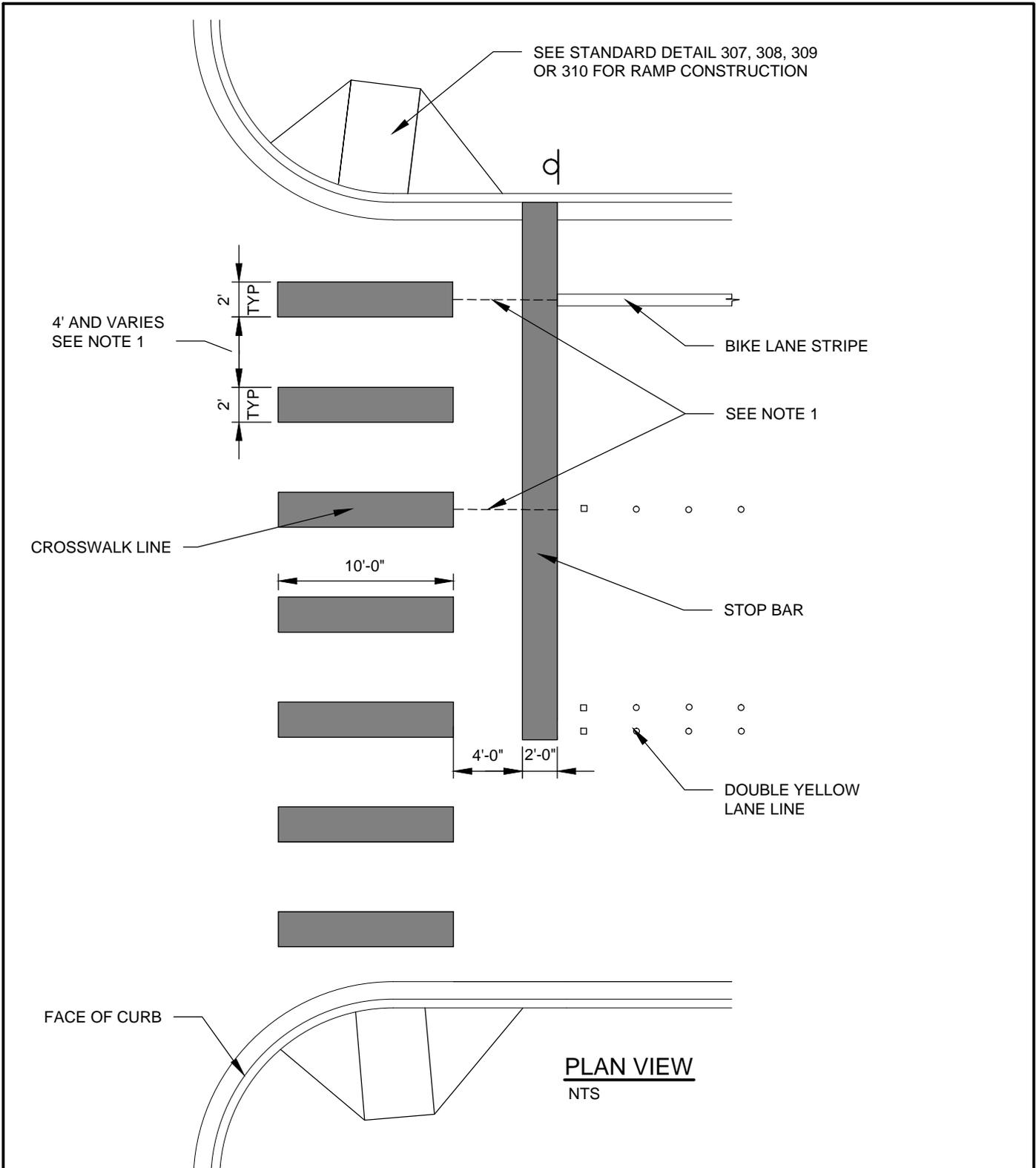
City of Redmond  
WASHINGTON

STANDARD DETAILS

CROSSWALK  
& STOP BAR

FILE NAME: SD311A.DWG

DETAIL NUMBER: 311A



SEE STANDARD DETAIL 307, 308, 309 OR 310 FOR RAMP CONSTRUCTION

4' AND VARIES SEE NOTE 1

CROSSWALK LINE

FACE OF CURB

**PLAN VIEW**  
NTS

NOTES:

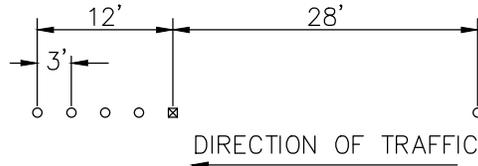
1. CENTER CROSSWALK BARS ON LANE LINES AND AT CENTER OF LANES AS SHOWN IN ORDER TO AVOID TIRE WEAR.
2. USE WHITE THERMOPLASTIC UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
3. INSTALLATION OF OFFSET STOP BAR SHALL BE ENGINEER-APPROVED.

*Rob Crittenden*  
 APPROVED BY: ROB CRITTENDEN  
 TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER  
 REVISION DATE: JULY 01, 2015



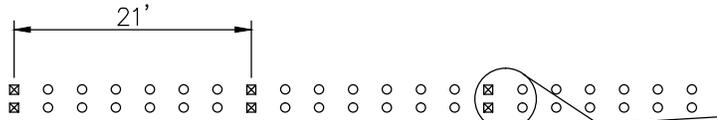
STANDARD DETAILS  
**CROSSWALK & STOP BAR**  
 FILE NAME: SD311B      DETAIL NUMBER: 311B

TYPE A  
LANE LINE

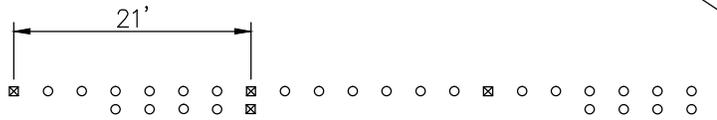


- WSDOT TYPE 1  
(4" YELLOW OR WHITE)
- ⊠ WSDOT TYPE 2  
(4" YY OR 2W  
REFLECTORIZED)

TYPE B  
DOUBLE YELLOW  
CENTER LINE



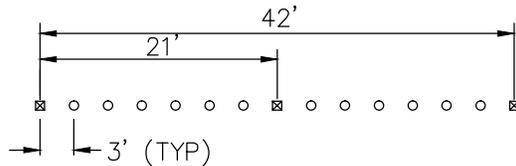
TYPE C  
TWO-WAY LEFT TURN/  
NO-PASS LINE



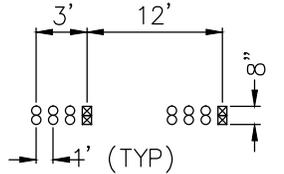
TYPE D  
WIDE LINE



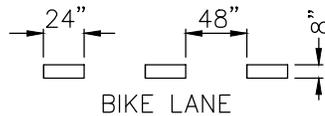
TYPE E  
NO-PASS LINE



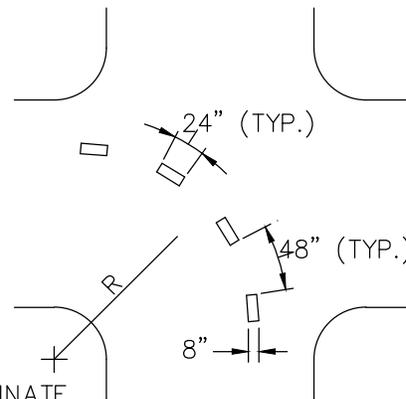
TYPE F  
DROP LANE LINE



TYPE G  
DOTTED WIDE LINE



COORDINATE  
OR STA+OFF



DOTTED EXTENSION  
LINE  
THERMOPLASTIC

NOTES:

1. FOR RAISED PAVEMENT MARKERS USE HOT MELT POLYMER BASED BITUMINOUS ADHESIVE.
2. BIKE LANE STRIPES ARE 8" PAINT, OR AS DIRECTED BY THE ENGINEER.
3. EDGE LANE STRIPES ARE 4" PAINT, OR AS DIRECTED BY THE ENGINEER.

*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



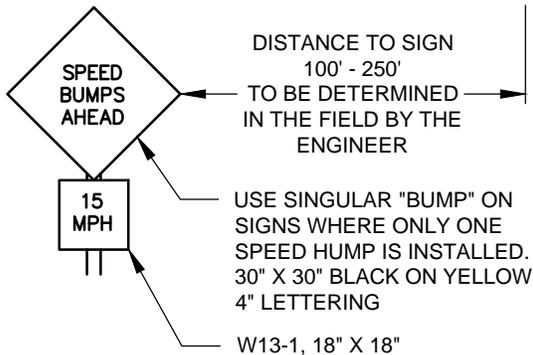
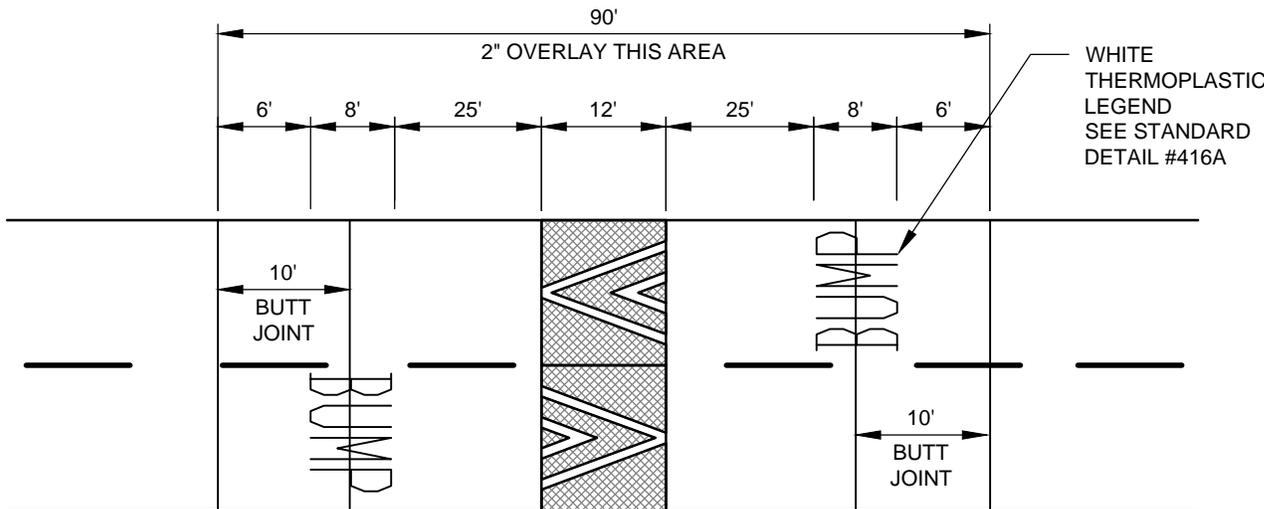
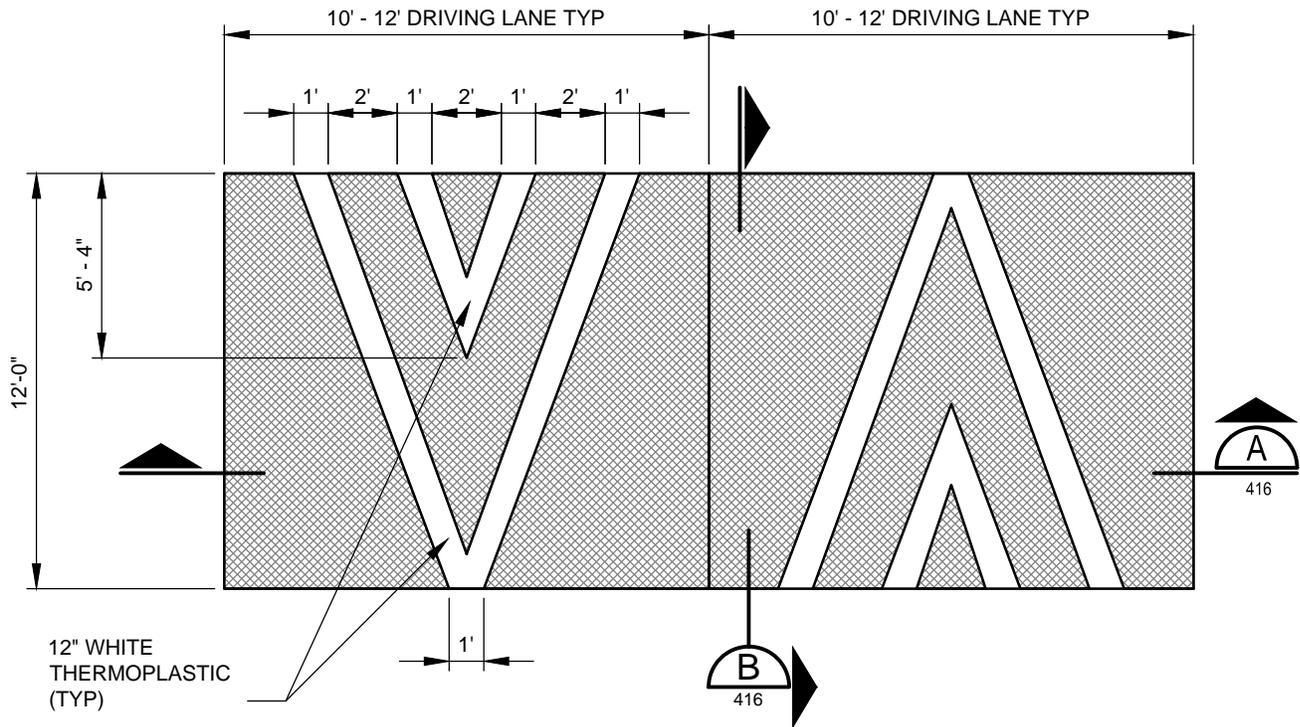
City of Redmond  
WASHINGTON

STANDARD DETAILS

CHANNELIZATION  
LANE LINE TYPES

FILE NAME: SD312.DWG

DETAIL NUMBER: 312



NOTES:

1. SIGNS & MARKINGS TYPICAL BOTH DIRECTIONS.
2. SEE "POST MOUNTED SIGN" STANDARD DRAWING 426C OR 426D.
3. A TEMPLATE MUST BE USED IN SHAPING SPEED HUMPS.
4. SPEED HUMPS MUST BE MADE IN TWO LIFTS.
5. 2" OVERLAY NOT REQUIRED FOR NEW PAVEMENT OR EXISTING PAVEMENT DETERMINED TO BE IN GOOD CONDITION BY THE ENGINEER.

*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015

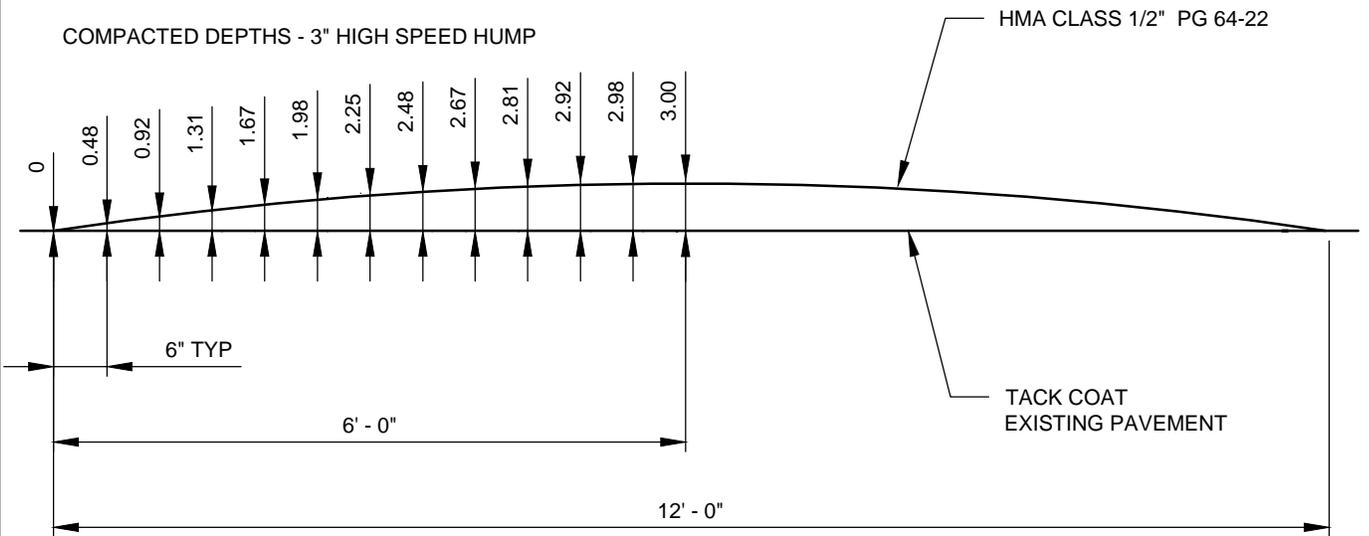
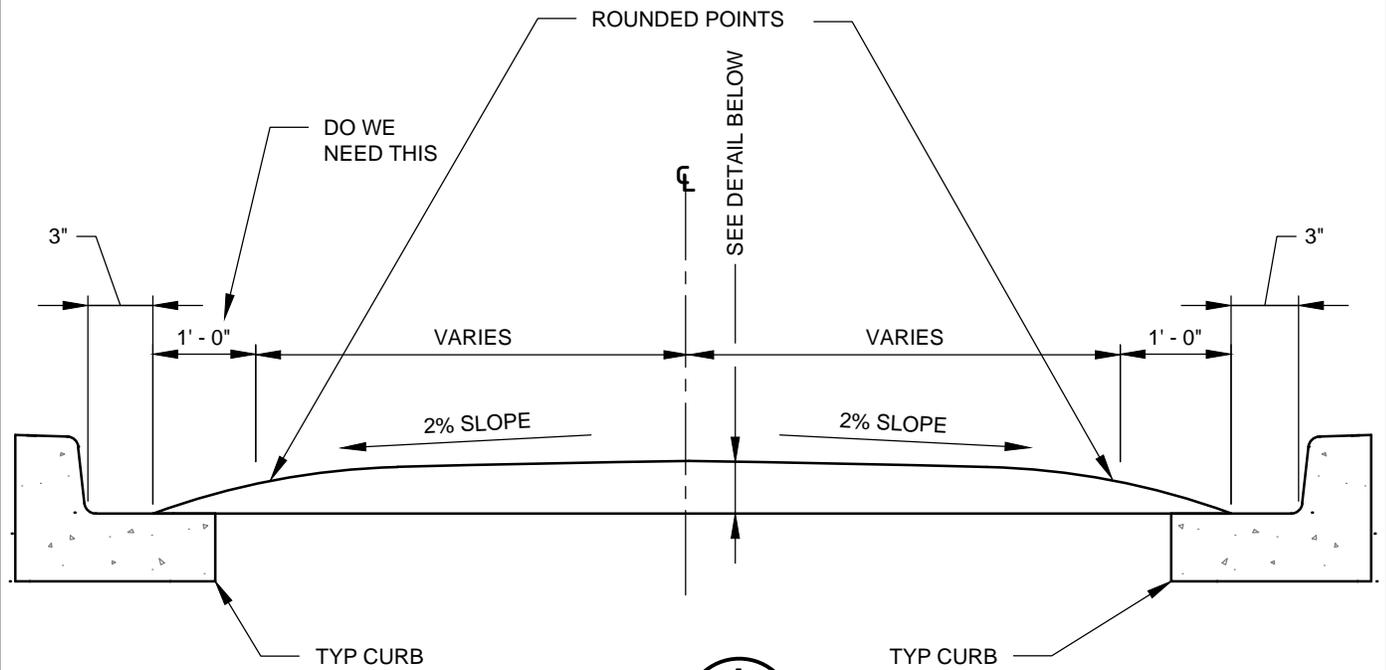


STANDARD DETAILS

SPEED HUMP  
(1 OF 2)

FILE NAME: SD415.DWG

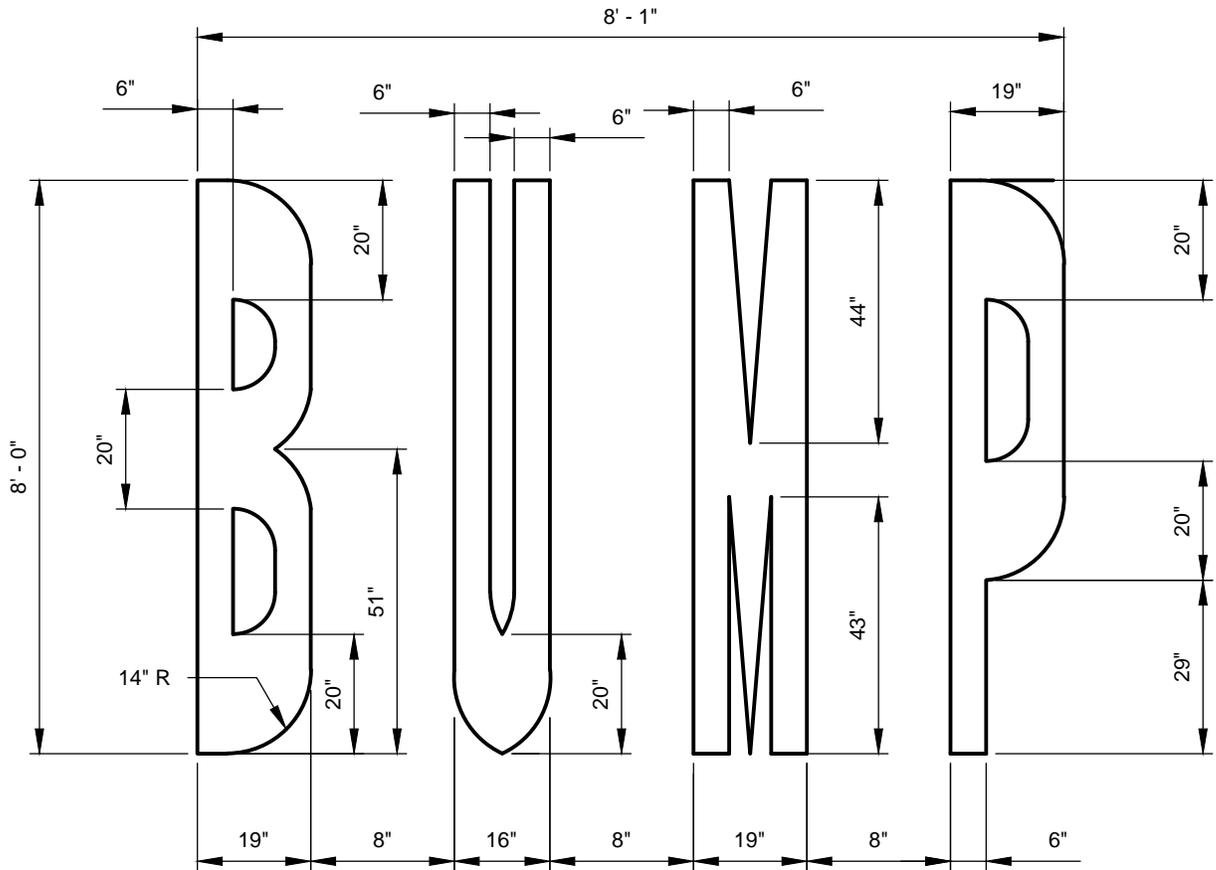
DETAIL NUMBER: 415



*Rob Crittenden*  
 APPROVED BY: ROB CRITTENDEN  
 TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER  
 REVISION DATE: JULY 01, 2015



STANDARD DETAILS  
 SPEED HUMP  
 (2 OF 2)  
 FILE NAME: SD416.DWG    DETAIL NUMBER: 416



**BUMP LEGEND**

NTS

*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



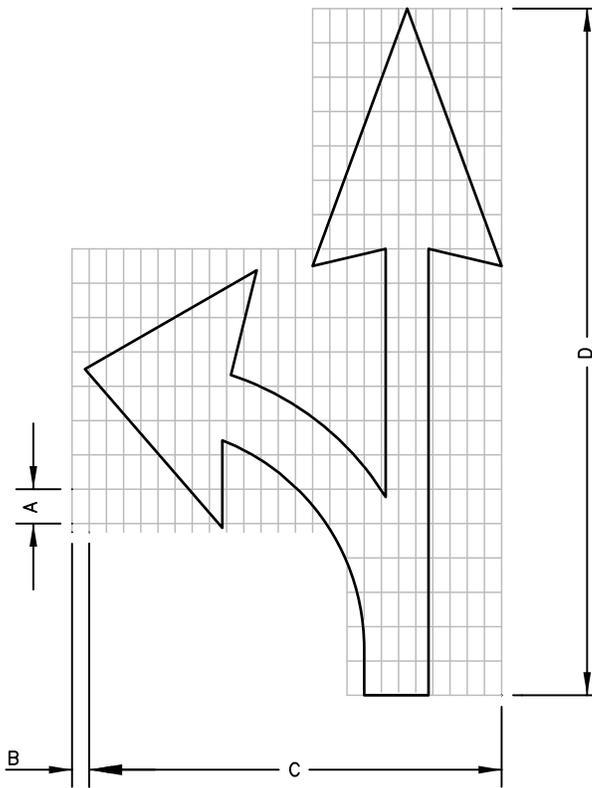
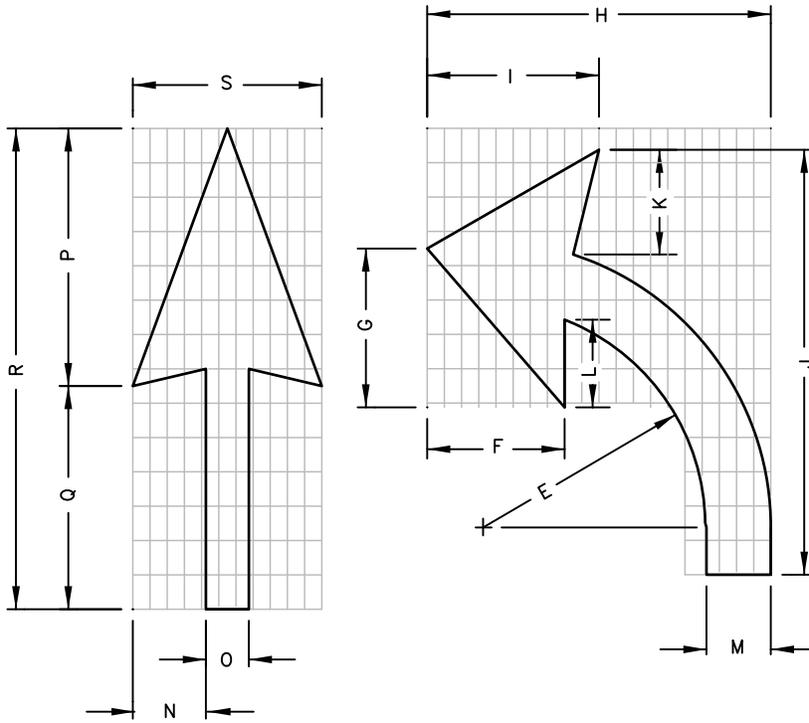
City of Redmond  
WASHINGTON

STANDARD DETAILS

BUMP LEGEND

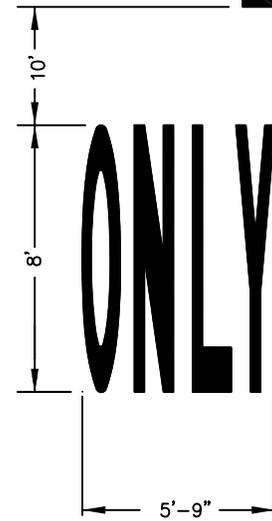
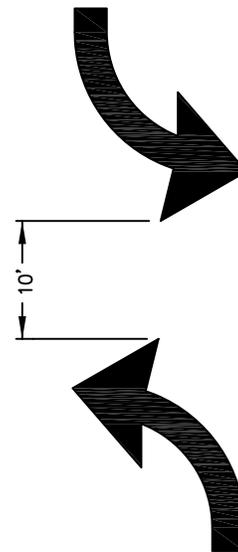
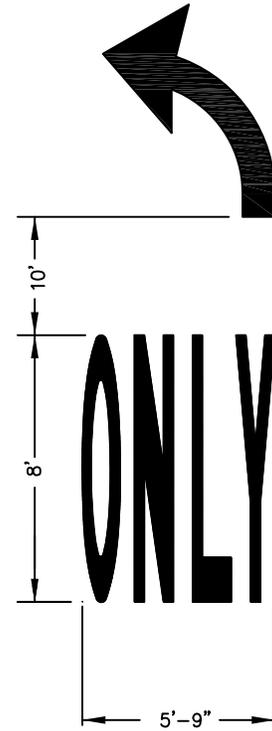
FILE NAME: SD416A.DWG

DETAIL NUMBER: 416A

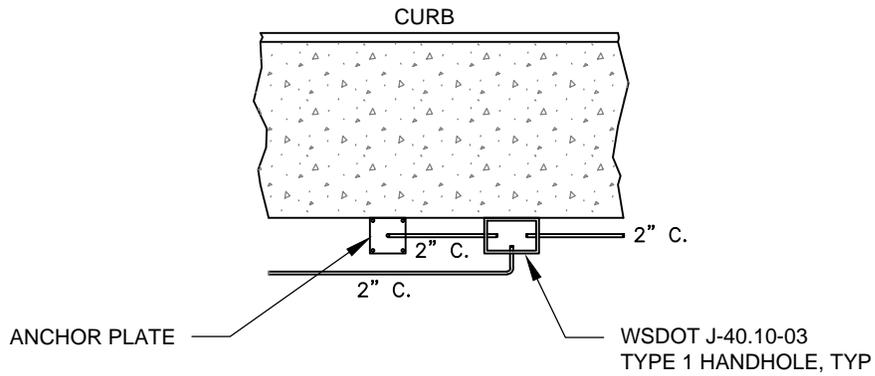


**NOTE:**

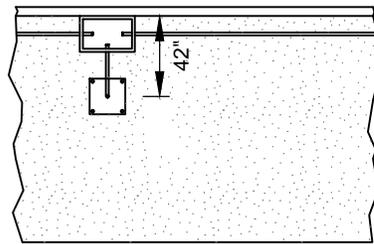
USE THERMOPLASTIC UNLESS DIRECTED OTHERWISE BY CITY TRAFFIC ENGINEER



|    |    |       |        |       |       |       |       |       |       |        |       |       |       |     |       |       |       |       |
|----|----|-------|--------|-------|-------|-------|-------|-------|-------|--------|-------|-------|-------|-----|-------|-------|-------|-------|
| A  | B  | C     | D      | E     | F     | G     | H     | I     | J     | K      | L     | M     | N     | O   | P     | Q     | R     | S     |
| 8" | 4" | 8'-0" | 13'-4" | 4'-4" | 2'-8" | 3'-1" | 6'-7" | 3'-4" | 8'-3" | 1'-11" | 1'-8" | 1'-3" | 1'-5" | 10" | 5'-0" | 4'-4" | 9'-4" | 3'-8" |



SIDEWALK  
NTS



NO SIDEWALK  
NTS

*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015

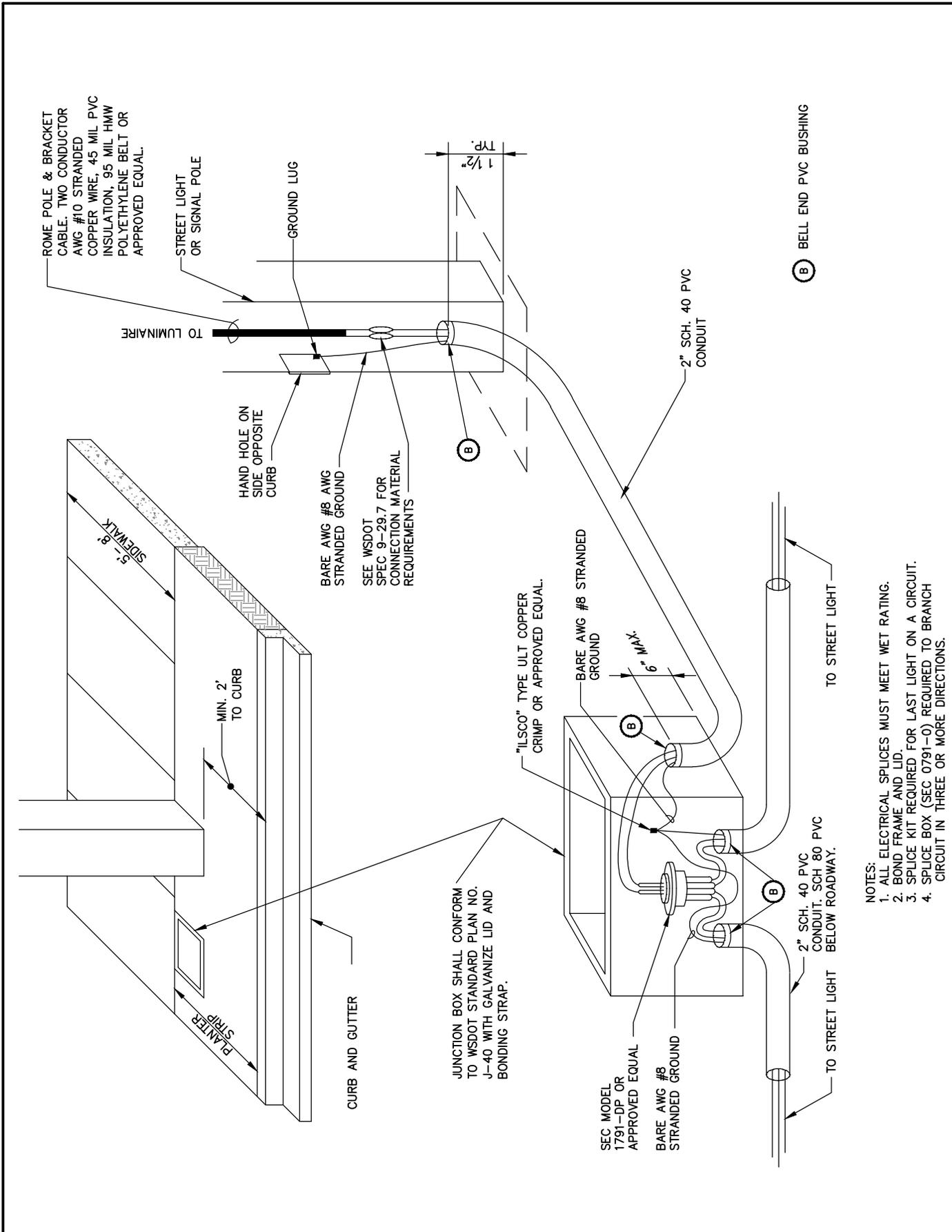


City of Redmond  
WASHINGTON

STANDARD DETAILS

TYPICAL LUMINARE LOCATIONS

FILE NAME: SD420A.DWG | DETAIL NUMBER: 420A



ⓑ BELL END PVC BUSHING

- NOTES:
1. ALL ELECTRICAL SPLICES MUST MEET WET RATING.
  2. BOND FRAME AND LID.
  3. SPLICE KIT REQUIRED FOR LAST LIGHT ON A CIRCUIT.
  4. SPLICE BOX (SEC 0791-0) REQUIRED TO BRANCH CIRCUIT IN THREE OR MORE DIRECTIONS.

*Rob Crittenden*  
 APPROVED BY: ROB CRITTENDEN  
 TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



STANDARD DETAILS

UNIFORM LUMINAIRE WIRING

FILE NAME: SD421.DWG    DETAIL NUMBER: 421

90 DEGREE CROSSPIECE  
WITH 12" BLADE HOLDERS



2" SQUARE CAP WITH  
12" BLADE HOLDER

SIGN FABRICATION DETAIL

NTS

MUTCD TABLE 2D-2. MINIMUM LETTER HEIGHTS ON STREET NAME SIGNS

| TYPE OF MOUNTING | TYPE OF STREET | SPEED LIMIT      | MINIMUM LETTER HEIGHT |             | BLADE SIZE |           |
|------------------|----------------|------------------|-----------------------|-------------|------------|-----------|
|                  |                |                  | INITIAL UPPER CASE    | LOWER CASE  | MIN        | MAX       |
| OVERHEAD         | ALL TYPES      | ALL SPEED LIMITS | 12 INCHES             | 9 INCHES    | 18" HT.    | 18" HT.   |
| POST-MOUNTED     | MULTI-LANE     | MORE THAN 40 MPH | 8 INCHES              | 6 INCHES    | 10" X 36"  | 10" X 48" |
| POST-MOUNTED     | MULTI-LANE     | 40 MPH OR LESS   | 6 INCHES              | 4.5 INCHES  | 8" X 30"   | 8" X 36"  |
| POST-MOUNTED     | 2-LANE         | ALL SPEED LIMITS | 6 INCHES*             | 4.5 INCHES* | 8" X 30"   | 8" X 36"  |

\* ON LOCAL TWO-LANE STREETS WITH SPEED LIMITS OF 25 MPH OR LESS, 4 INCH INITIAL UPPER-CASE LETTERS WITH 3 INCH LOWER CASE LETTERS MAY BE USED.

NOTES:

1. LETTERS SHALL BE HIGHWAY B SERIES FONT.
2. SIGN CORNERS SHALL BE ROUNDED.
3. FOR INTERSECTIONS THAT HAVE DIFFERING SPEEDS, THE HIGHEST SPEED SHALL DICTATE THE SIZE OF ALL STREET NAME SIGNS.
4. SIGN MATERIAL SHALL BE 0.080 INCH THICK STOCK ALUMINUM WITH WHITE SIGN LETTERS ON GREEN.
5. UNLESS OTHERWISE SPECIFIED, ALL SIGN SHEETING SHALL BE DG<sup>3</sup>.

*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



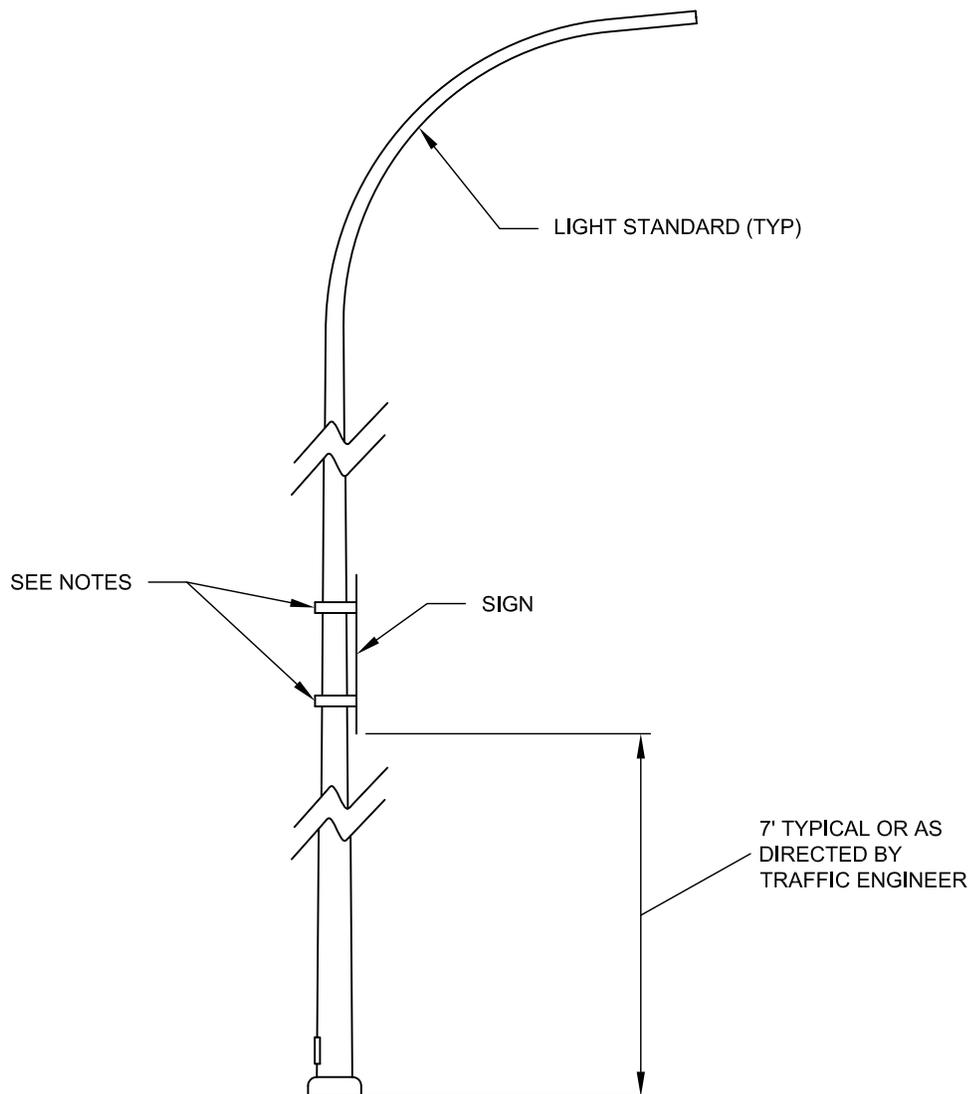
City of Redmond  
WASHINGTON

STANDARD DETAILS

SIGN AND POST  
DETAIL

FILE NAME: SD426A.DWG

DETAIL NUMBER: 426A



**SIGN MOUNTING DETAIL**  
NTS

NOTES:

1. SIGN MOUNTED ON STREETLIGHTS SHALL BE BANDED (3/4" WIDE 0.30 THICKNESS STAINLESS STEEL BANDING WITH BANDING BUCKLES AND BOLT FLARED LEG BUCKLES).
2. ATTACH SIGN TO POST WITH 3/8" X 3" GALVANIZED BOLTS WITH WASHERS ON FRONT AND BACK.

*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2014

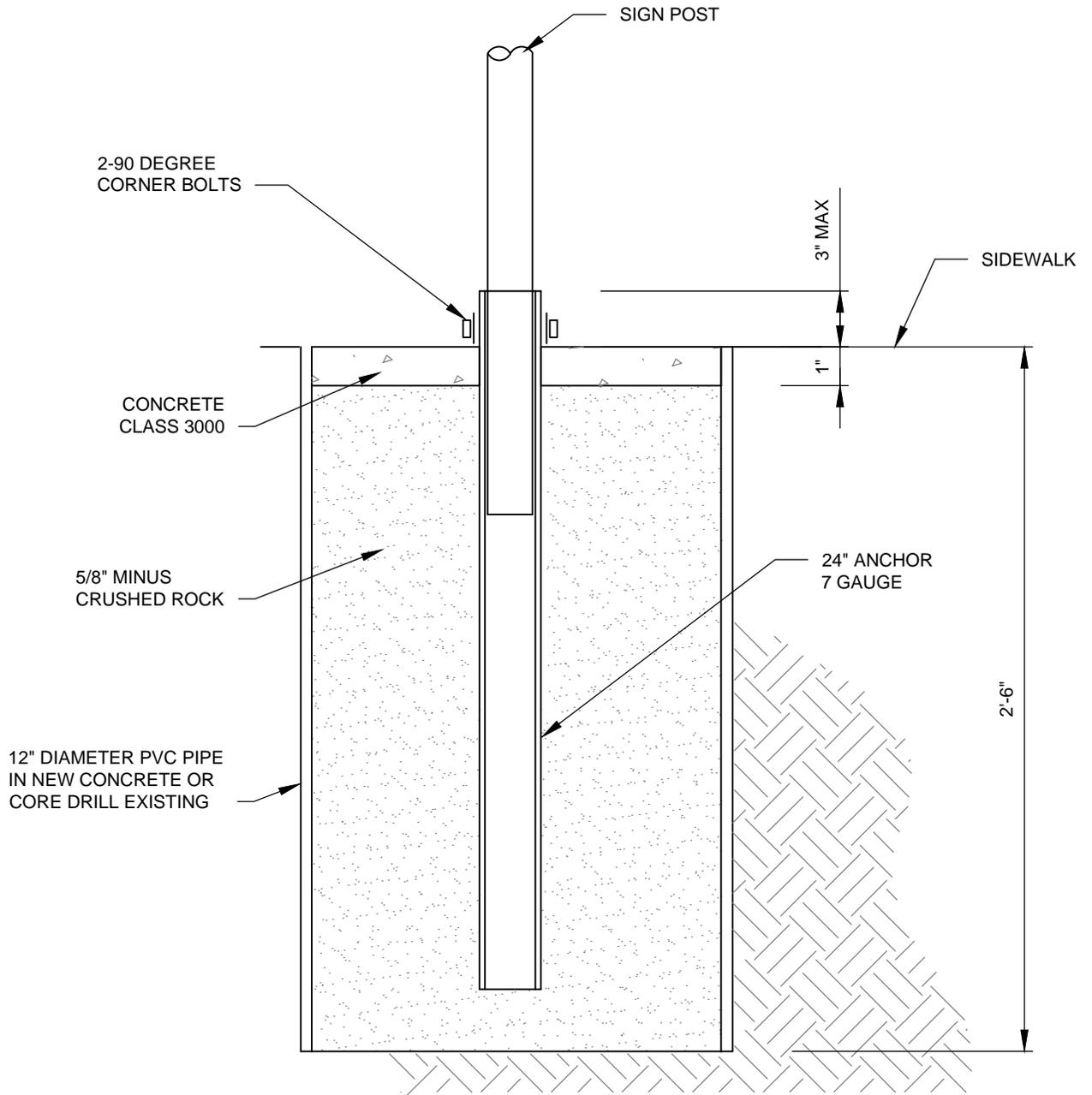


City of Redmond  
WASHINGTON

STANDARD DETAILS

**STREET LIGHT POLE  
MOUNTING SIGN**

FILE NAME: SD426B.DWG | DETAIL NUMBER: **426B**



NOTES:

1. POST: 2" X 2" 14 GAUGE (0.83) QWIK PUNCH A.S.T.M. A570 WITH ANCHOR AND STIFFENER SLEEVE.
2. SIGN HEIGHT: FROM THE GROUND TO THE BOTTOM OF THE SIGN SHALL BE 7'-0" (TYPE PER MUTCD).
3. OFFSET LOCATION PER MUTCD. SIGN POST NOT TO EXTEND PAST TOP OF SIGN. ATTACH SIGN TO POST WITH 3/8" X 3" GALVANIZED BOLTS WITH WASHERS ON FRONT AND BACK.

*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



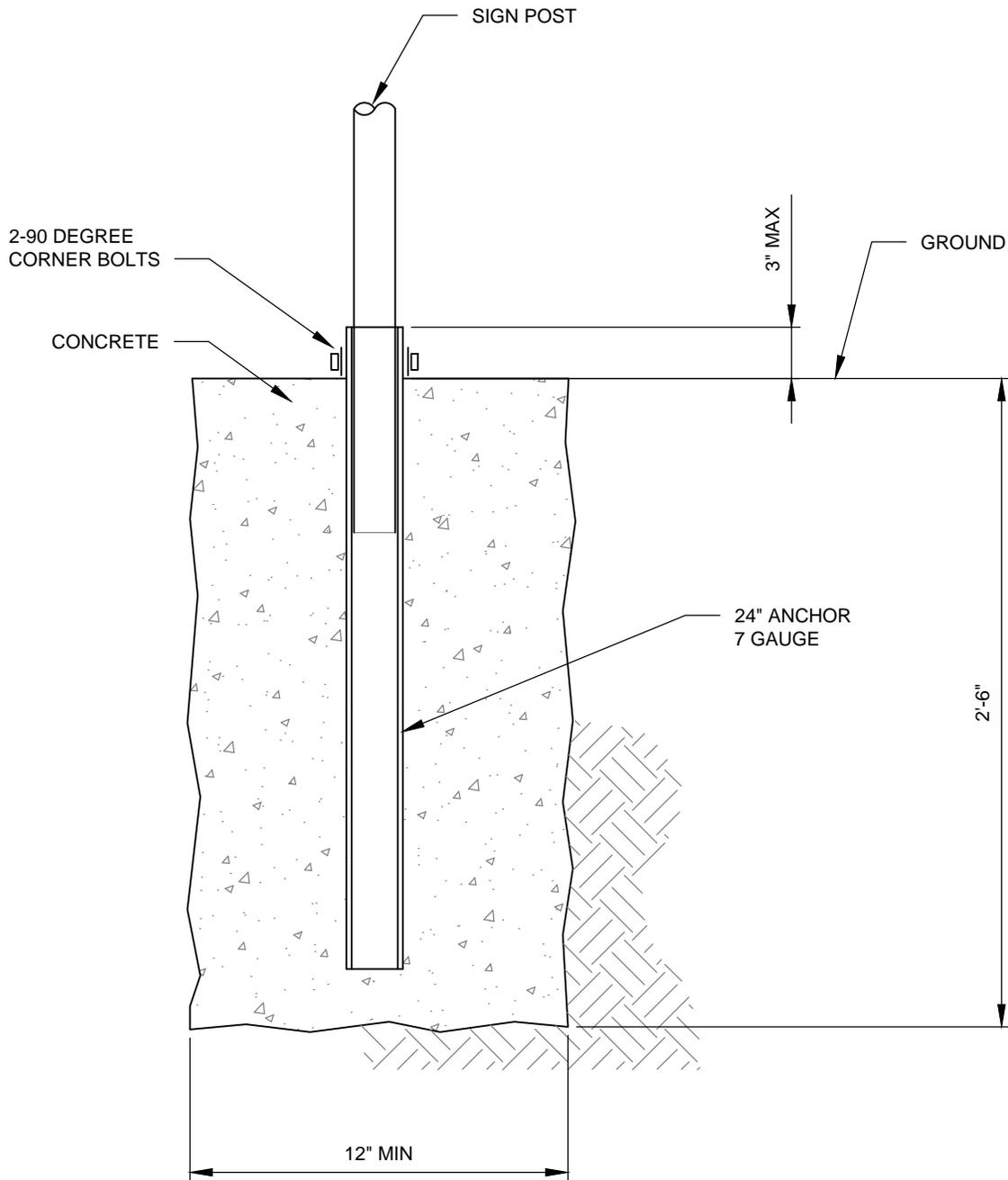
City of Redmond  
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STANDARD DETAILS

SIGN POST IN  
CONCRETE/ASPHALT

FILE NAME: SD426C.DWG

DETAIL NUMBER: 426C



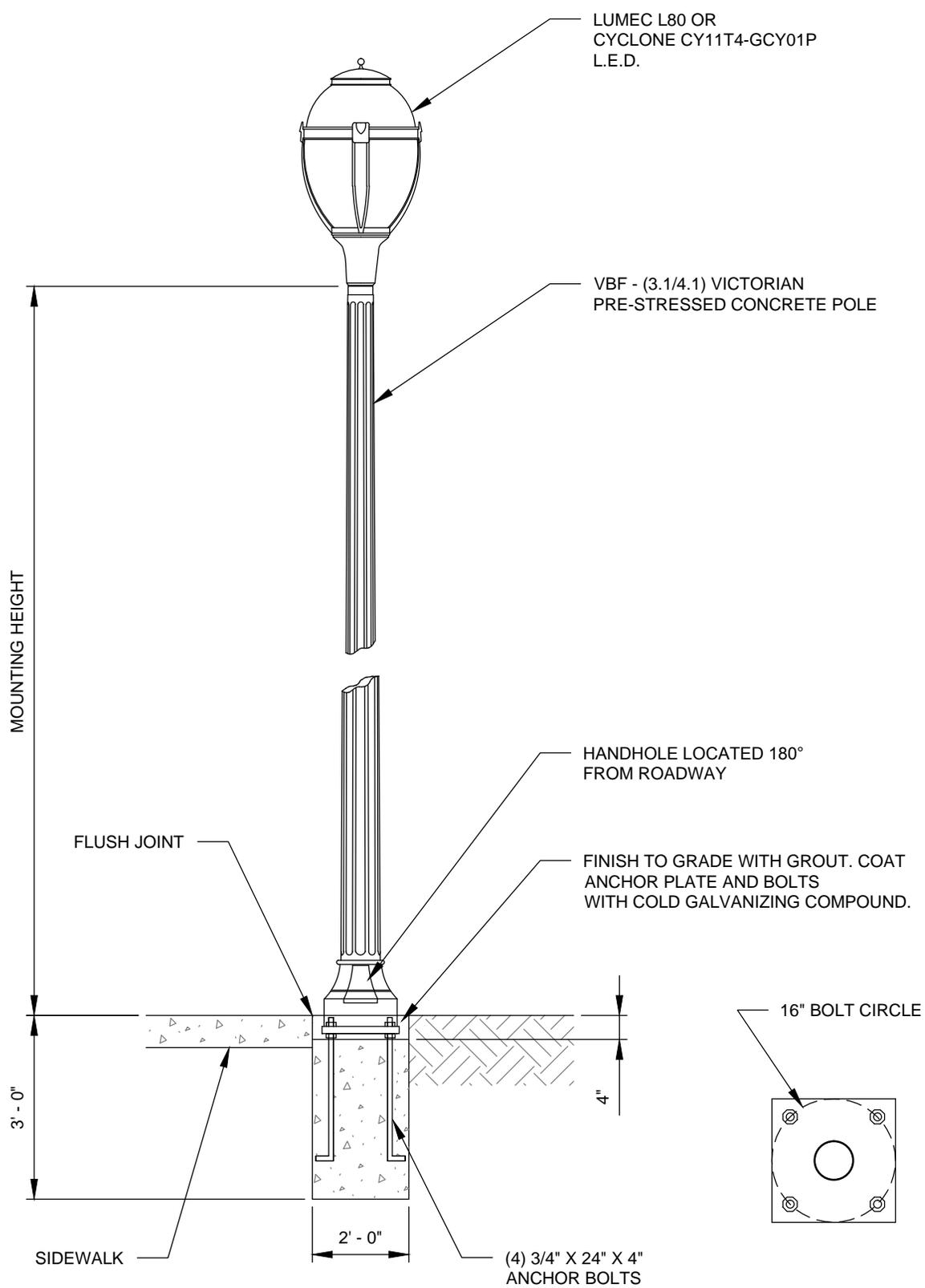
NOTES:

1. POSTS: 2"x2" 14 GAUGE (.083) QWIK PUNCH A.S.T.M. A570, WITH ANCHOR AND STIFFENER SLEEVE.
2. SIGN HEIGHT: FROM THE GROUND TO THE BOTTOM OF THE SIGN SHALL BE 7'-0" (TYP. PER MUTCD).
3. OFFSET LOCATION PER MUTCD: SIGN POST NOT TO EXTEND PAST TOP OF SIGN. ATTACH SIGN TO POST WITH 3/8"x3" GALVANIZED BOLTS WITH WASHERS ON FRONT AND BACK.

*Rob Crittenden*  
 APPROVED BY: ROB CRITTENDEN  
 TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER  
 REVISION DATE: JULY 01, 2015



STANDARD DETAILS  
 SIGN POST IN GROUND  
 FILE NAME: SD426D.DWG | DETAIL NUMBER: 426D



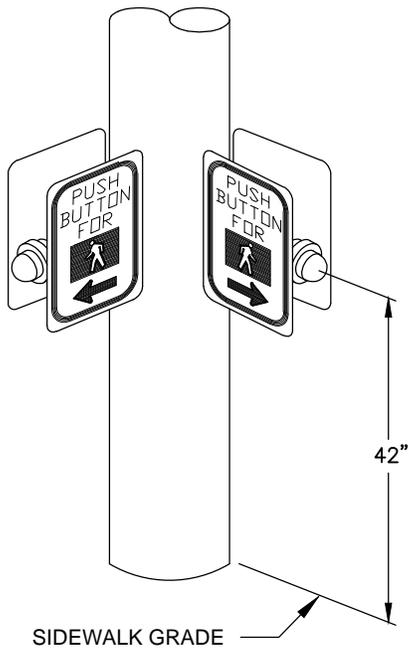
**ELEVATION**  
NTS

**BASE PLATE**  
NTS

*Rob Crittenden*  
APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER  
REVISION DATE: JULY 01, 2015



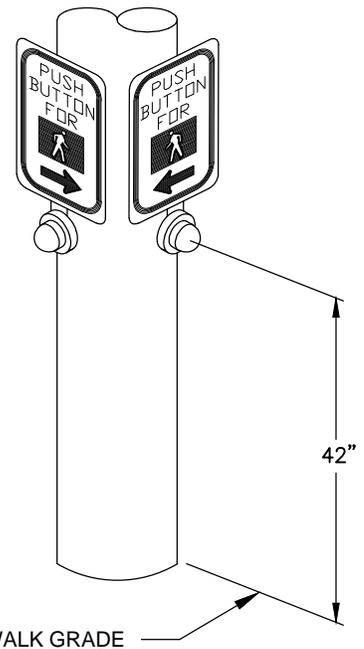
STANDARD DETAILS  
CONCRETE PEDESTRIAN SCALE  
POLE  
FILE NAME: SD430B.DWG | DETAIL NUMBER: 430B



SIDEWALK GRADE

**STYLE H-1**

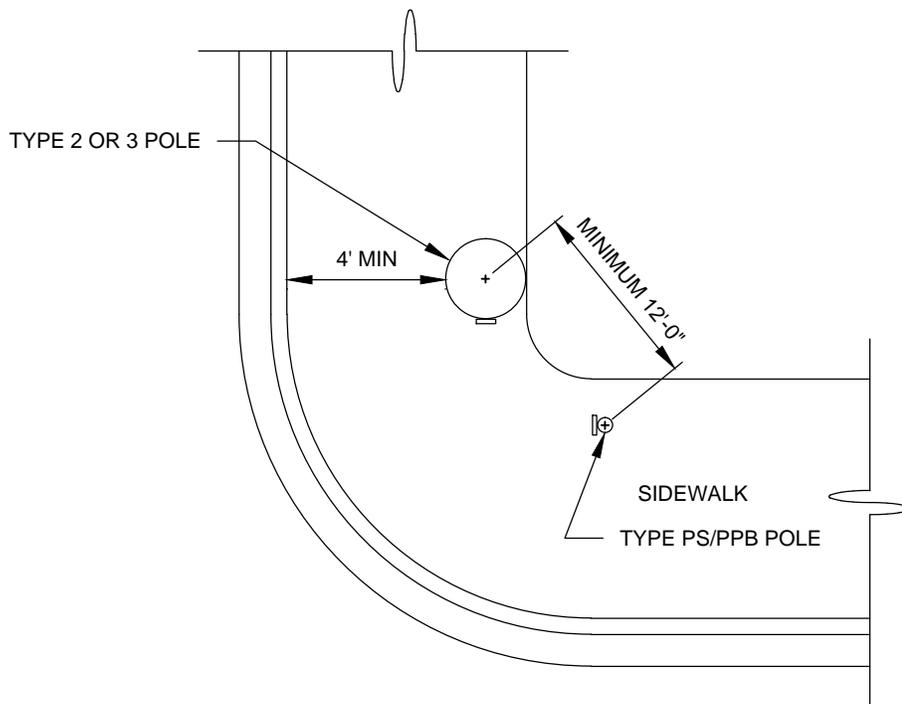
NTS



SIDEWALK GRADE

**STYLE ADA**

NTS



**PLAN VIEW**

NTS

NOTES:

1. PEDESTRIAN PUSH - BUTTON SHALL BE ACCESSIBLE STYLE UNLESS OTHERWISE SPECIFIED IN THE PLANS.
2. SEE REDMOND SIGNAL STANDARDS MANUAL FOR POLE PLACEMENT LOCATION RELATIVE TO WHEELCHAIR RAMPS.

*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



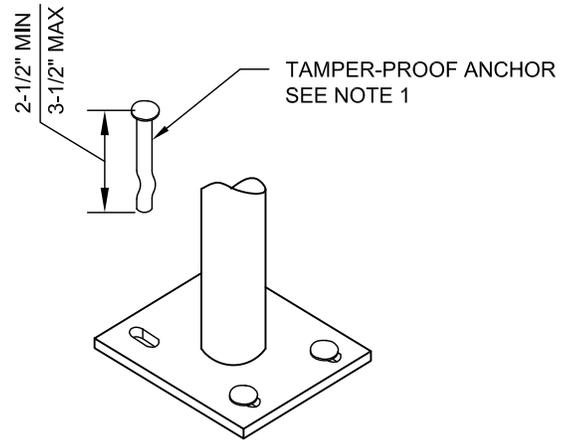
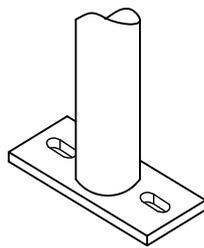
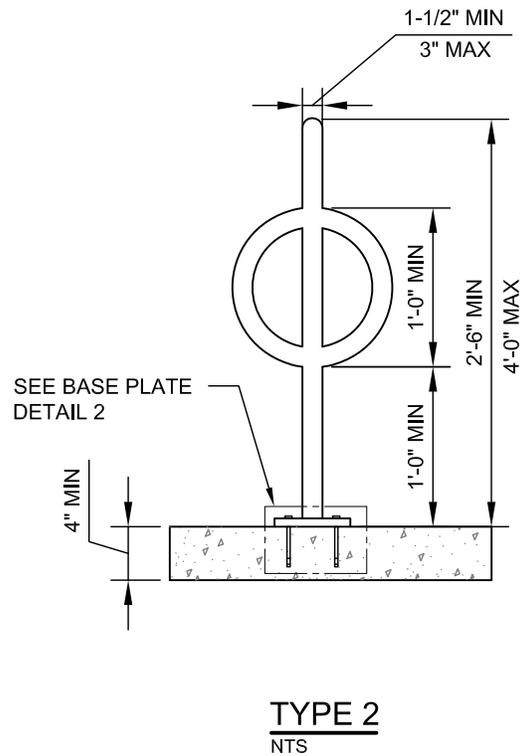
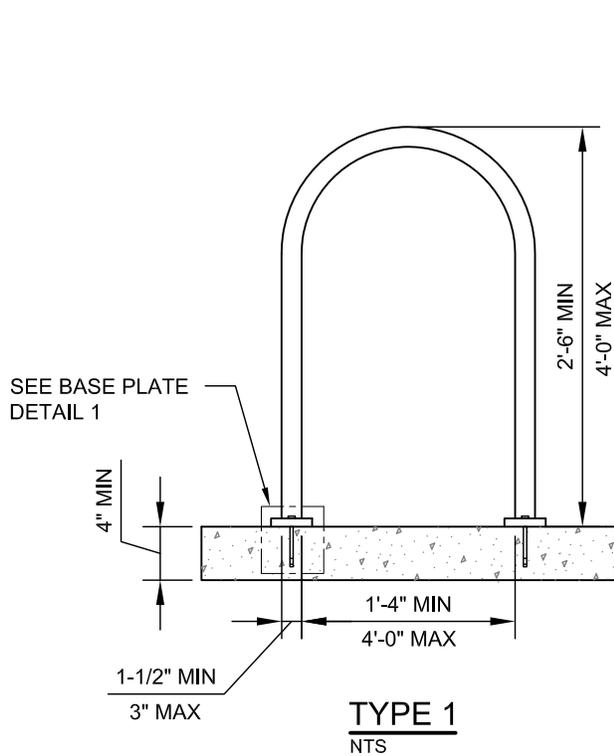
City of Redmond  
WASHINGTON

STANDARD DETAILS

PEDESTRIAN PUSH BUTTON  
ORIENTATION

FILE NAME: SD453.DWG

DETAIL NUMBER: 453



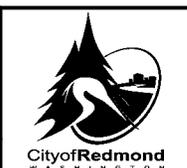
NOTES:

1. SECURE RACKS TO CONCRETE WITH TAMPER-PROOF ANCHORS AS PROVIDED BY VENDOR, OR CONCRETE SPIKES. ANCHOR HEAD WILL BE SIZED TO SECURED RACK AND BETWEEN 2-1/2" AND 3-1/2" LONG.
2. THE HOLES HAVE TO BE AT LEAST 6" AWAY FROM ANY CRACKS IN THE BASE MATERIAL.
3. OTHER BICYCLE RACKS MAY BE APPROVED BY THE TRANSPORTATION ENGINEER.

*Donald Cairns*

APPROVED BY: DONALD CAIRNS  
TRANSPORTATION PLANNING AND ENGINEERING MANAGER

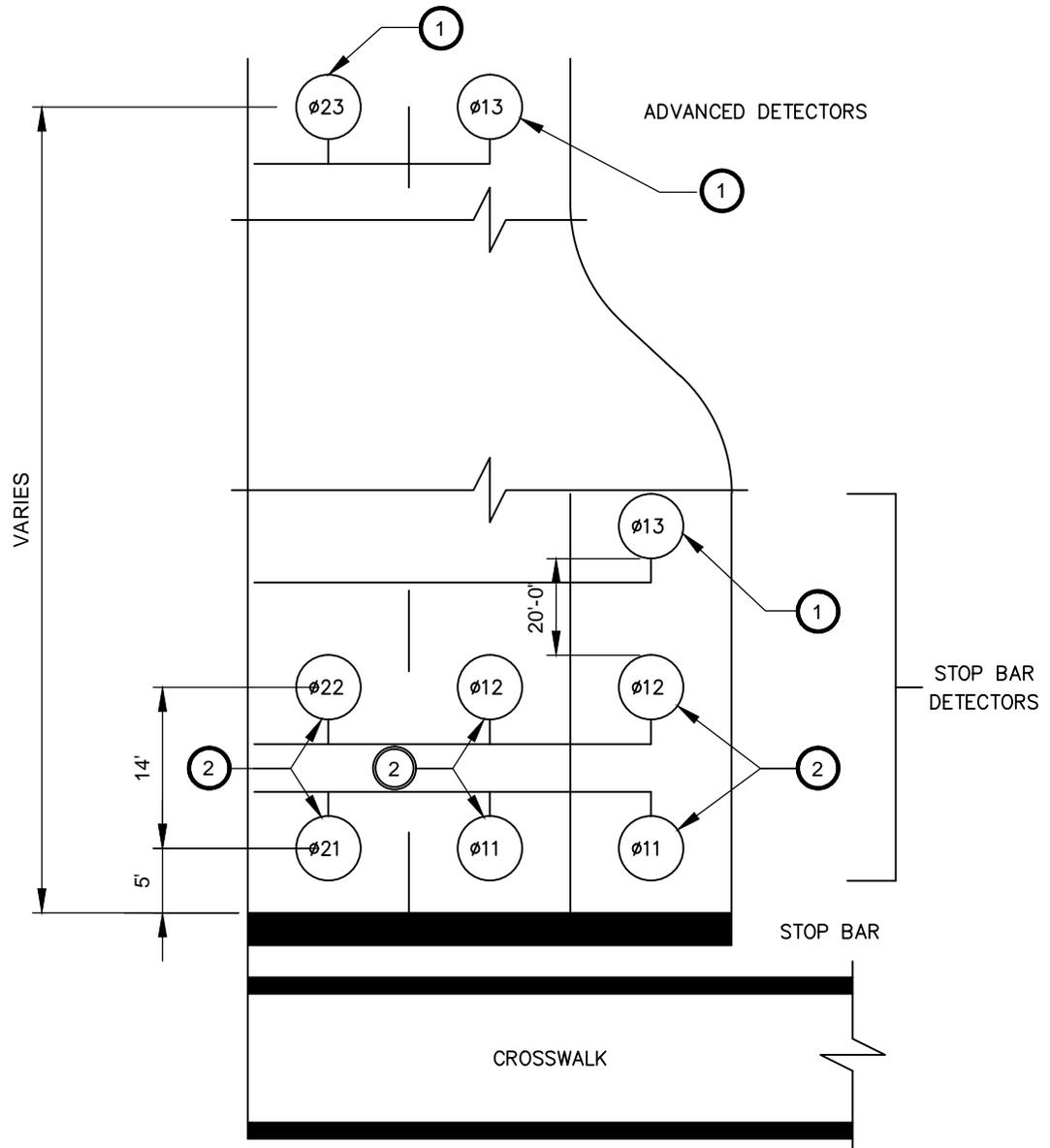
REVISION DATE: JULY 01, 2014



STANDARD DETAILS

**APPROVED BICYCLE RACKS**

FILE NAME: SD454.DWG    DETAIL NUMBER: **454**



NOTES:

1. SPLICE INDIVIDUAL LEADS TO SHIELDED, TWISTED CONDUCTOR PAIR IN ADJACENT JUNCTION BOX.
2. CONNECT DETECTOR LEADS IN SERIES AND SPLICE TO SHIELDED, TWISTED CONDUCTOR PAIR IN ADJACENT JUNCTION BOX.
3. LOOPS TO BE CENTERED IN LANES.
4. NUMBER CORRESPONDS TO THE PHASE NUMBER AS SHOWN ON THE DESIGN.
5. SEE STANDARD DETAIL 455, 456 AND WSDOT STANDARD PLAN J-50 FOR LOOP CONSTRUCTION DETAILS. SEE STANDARD DETAIL 311A AND 311B FOR CROSSWALK AND STOP LINE DETAILS.
6. FINAL LOOP LOCATION TO BE APPROVED BY THE ENGINEER.

*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



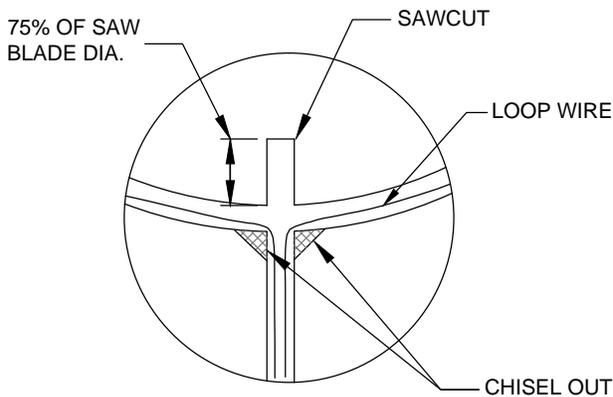
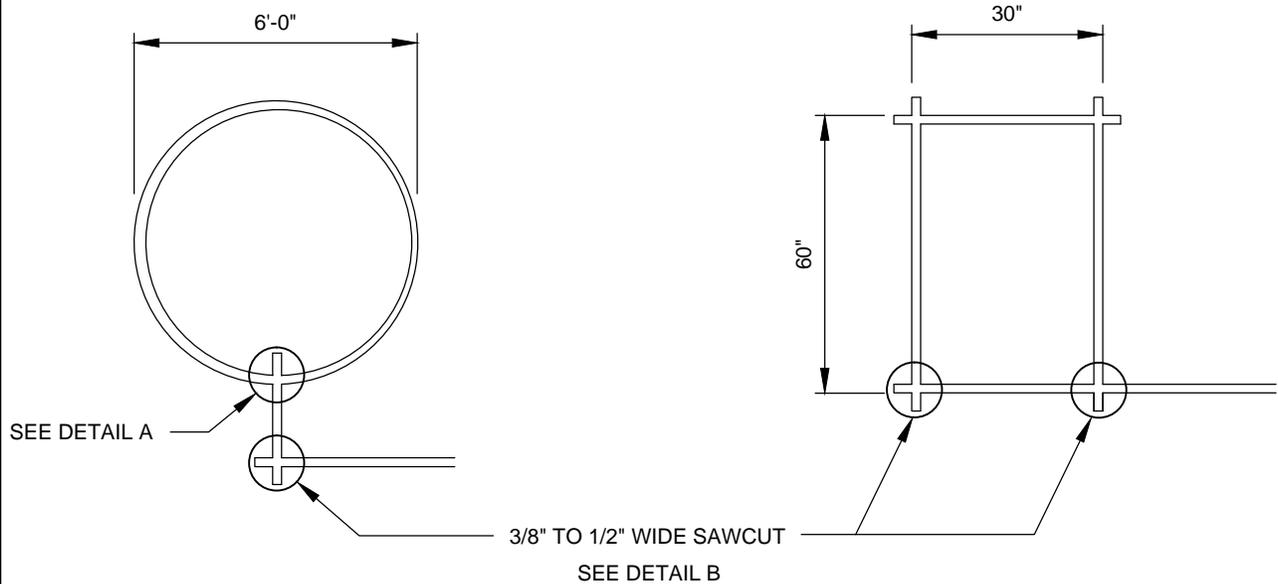
City of Redmond  
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STANDARD DETAILS

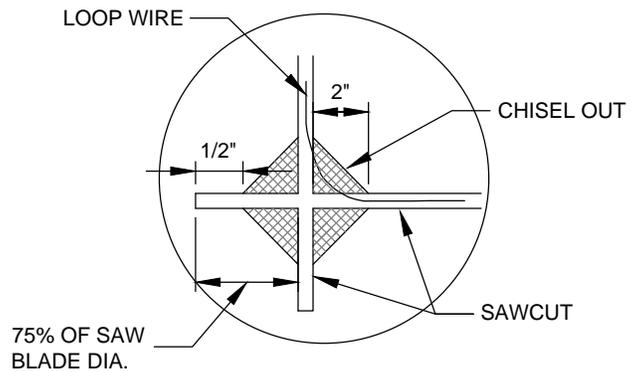
LOOP DETECTOR NUMBERING  
AND SPACING

FILE NAME: SD454A.DWG

DETAIL NUMBER: 454A



**DETAIL A**  
NTS



**DETAIL B**  
NTS

NOTES:

1. DEPTH OF SAWCUT SHALL BE 2.5" UNLESS SPECIFIED OTHERWISE (2.75" AT PAVEMENT JOINTS)
2. ALL AREAS WHERE LOOP WIRES ARE TO BE PLACED SHALL HAVE A UNIFORM DEPTH AND SMOOTHNESS.
3. LOOP SEALANT SHALL BE CRAFCO MSI 34271. APPLY PER MANUFACTURER'S RECOMMENDATIONS.
4. ALL LOOPS SHALL HAVE 4 TURNS OF WIRE.
5. BICYCLE LOOPS SHALL HAVE 5 TURNS OF WIRE.
6. SAWCUT SHALL BE CONSTRUCTED TO MINIMIZE NUMBER OF BENDS BETWEEN LOOPS AND THE ADJACENT BOX.

*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



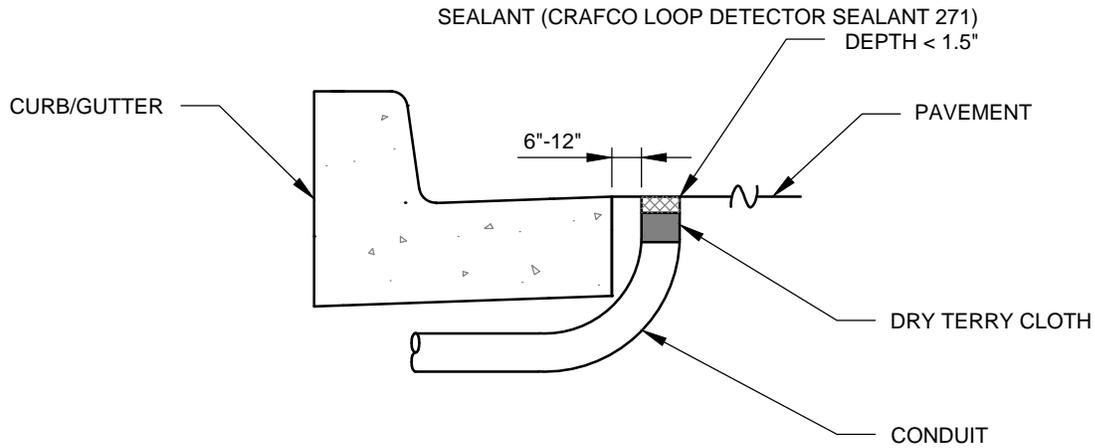
City of Redmond  
WASHINGTON

STANDARD DETAILS

LOOP DETECTOR SAWCUT

FILE NAME: SD455.DWG

DETAIL NUMBER: 455

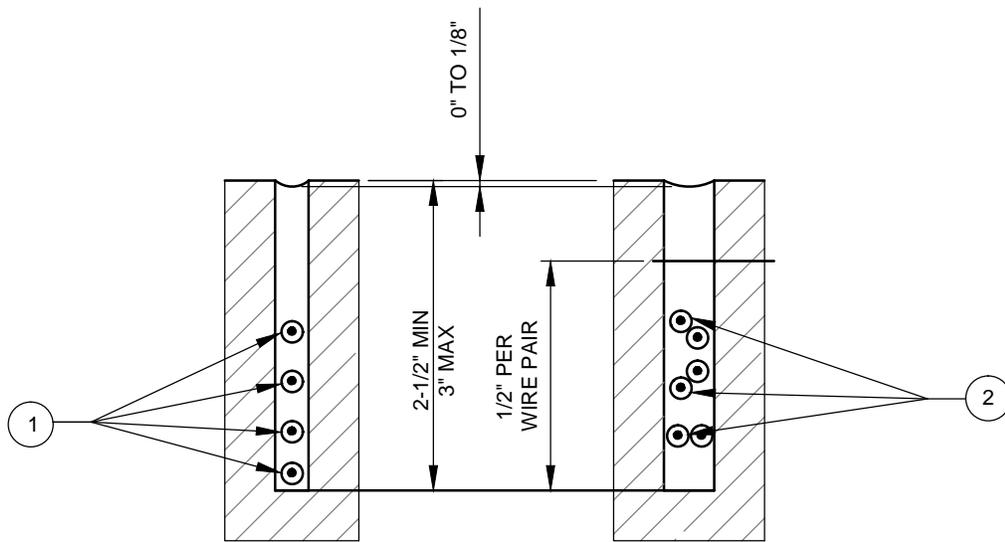


**CONDUIT UNDER CURB**

NTS

**NOTE:**

HOLES > 3-1/2" DIA. REQUIRE AN ASPHALT PATCH.  
 ASPHALT SHALL BE HMA FOR PAVEMENT REPAIR.



**1/4" LOOP SAWCUT**

NTS

**1/2" LEAD-IN SAWCUT**

NTS

**CROSS-SECTION OF SAWCUTS**

NTS

**NOTES:**

- ① LOOP WIRE - NUMBER VARIES. (SEE LOOP WINDING DETAILS)
- ② LEAD - IN WIRES: ONE PAIR FOR EACH LOOP SERVED. THREE PAIRS MAXIMUM PER SAWCUT.
- 3. EXTEND SAWCUT SUFFICIENT LENGTH TO PROVIDE FULL DEPTH AROUND CORNER.
- 4. STOP BAR DETECTION STUB-OUT TYPICALLY 13 FEET FROM STOP LINE.

*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
 TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



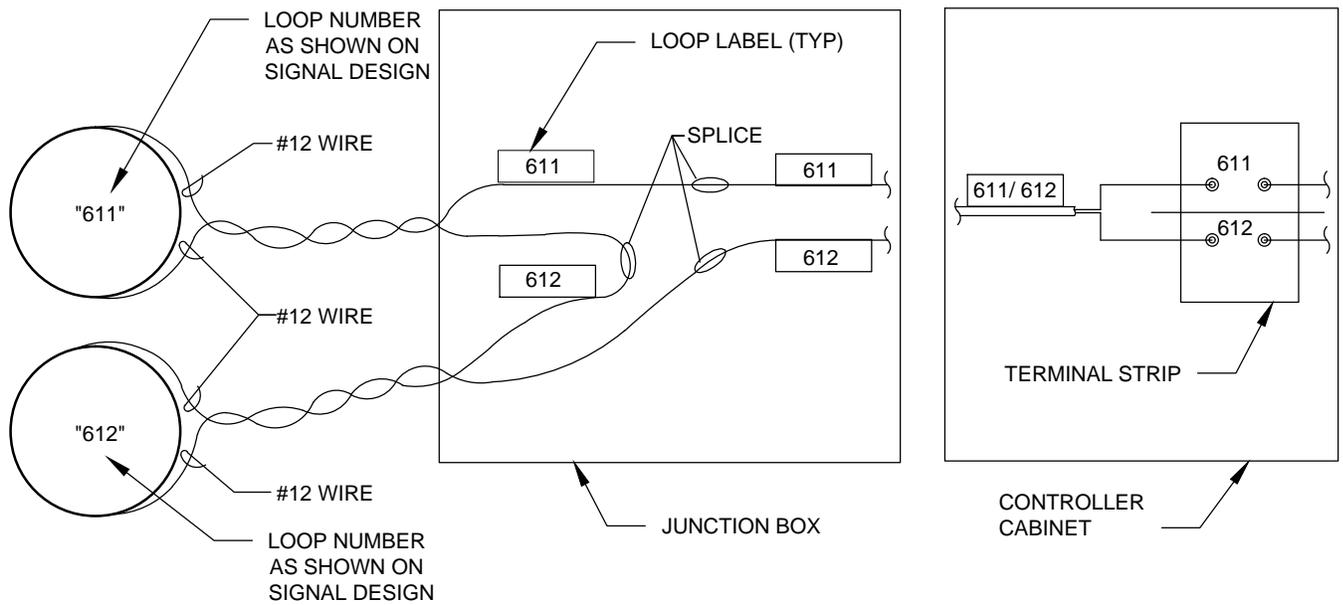
City of Redmond  
 WASHINGTON

STANDARD DETAILS

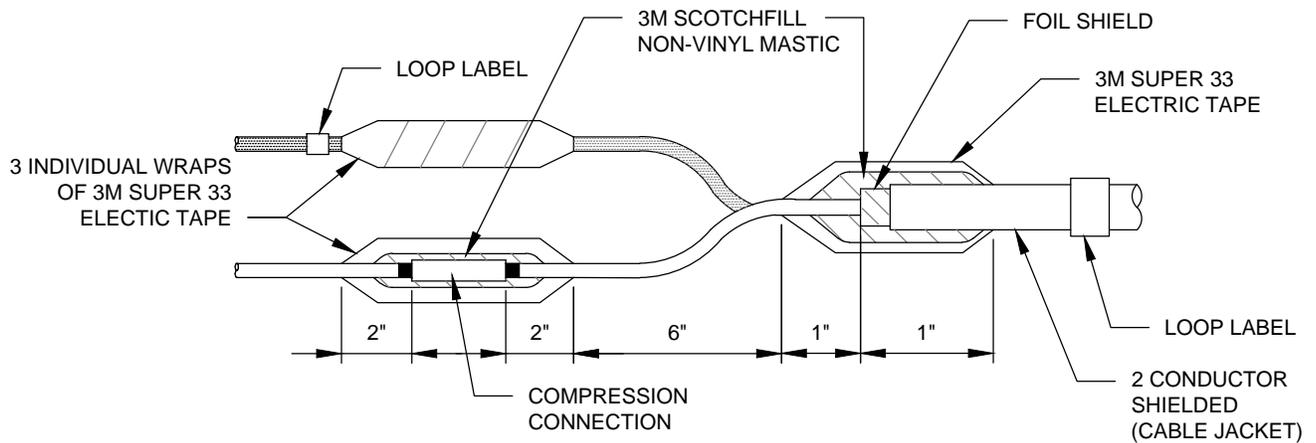
**SAWCUT AND CONDUIT  
 UNDER CURB**

FILE NAME: SD456.DWG

DETAIL NUMBER: 456



**LOOP WIRE LABELING DETAIL**  
NTS



**LOOP SPLICE DETAIL**  
NTS

*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



City of Redmond  
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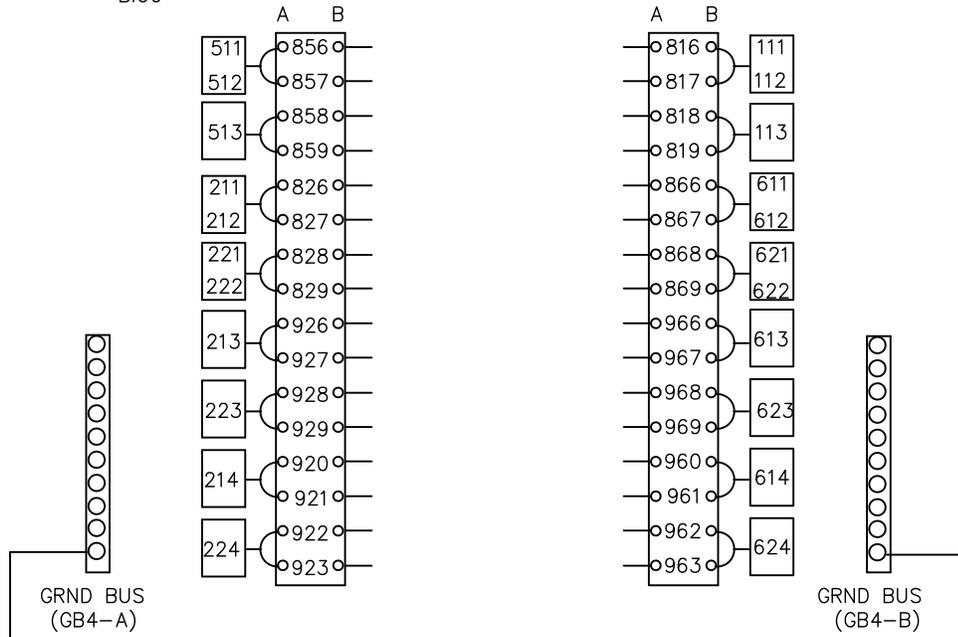
STANDARD DETAILS

LOOP DETECTOR SPLICE AND  
WIRING LABEL

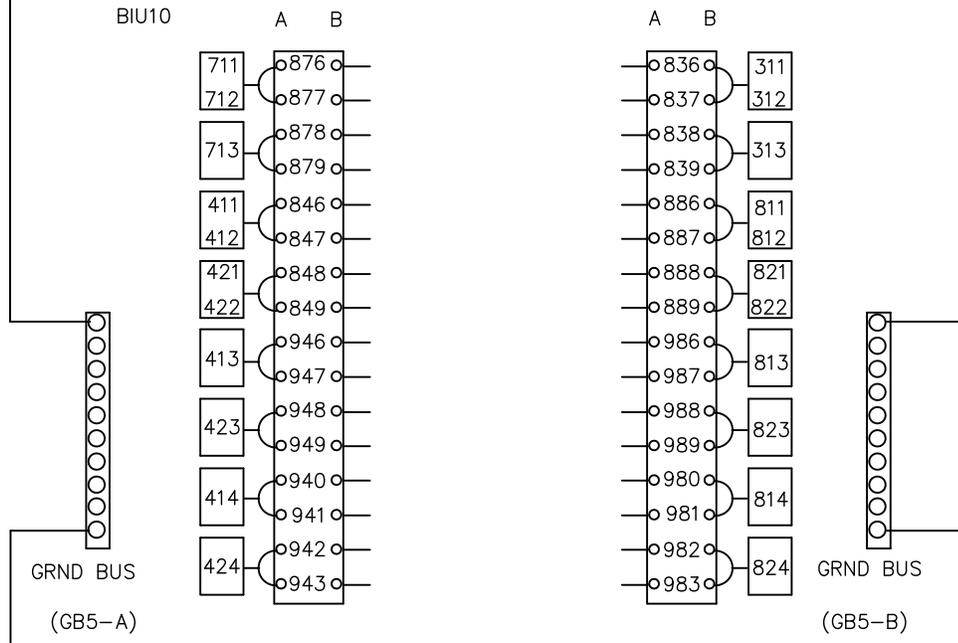
FILE NAME: SD457.DWG

DETAIL NUMBER: 457

BIU9



BIU10



*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2014



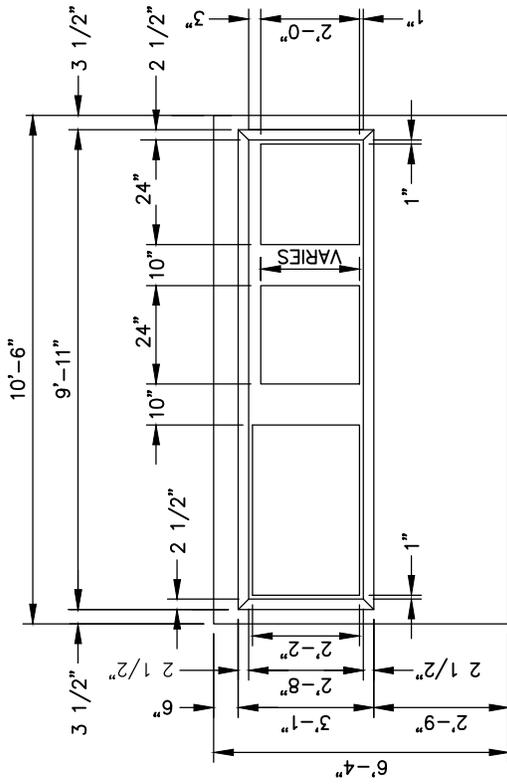
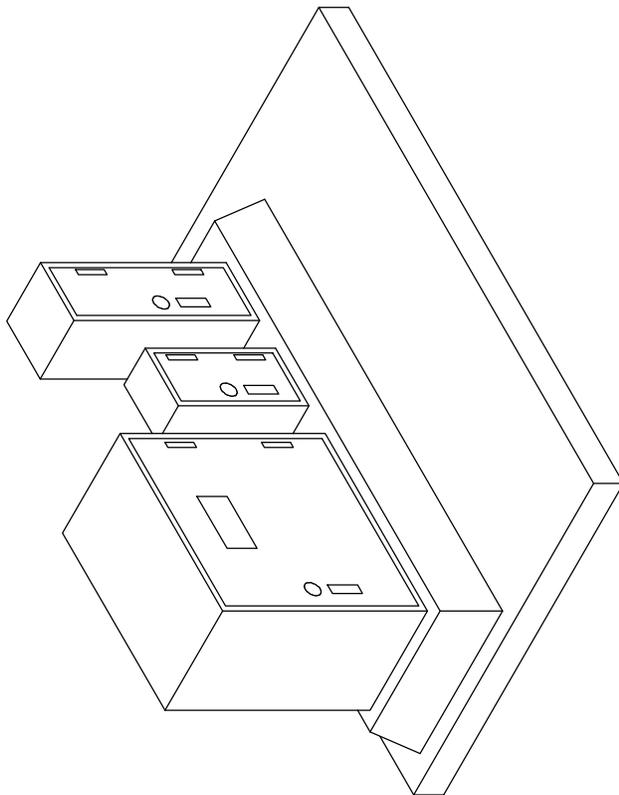
City of Redmond  
WASHINGTON

STANDARD DETAILS

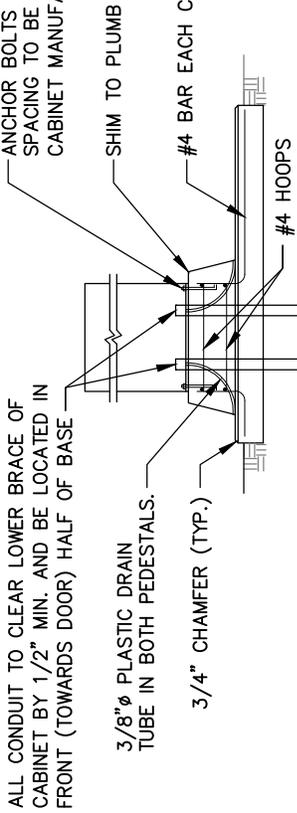
**LOOP TERMINAL STRIP**

FILE NAME: SD458.DWG

DETAIL NUMBER: **458**



ANCHOR BOLTS AND DATA FOR SPACING TO BE SUPPLIED BY CABINET MANUFACTURER (TYP.)



NOTES:

1. CABINET FOUNDATION SHALL BE CLASS 3000 AIR ENTRAINED CONCRETE.
2. WHERE CABINET FOUNDATION IS LOCATED ADJACENT TO A SIDEWALK, CONSTRUCT THE TOP FLUSH WITH SIDEWALK GRADE, OMITTING CHAMFER WHERE TOP AND SIDEWALK ABUT.
3. FOUNDATION DESIGN IS TYPICAL, CONTRACTOR SHALL UTILIZE CABINET MANUFACTURER'S SPECIFICATIONS TO ASSURE PROPER FIT OF CABINET ON BASE WITH RESPECT TO CONDUIT PLACEMENT. ENGINEER MUST APPROVE FORM WORK BEFORE PLACEMENT OF CONCRETE.
4. EXPANSION ANCHOR BOLTS (KWIK-BOLT, WEJ-IT, OR EQUAL) MAY BE USED TO FIT CABINETS TO EXISTING FOUNDATION.

5. SILICON SEAL SHALL BE PLACED BETWEEN CABINET AND BASE.
6. SEE PLANS FOR DOOR ORIENTATION.
7. A SPARE 3"Ø CONDUIT SHALL BE INSTALLED FROM CONTROLLER CABINET TO NEAREST J-BOX.
8. A 1"Ø CONDUIT SHALL BE INSTALLED UNDER SERVICE CABINET FOR GROUND.
9. IF UPS CABINET NOT PART OF PROJECT, INSTALL CITY FURNISHED CAP FOR EXPOSED CONDUITS.

*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



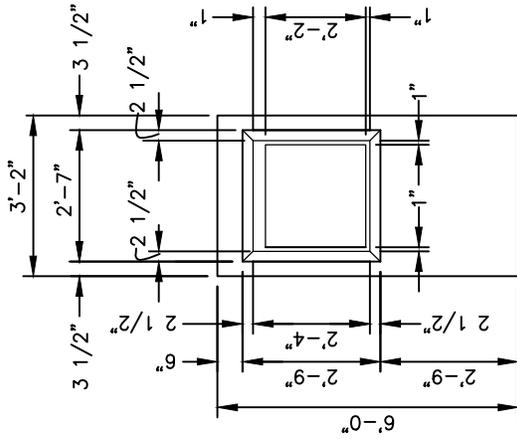
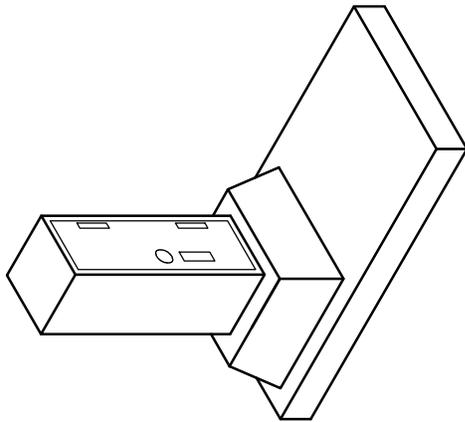
City of Redmond  
WASHINGTON

STANDARD DETAILS

CABINET FOUNDATION

FILE NAME: SD459A.DWG

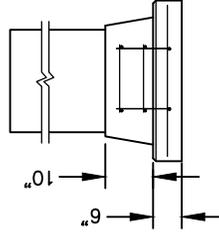
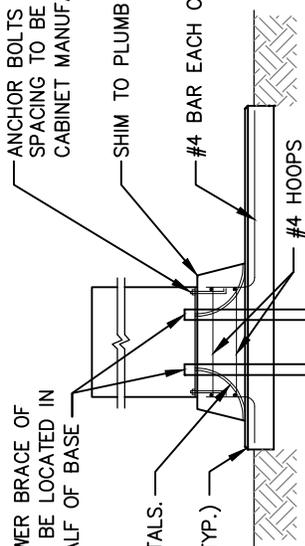
DETAIL NUMBER: 459A



ALL CONDUIT TO CLEAR LOWER BRACE OF CABINET BY 1/2" MIN. AND BE LOCATED IN FRONT (TOWARDS DOOR) HALF OF BASE.

3/8" Ø PLASTIC DRAIN TUBE IN BOTH PEDESTALS.

3/4" CHAMFER (TYP.)



**NOTES:**

1. CABINET FOUNDATION SHALL BE CLASS 3000 AIR ENTRAINED CONCRETE.
2. WHERE CABINET FOUNDATION IS LOCATED ADJACENT TO A SIDEWALK, CONSTRUCT THE TOP FLUSH WITH SIDEWALK GRADE, OMITTING CHAMFER WHERE TOP AND SIDEWALK ABUT.
3. FOUNDATION DESIGN IS TYPICAL, CONTRACTOR SHALL UTILIZE CABINET MANUFACTURER'S SPECIFICATIONS TO ASSURE PROPER FIT OF CABINET ON BASE WITH RESPECT TO CONDUIT PLACEMENT. INSPECTOR MUST APPROVE FORM WORK BEFORE PLACEMENT OF CONCRETE.
4. EXPANSION ANCHOR BOLTS (KWIK-BOLT, WEJ-IT, OR EQUAL) MAY BE USED TO FIT CABINETS TO EXISTING FOUNDATION.
5. SILICON SEAL SHALL BE PLACED BETWEEN CABINET AND BASE.
6. SEE PLANS FOR DOOR ORIENTATION.
7. A SPARE 3" Ø CONDUIT SHALL BE INSTALLED FROM CONTROLLER CABINET TO NEAREST J-BOX.
8. A 1" Ø CONDUIT SHALL BE INSTALLED UNDER SERVICE CABINET FOR GROUND.

*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



City of Redmond  
WASHINGTON

STANDARD DETAILS

SERVICE CABINET  
FOUNDATION

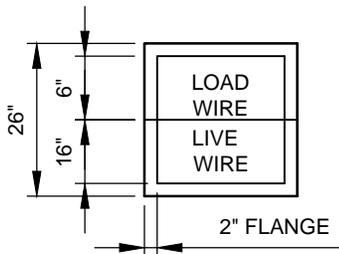
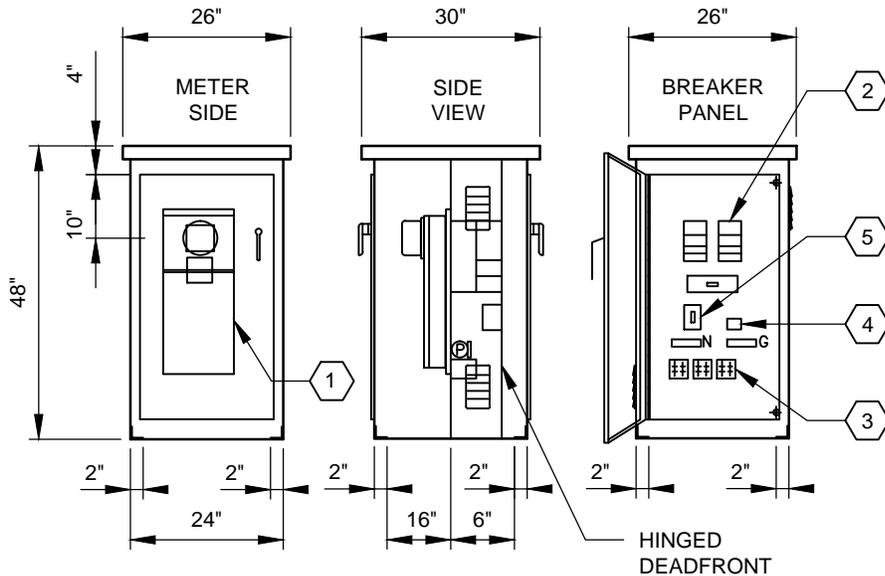
FILE NAME: SD459C.DWG

DETAIL NUMBER: 459C

COMPONENT SCHEDULE

- ① METERBASE: 200 AMP, 4 JAW, AW NO. 124TB SAFETY SOCKET, WITH 5TH JAW AT 9:00 POSITION
- ② PANELBOARD: 120/240 VAC, 200 AMP. 1 PHASE, 3 WIRE. COPPER BUS, 12 CIRCUIT  
 MAIN BREAKER: 125 AMP, 2 POLE, CUTLER HAMMER NO. ED2125 65K AIC SERIES RATED  
 CUTLER HAMMER TYPE BAB BOLT-ON BRANCH BREAKERS:  
 3-15/2 ILLUMINATION BRANCH  
 1-50/1 SIGNAL BRANCH  
 1-15/1 CONTROL CKT BRANCH
- ③ CONTACTOR: 30 A LIGHTING RATED, 2 POLE, 120 VAC COIL  
 1-REQUIRED PER ILLUMINATION CKT
- ④ TERMINAL BLOCK TO REMOTE PHOTOELECTRIC CELL
- ⑤ PHOTO-CELL BYPASS SWITCH, SPST 15 AMP, 277 VAC

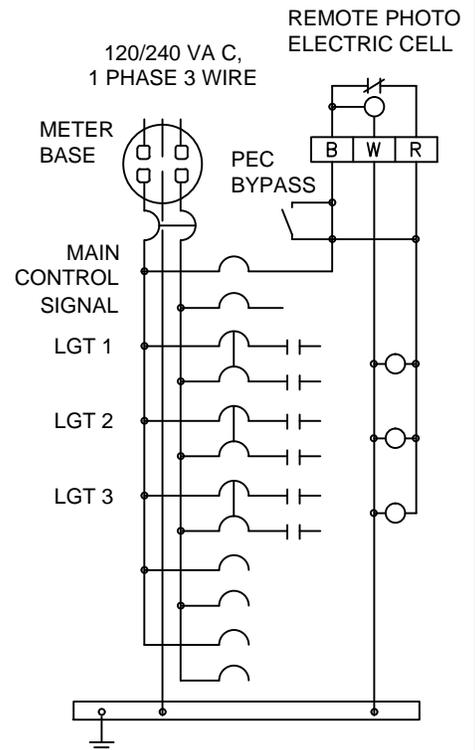
CABINET: NEMA 3R PADMOUNT, 1/8" ALUMINUM TYPE 5052-H32 CONSTRUCTION  
 2 SCREENED AND GASKETED VENTS  
 HINGED DEADFRONT WITH 1/4 TURN FASTENERS ON DISTRIBUTION SECTION  
 DOORS: HEAVY DUTY CONCEALED HINGES (LIFT-OFF TYPE)  
 STAINLESS STEEL VAULT HANDLES  
 PADLOCKABLE ON METER DOOR, INTEGRAL "BEST LOCK" ON DISTRIBUTION DOOR  
 POLISHED WIRE GLASS WINDOW IN METER DOOR  
 CLOSED CELL NEOPRENE GASKET, CARD HOLDER  
 FINISH: OUTSIDE FINISH SHALL BE ANSI 61 GRAY POLYESTER POWDERCOAT OVER ZINC PRIME. INSIDE FINISH SHALL BE WHITE.



UL LISTED PER STANDARD #508A  
 SUITABLE FOR USE AS SERVICE  
 ENTRANCE EQUIPMENT

CITY OF REDMOND  
 SERVICE CABINET SERIES 47995-R4  
 10-7-14 M.E. BELL  
 SKYLINE ELECTRIC AND MFG COMPANY

**SERVICE CABINET**  
 NTS



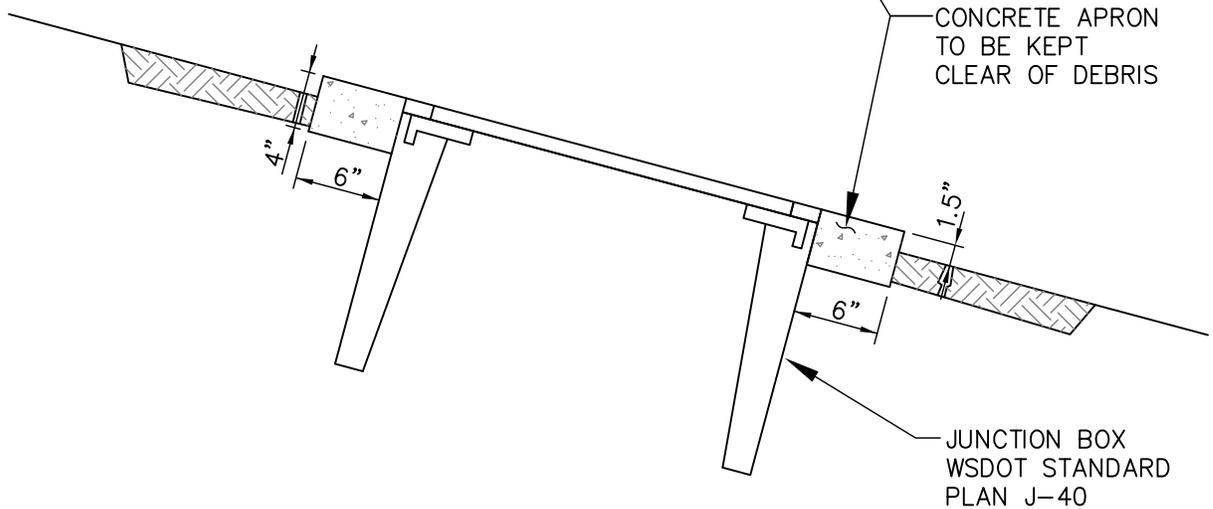
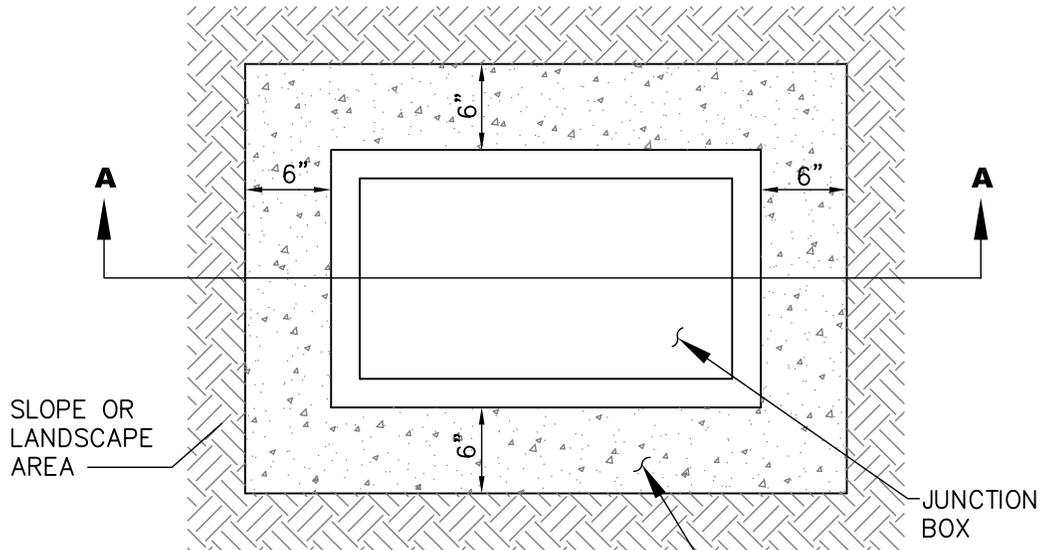
**WIRING SCHEMATIC**  
 NTS

*Rob Crittenden*  
 APPROVED BY: ROB CRITTENDEN  
 TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER  
 REVISION DATE: JULY 01, 2015



STANDARD DETAILS  
**SERVICE CABINET**  
 FILE NAME: SD461.DWG    DETAIL NUMBER: 461

# JUNCTION BOX ON SLOPE OR LANDSCAPE AREAS

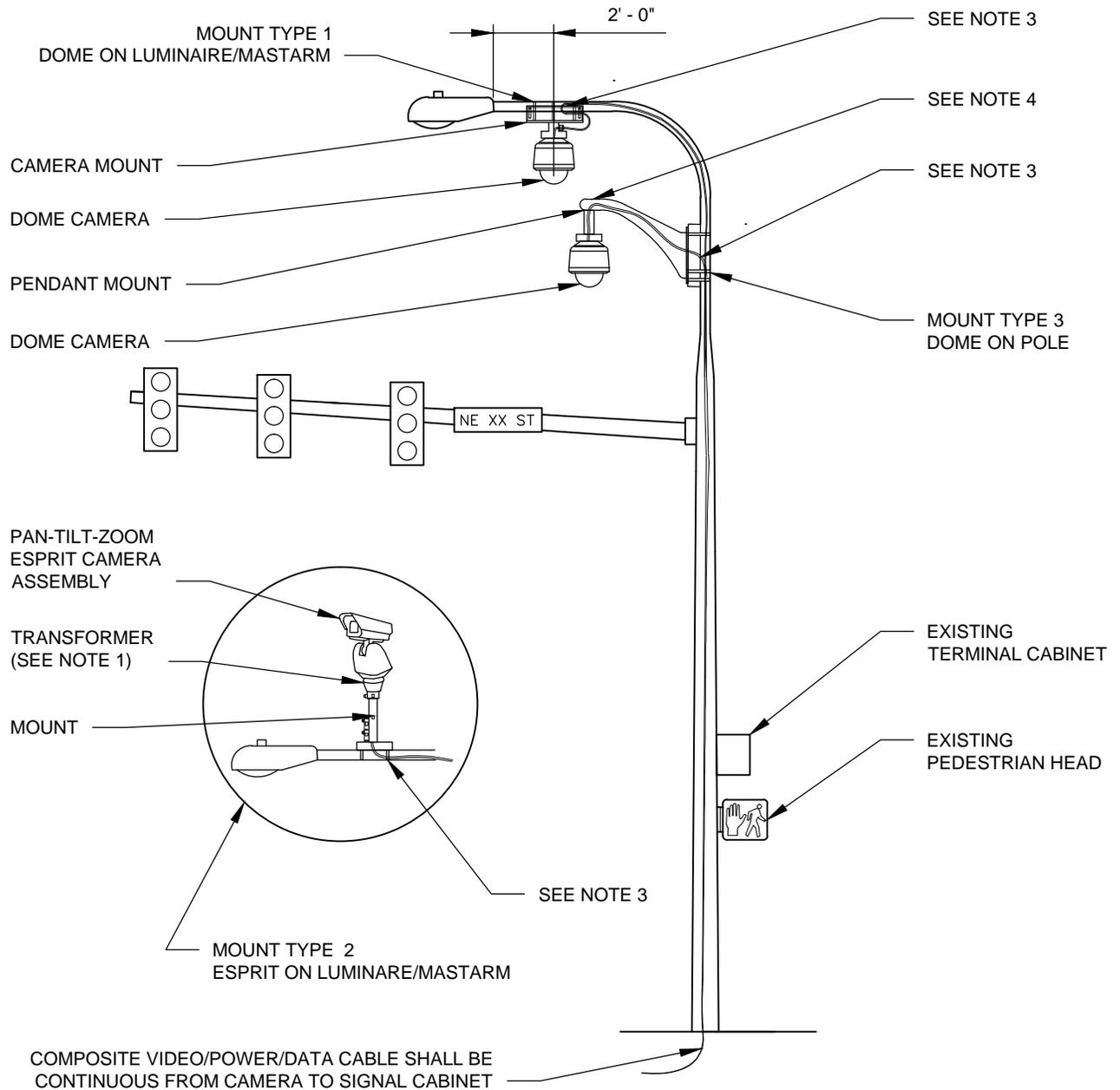


FOR NEW CONSTRUCTION AND MODIFICATION OF EXISTING J-BOX

*Rob Crittenden*  
APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER  
REVISION DATE: JULY 01, 2014



STANDARD DETAILS  
**JUNCTION BOX ON SLOPE OR  
SOFT AREAS**  
FILE NAME: SD464.DWG    DETAIL NUMBER: **464**



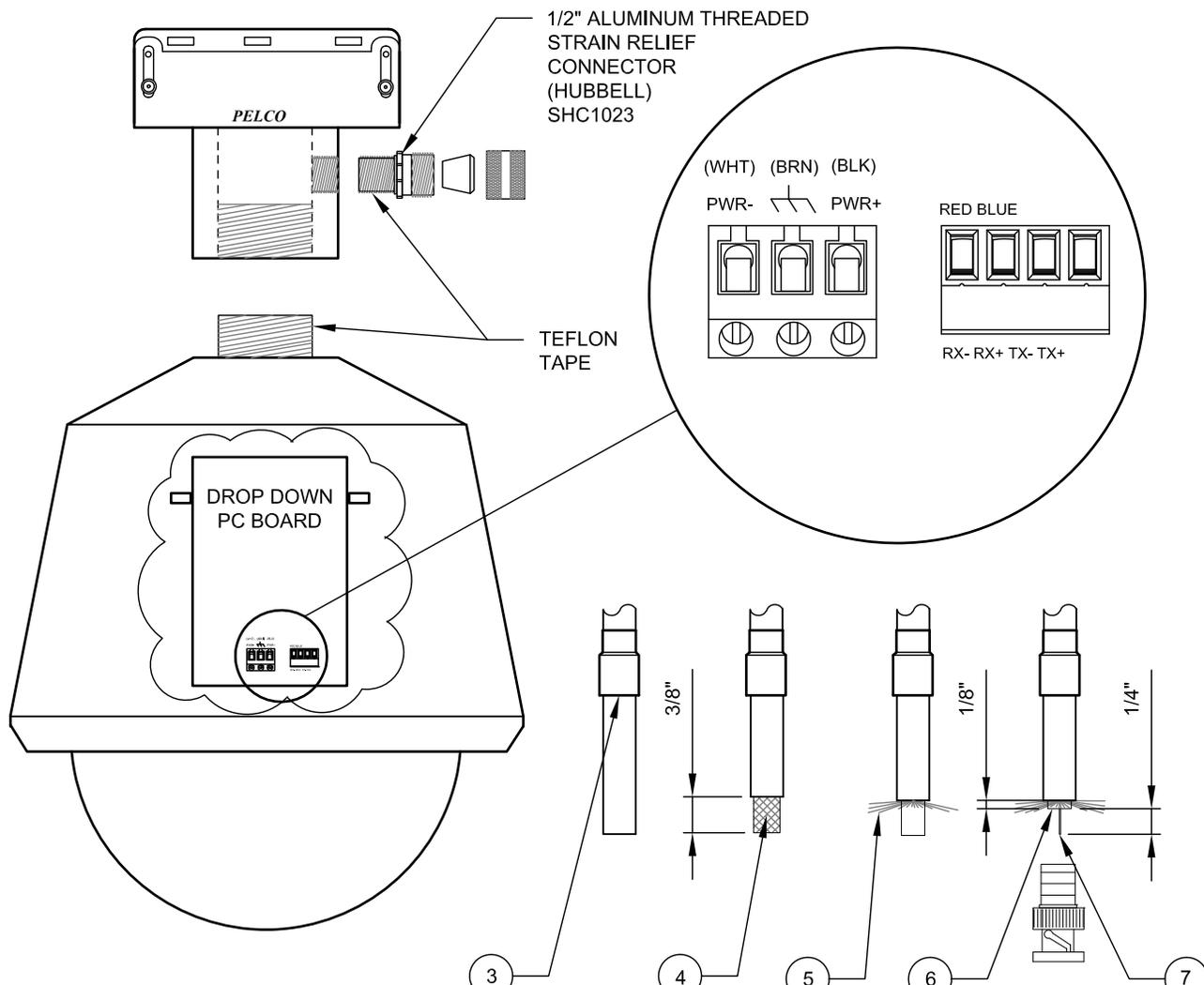
NOTES:

1. THE CONTRACTOR SHALL CONNECT THE POWER CONDUCTORS OF THE COMPOSITE CABLE TO THE TRANSFORMER IN THE TRANSFORMER BOX OF THE ESPRIT CAMERA. POWER CONDUCTORS ARE PROVIDED FROM THE TRANSFORMER TO THE CAMERA. THE TRANSFORMER FOR THE DOME CAMERAS WILL BE LOCATED IN THE CONTROLLER CABINET.
2. ATTACH POLE-MOUNTED CAMERAS DIRECTLY BELOW THE BEND IN THE LUMINAIRE ARM OR BELOW THE LUMINAIRE ARM ATTACHMENT POINT IF ONE EXISTS UNLESS OTHERWISE SPECIFIED ON THE PLANS.
3. DRILL 1.0-INCH TAPPED HOLE IN POLE, MAST ARM, OR LUMINAIRE ARM FOR CAMERA CABLE ENTRANCE. INSERT 1.0-INCH ALUMINUM THREADED STRAIN RELIEF TO PROTECT CABLE ENTERING POLE AND PROVIDE WATERTIGHT FIT.
4. THE WIRE ACCESS PANEL ON THE TOP OF THE PENDANT ARM SHALL BE PACKED WITH DUCT SEAL TO BLOCK MOISTURE PENETRATION.

*Rob Crittenden*  
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 TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER  
 REVISION DATE: JULY 01, 2015



STANDARD DETAILS  
 CCTV CAMERA MOUNTING  
 FILE NAME: SD465.DWG    DETAIL NUMBER: 465



**CCTV VIDEO CABLE CONNECTION**

NTS

**BNC CONNECTOR #3071-75**

NTS

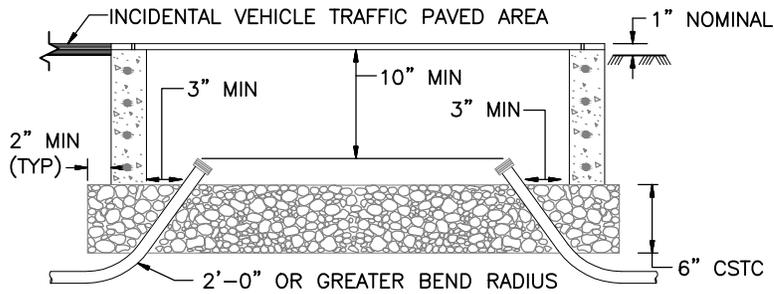
NOTES:

1. INSERT CABLE THROUGH CONNECTOR AND CAMERA HOUSING. LEAVE SUFFICIENT SLACK FOR THE PTZ CONNECTIONS.
2. USE PROPER TOOLS TO STRIP AND CRIMP RG59/U CABLE AND BNC CONNECTOR.
3. INSERT BNC BOOT ON CABLE FIRST.
4. STRIP INSULATION BACK 3/8" EXPOSING COPPER SHIELD.
5. FOLD BACK COPPER SHIELD.
6. CUT OFF DIELECTRIC LEAVING 1/8" FROM COPPER SHIELD.
7. EXPOSE MINIMUM 1/4" OF SOLID COPPER CONDUCTOR.
8. INSERT COPPER WIRE INTO BNC CONNECTOR. FOLD COPPER SHIELD OVER BNC CONNECTOR. MAKE SURE NO COPPER SHIELD CONTACTS CENTER CONDUCTOR.
9. SLIDE BNC BOOT OVER COPPER SHIELD AND BNC CONNECTOR. CRIMP BOOT ONTO THE CONNECTOR AND CABLE.

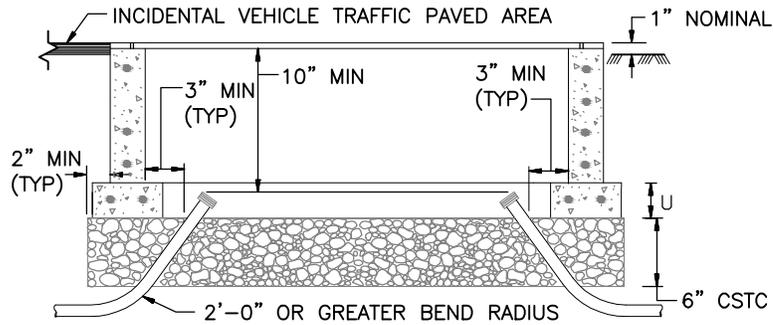
*Rob Crittenden*  
 APPROVED BY: ROB CRITTENDEN  
 TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER  
 REVISION DATE: JULY 01, 2014



STANDARD DETAILS  
**CCTV VIDEO CABLE CONNECTION**  
 FILE NAME: SD465A.DWG | DETAIL NUMBER: **465A**

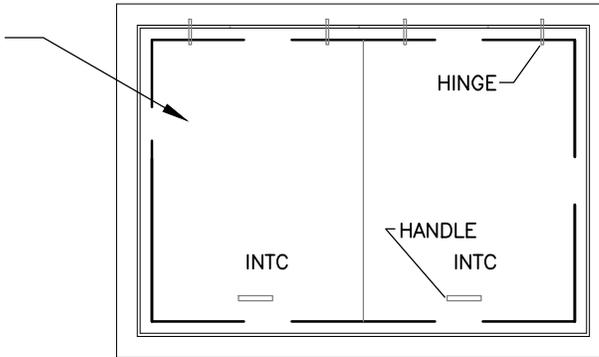


**TYPE 1 OR 2 JUNCTION BOX INSTALLATION DETAIL**  
NTS

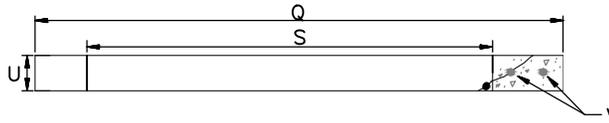


**TYPE 7 JUNCTION BOX INSTALLATION DETAIL**  
NTS

NON SKID COATING  
IKG-MEBAC1 HARSCO  
INDUSTRIAL 9-29.2



**DUAL LID DETAIL - TYPE 7**  
NTS



**FOUNDATION ELEVATION - TYPE 7**  
NTS

*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



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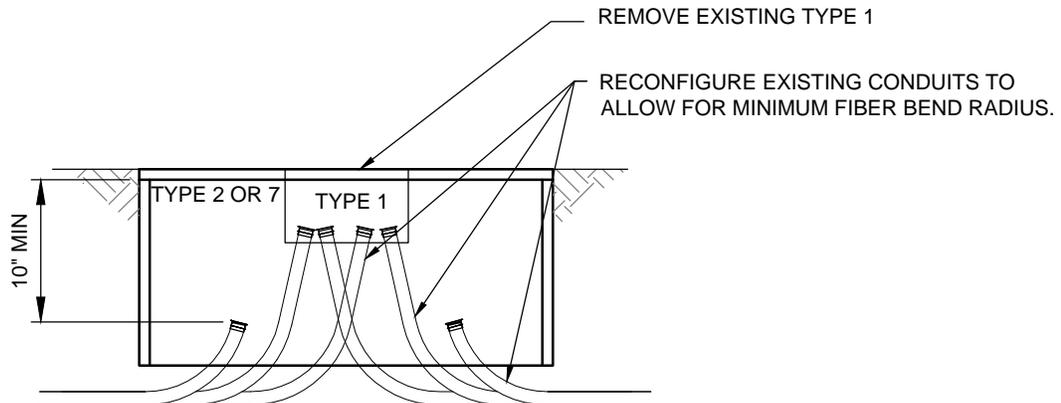
STANDARD DETAILS

JUNCTION BOX DETAILS 1 OF 2

FILE NAME: SD467A.DWG    DETAIL NUMBER: 467A

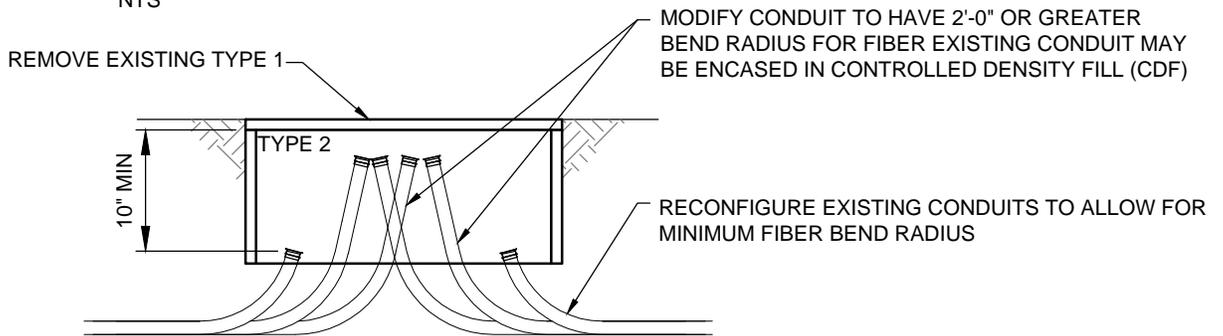
NOTES:

1. ALL BOX DIMENSIONS ARE NOMINAL. EXACT CONFIGURATIONS VARY AMONG DIFFERENT MANUFACTURERS.
2. THE NOTED LID THICKNESS ARE OVERALL MINIMUMS. THE DIAMOND PATTERN FOR TYPE 1 AND 2 BOXES SHALL BE 28% MINIMUM OF OVERALL THICKNESS. THE DIAMOND PATTERN FOR TYPE 7 BOXES SHALL HAVE A MINIMUM THICKNESS OF  $\frac{3}{32}$ "
3. LID SUPPORT MEMBERS SHALL BE  $\frac{3}{16}$ " MIN. THICK STEEL C, L OR T SHAPE WELDED TO THE FRAME.
4. WHEN SPECIFIED IN THE CONTRACT, TYPE 2 AND TYPE 7 BOXES SHALL BE PROVIDED WITH 12" DEEP EXTENSION BOXES.
5. A  $\frac{1}{4}$ " NC X  $\frac{3}{4}$ " STAINLESS STEEL GROUND STUD WITH S.S. NUT SHALL BE WELDED TO THE BOTTOM OF THE LID.
6. SEE THE STANDARD SPECIFICATIONS FOR ALTERNATE USE OF REINFORCEMENT.
7. WHEN REPLACING AN EXISTING JUNCTION BOX, THE END OF EACH EXISTING CONDUIT FOR NEW FIBER OPTIC CABLE INSTALLATION SHALL BE CUT BACK AND REPLACED WITH A 45 DEGREE FACTORY BEND WITH A MINIMUM 24" BENDING RADIUS. IF NECESSARY, EXISTING CABLES SHALL BE DISCONNECTED, REMOVED, LABELED, REINSTALLED AND RECONNECTED BY CONTRACTOR. COR SHALL BE NOTIFIED AT LEAST 3 WORKING DAYS PRIOR TO ANY WORK IN A COR CABINET.
8. JUNCTION BOX PLACED IN STREET SHALL BE H-35 TRAFFIC BEARING OR HIGHER.
9. THE TYPE 7 LIDS SHALL BE GALVANIZED AND BOTH LIDS SHALL BE BONDED. HINGES SHALL ALLOW THE LID TO OPEN 180°.
10. ALL CONDUIT SHALL HAVE BELL ENDS.
11. JUNCTION BOXES WITHIN SIDEWALK SHALL HAVE NON-SKID LIDS. IKG-MEBAC1 HARSCO INDUSTRIAL.
12. EXISTING JUNCTION BOXES WITHIN SIDEWALK SHALL BE RETROFITTED WITH NON-SKID LIDS.
13. EXISTING JUNCTION BOXES THAT ARE MODIFIED INCLUDING NEW CONDUCTORS PULLED SHALL MEET CURRENT GROUNDING AND BONDING REQUIREMENTS.



**TYPE 1 CONVERSION TO TYPE 2 OR 7**

NTS



**TYPE 2 RECONFIGURATION**

NTS

*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



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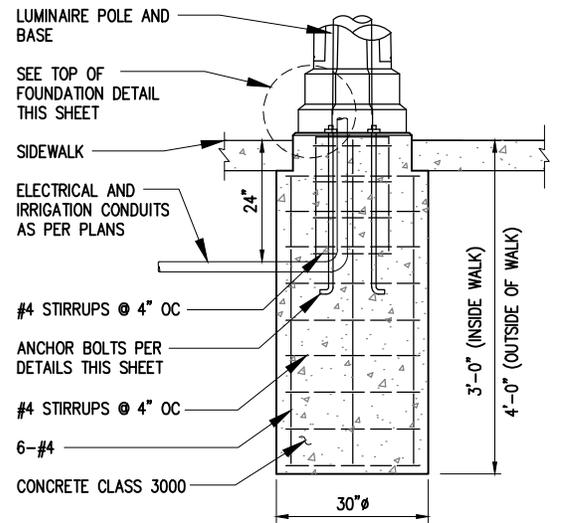
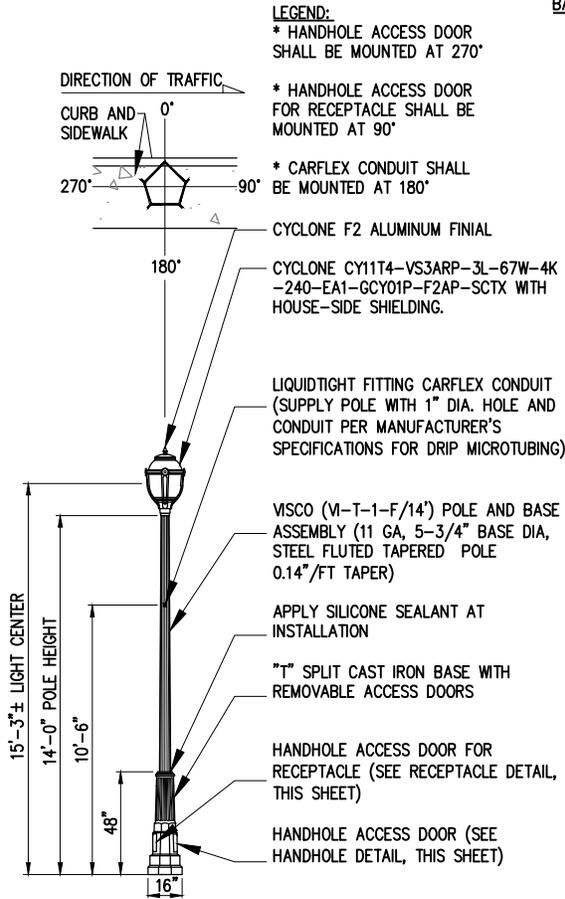
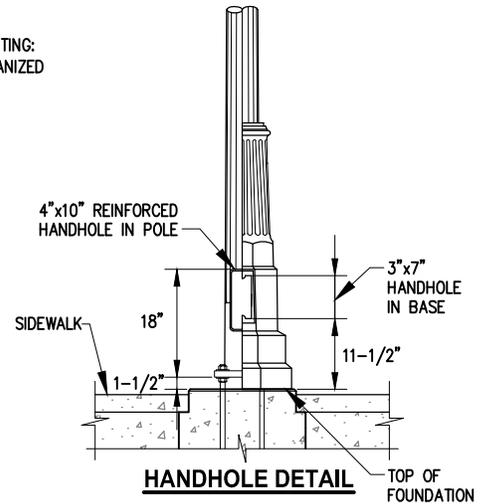
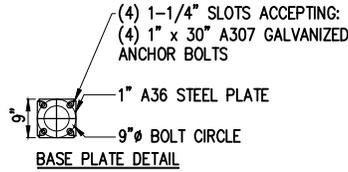
STANDARD DETAILS

JUNCTION BOX DETAILS 2 OF 2

FILE NAME: 467B.DWG

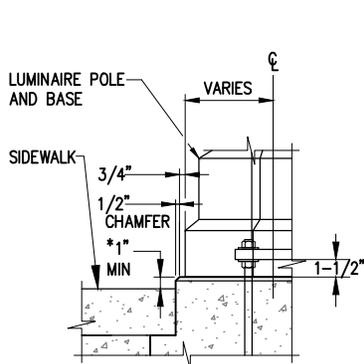
DETAIL NUMBER: 467B

**PAINT SPECIFICATIONS:** ALL CAST IRON AND STEEL LIGHT POLE PARTS ARE TO BE FACTORY POLYESTER POWDER COATED WITH PEKING BLUE, ENDURASHIELD TREMEC #F073B2705A - 8556A, OR APPROVED EQUAL.



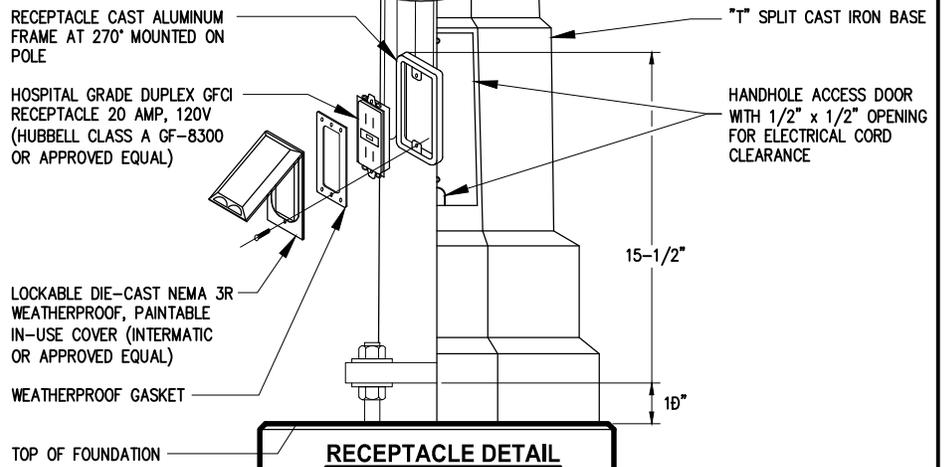
**PEDESTRIAN LUMINAIRE AND POLE**

**LUMINAIRE POLE FOUNDATION DETAIL**



\* TOP OF FOUNDATION TO BE LEVEL, 1" MIN HEIGHT ABOVE TOP OF SIDEWALK

**TOP OF FOUNDATION DETAIL**



*Rob Crittenden*  
APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

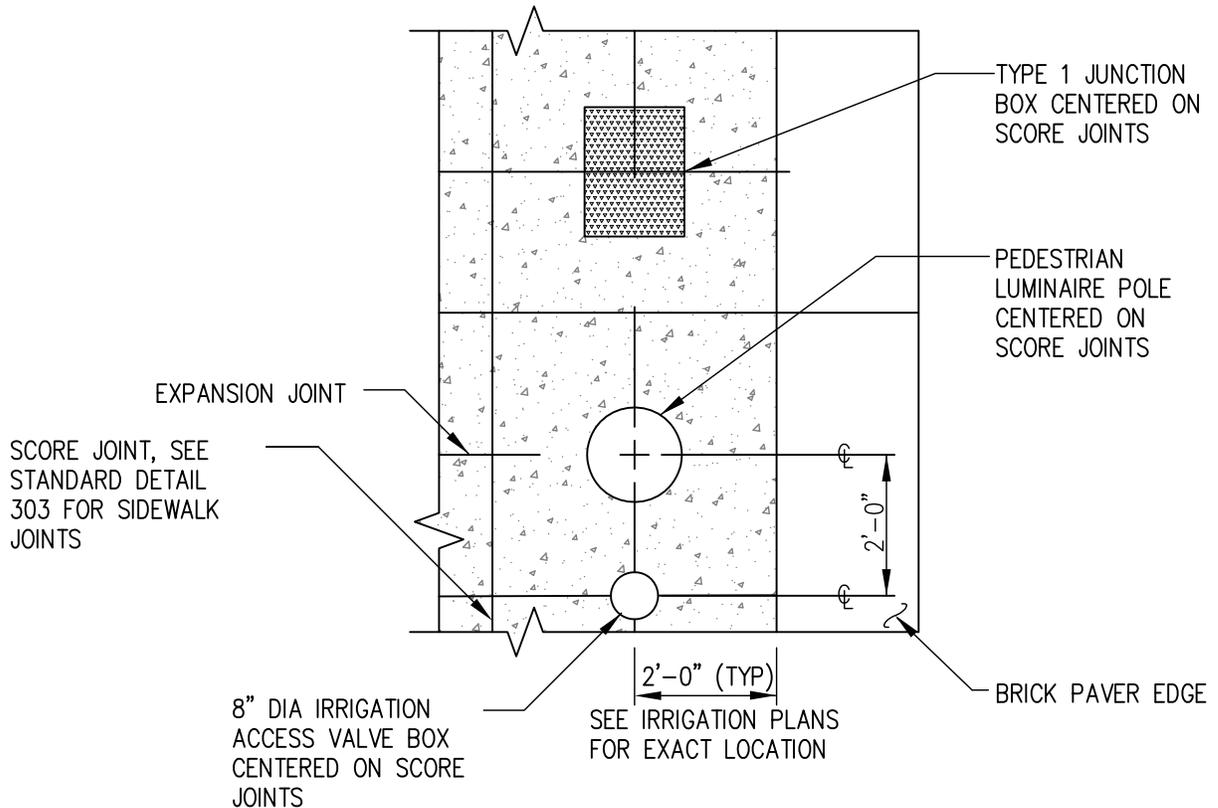
REVISION DATE: JULY 01, 2015



STANDARD DETAILS  
**PEDESTRIAN LUMINAIRE AND POLE**

FILE NAME: SD471.DWG

DETAIL NUMBER: 471



**PEDESTRIAN LUMINAIRE LOCATION AND PAVEMENT SCORING**

NTS

*Donald Cairns*

APPROVED BY: DONALD CAIRNS  
 TRANSPORTATION PLANNING AND ENGINEERING MANAGER

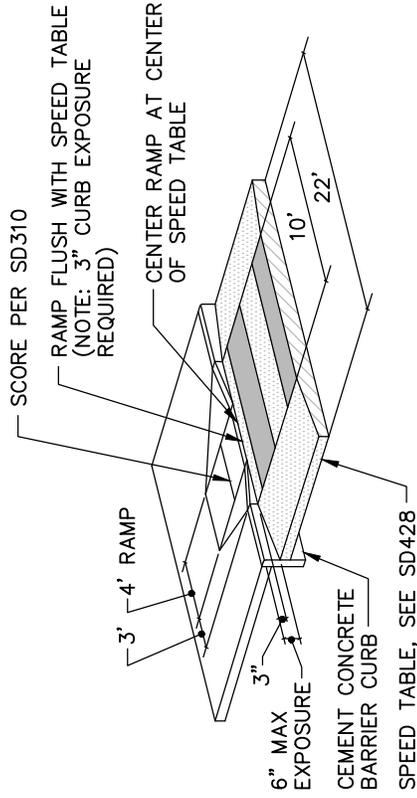
REVISION DATE: JULY 01, 2014



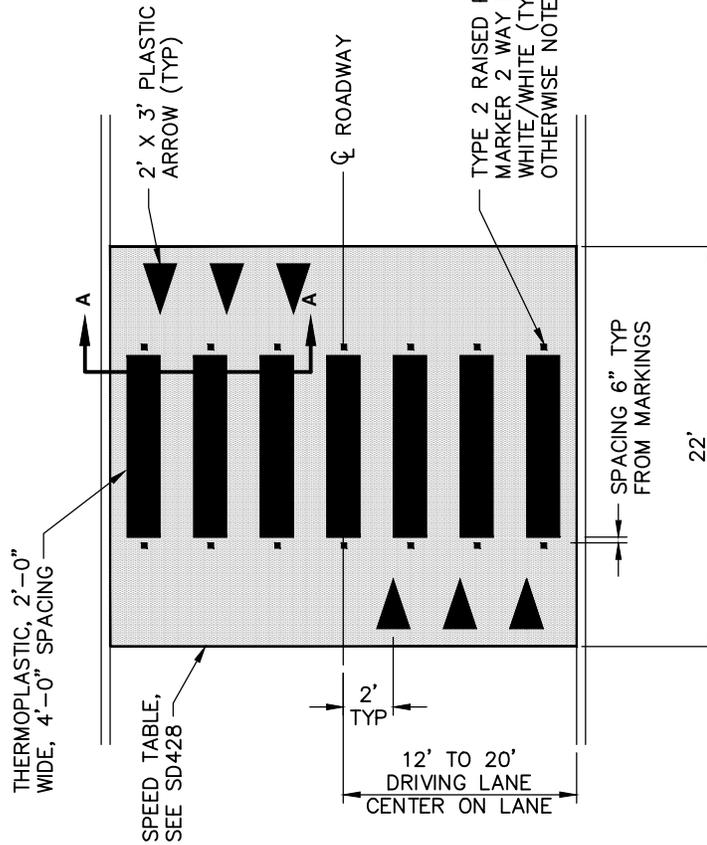
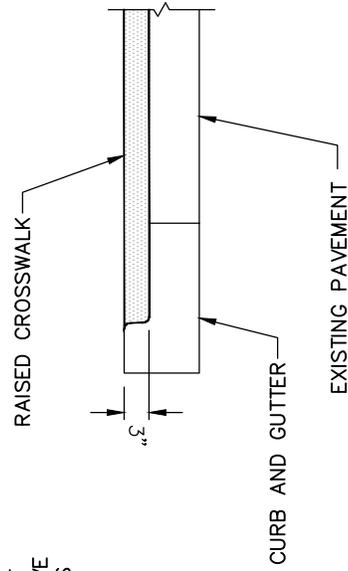
STANDARD DETAILS

**PEDESTRIAN LUMINAIRE  
 LOCATION AND PAVEMENT  
 SCORING**

FILE NAME: SD474.DWG | DETAIL NUMBER: **474**



**TRANSITION FROM TYPE 1 CURB RAMP  
TO RAISED CROSSWALK**  
NTS



**MARKING DETAIL**  
NTS

**SECTION A-A**  
NTS

*Rob Crittenden*

APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



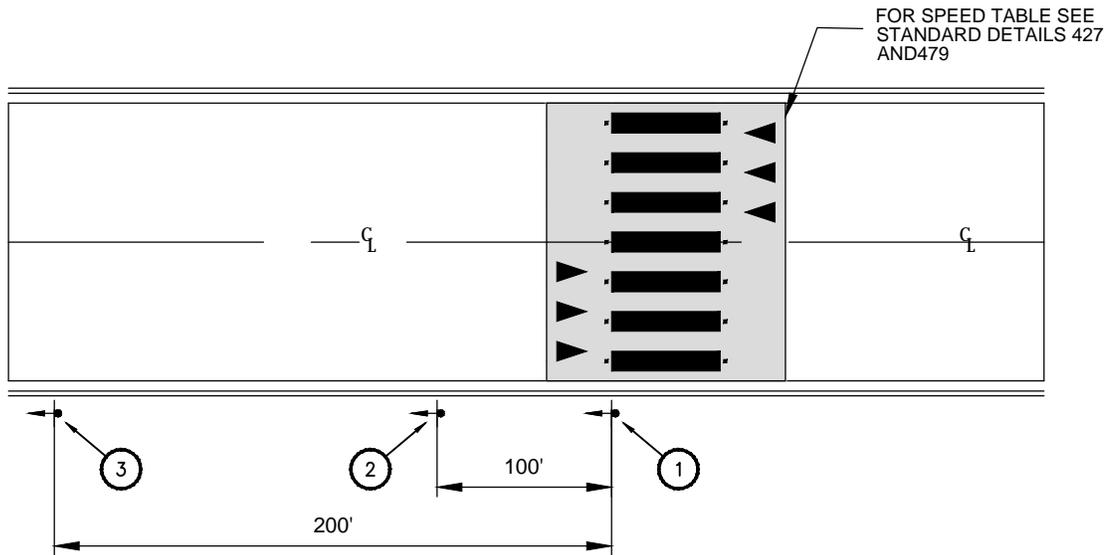
City of Redmond  
WASHINGTON

STANDARD DETAILS

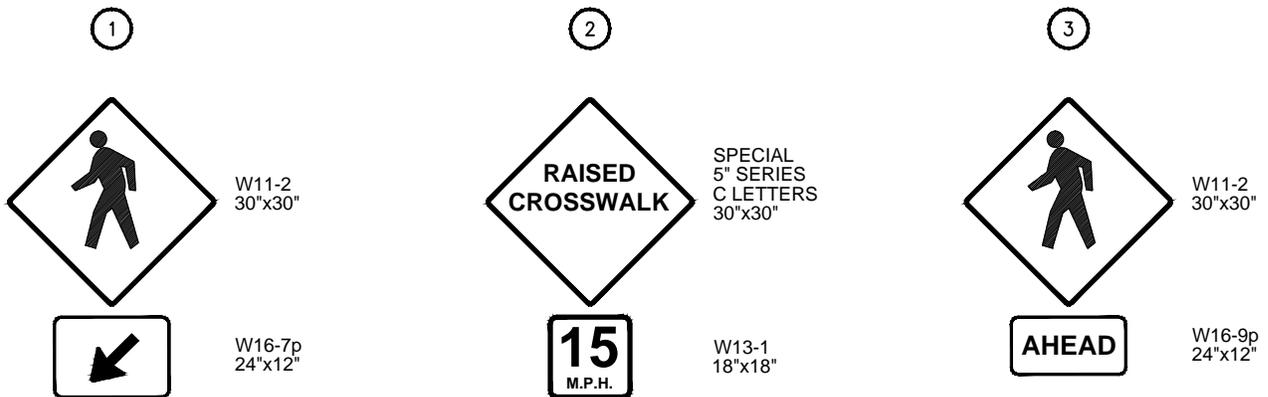
RAISED CROSSWALK

FILE NAME: SD479.DWG

DETAIL NUMBER: 479



**SIGNING PLACEMENT**  
NTS



**SIGNING DETAIL**  
NTS

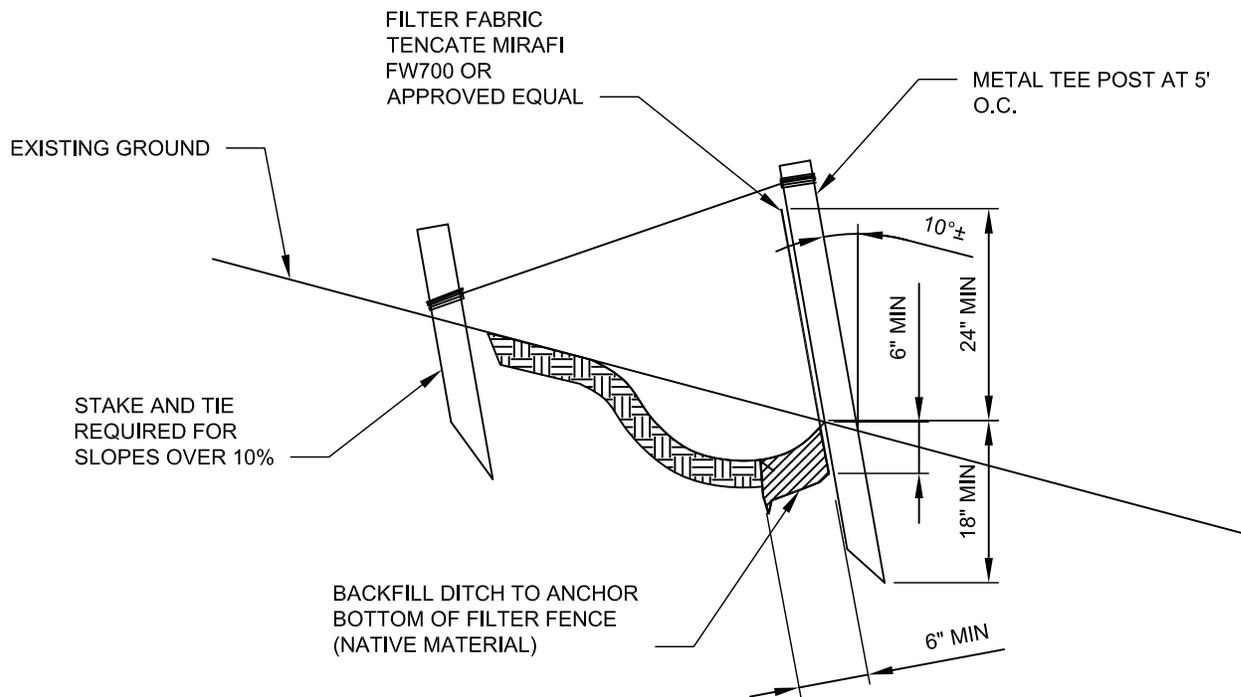
NOTES:

1. SIGNS HAVE FLUORESCENT YELLOW GREEN BACKGROUND WITH BLACK LEGEND AND BORDER.  
SIGN MATERIAL: 3M DIAMOND GRADE DG<sup>3</sup>
2. DIAGONAL ARROW POINTING DOWNWARD TOWARD CROSSWALK.
3. SIGN ①: INSTALL 2 SIGNS BACK TO BACK ON SAME POST.
4. SIGNS AND MARKINGS TYPICAL BOTH TYPICAL DIRECTIONS.

*Rob Crittenden*  
APPROVED BY: ROB CRITTENDEN  
TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER  
REVISION DATE: JULY 01, 2015



STANDARD DETAILS  
**RAISED CROSSWALK SIGNING**  
FILE NAME: SD481.DWG    DETAIL NUMBER: 481



**FILTER FABRIC FENCE**  
NTS

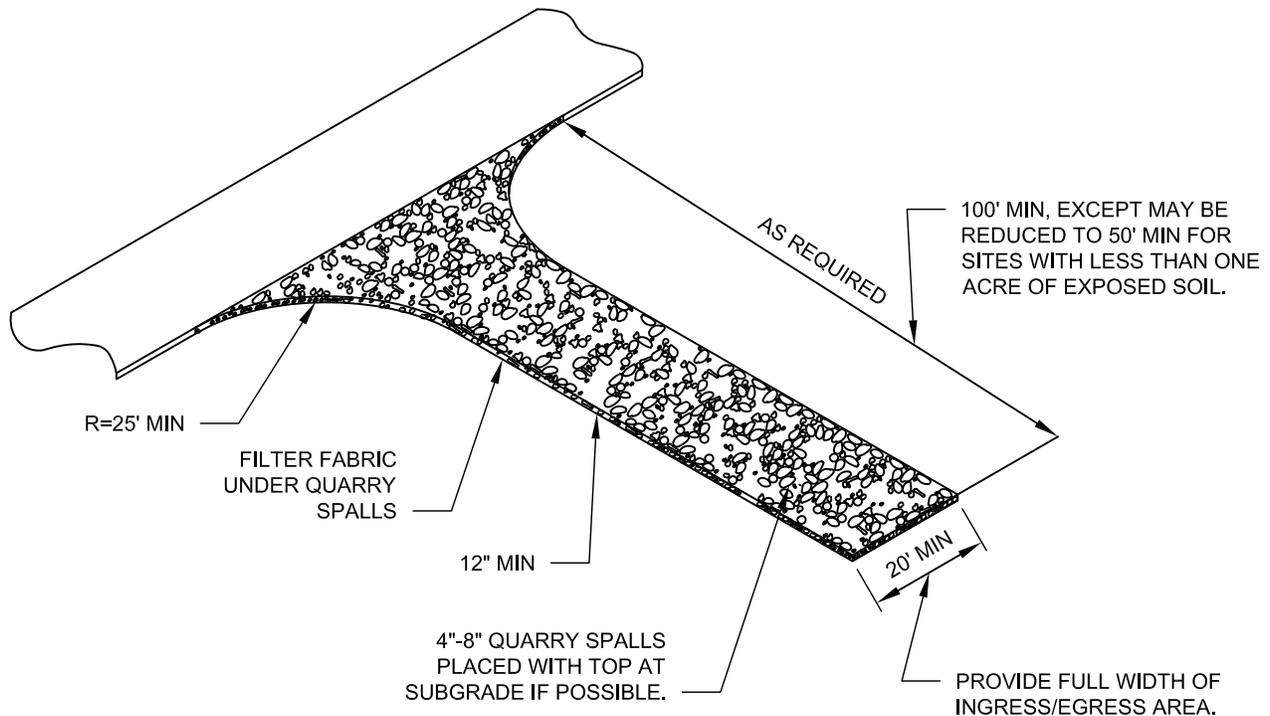
  
 APPROVED BY: GARY M. SCHIMEK  
 NATURAL RESOURCES/STORMWATER ENGINEERING MANAGER  
 REVISION DATE: JULY 01, 2015



STANDARD DETAILS

**FILTER FABRIC FENCE**

FILE NAME: SD502.DWG    DETAIL NUMBER: **502**



## STABILIZED CONSTRUCTION ENTRANCE

NTS

### NOTES:

1. LENGTH - AS REQUIRED BUT NOT LESS THAN 50' (EXCEPT ON SINGLE RESIDENCE LOT WHERE A 30' MINIMUM LENGTH WOULD APPLY).
2. THICKNESS - NOT LESS THAN 12"
3. WIDTH - 20' MINIMUM BUT NOT LESS THAN THE FULL WIDTH AT POINTS WHERE INGRESS OR EGRESS OCCURS.
4. "FILTER FABRIC SHALL BE WOVEN STABILIZATION FABRIC WITH A MINIMUM PERMITIVITY OF 0.9(SEC-1). PLACE FILTER FABRIC OVER THE ENTIRE AREA PRIOR TO PLACING OF STONE. FILTER FABRIC IS NOT REQUIRED FOR A SINGLE FAMILY RESIDENCE LOT"
5. MAINTENANCE - THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO PUBLIC RIGHT-OF-WAY. THIS MAY REQUIRE PERIODIC TIP DRESSING WITH ADDITIONAL STONE AS CONDITIONS DEMAND AND REPAIR AND/OR CLEAN OUT OF ANY MEASURES USED TO TRAP SEDIMENT. ALL SEDIMENT SPILLED, DROPPED, WASHED OR TRACKED ONTO PUBLIC RIGHT-OF-WAY MUST BE REMOVED IMMEDIATELY.
6. WASHING - WHEELS SHALL BE CLEANED TO REMOVE SEDIMENT PRIOR TO ENTRANCE ONTO PUBLIC RIGHT-OF-WAY. WHEN WASHING IS REQUIRED, IT SHALL BE DONE ON AN AREA STABILIZED WITH STONE AND WHICH DRAINS INTO AN APPROVED SEDIMENT TRAPPING DEVICE.
7. PERIODIC INSPECTION AND NEEDED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN.

*Gary M. Schimek*

APPROVED BY: GARY M. SCHIMEK  
NATURAL RESOURCES/STORMWATER ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



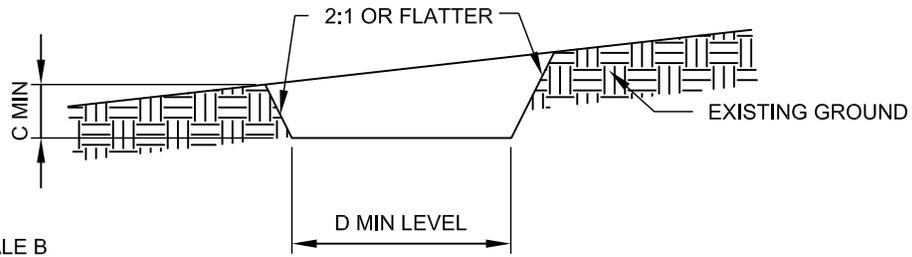
City of Redmond  
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STANDARD DETAILS

## **STABILIZED CONSTRUCTION ENTRANCE**

FILE NAME: SD503.DWG

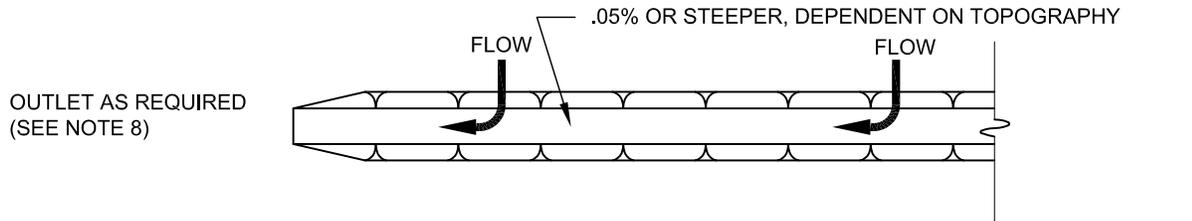
DETAIL NUMBER: **503**



|   | SWALE A | SWALE B |
|---|---------|---------|
| C | 1'      | 1'      |
| D | 4'      | 6'      |

**CROSS SECTION**

NTS



**PLAN VIEW**

NTS

STANDARD SYMBOL

A-2 / B-3

**NOTES:**

1. ALL TEMPORARY SWALES SHALL HAVE UNINTERRUPTED POSITIVE GRADE TO AN OUTLET.
2. DIVERTED RUNOFF FROM A DISTURBED AREA SHALL BE CONVEYED TO A SEDIMENT TRAPPING DEVICE.
3. DIVERTED RUNOFF FROM AN UNDISTURBED AREA SHALL OUTLET DIRECTLY INTO AN UNDISTURBED STABILIZED AREA AT NON-EROSIVE VELOCITY.
4. ALL TREES, BRUSH, STUMPS, OBSTRUCTIONS, AND OTHER OBJECTIONABLE MATERIAL SHALL BE REMOVED AND DISPOSED OF SO AS NOT TO INTERFERE WITH THE PROPER FUNCTIONING OF THE SWALE.
5. THE SWALE SHALL BE EXCAVATED OR SHAPED TO LINE, GRADE AND CROSS SECTION AS REQUIRED TO MEET THE CRITERIA SPECIFIED HEREIN AND BE FREE OF BANK PROJECTIONS OR OTHER IRREGULARITIES WHICH WILL IMPEDE NORMAL FLOW.
6. FILLS SHALL BE COMPACTED SUFFICIENT TO PREVENT EROSION ALL EARTH REMOVED AND NOT NEEDED ON CONSTRUCTION SHALL BE PLACED SO THAT IT WILL NOT INTERFERE WITH THE SWALE.
7. STABILIZATION SHALL BE AS PER THE CHART BELOW:

**FLOW CHANNEL STABILIZATION**

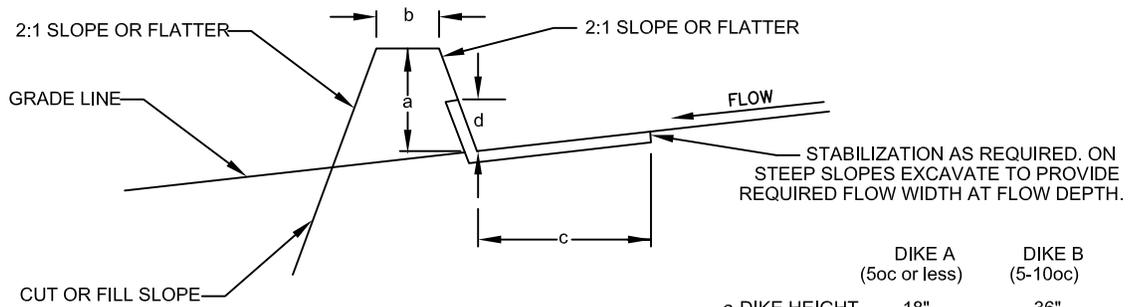
| TYPE OF TREATMENT | CHANNEL GRADE | FLOW CHANNEL STABILIZATION        |   |
|-------------------|---------------|-----------------------------------|---|
|                   |               | A (5 AC OR LESS)                  | B (5 AC - 10 AC)                                    |
| 1                 | 0.5% - 3.0%   | SEED AND STRAW MULCH              | SEED AND STRAW MULCH                                |
| 2                 | 3.1% - 5.0%   | SEED AND STRAW MULCH              | SEED USING JUTE OR EXCELSIOR                        |
| 3                 | 3.1% - 5.0%   | SEED USING JUTE OR EXCELSIOR; SOD | LINED RIP-RAP 4"-8"<br>RECYCLED CONCRETE EQUIVALENT |
| 4                 | 3.1% - 5.0%   | LINED 4"-8" RIP-RAP               | ENGINEERED DESIGN                                   |

9. PERIODIC INSPECTION AND REQUIRED MAINTENANCE MUST BE PROVIDED AFTER EACH RAIN EVENT.

*Gary M. Schimek*  
 APPROVED BY: GARY M. SCHIMEK  
 NATURAL RESOURCES/STORMWATER ENGINEERING MANAGER  
 REVISION DATE: JULY 01, 2015

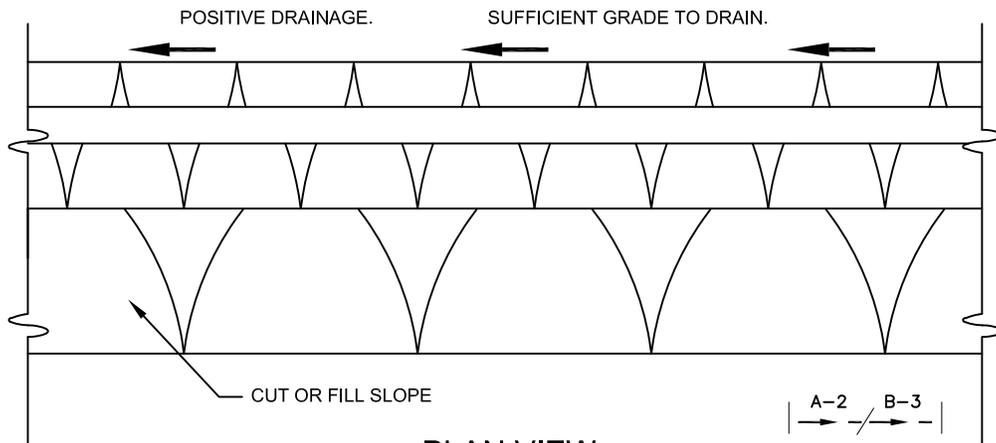


STANDARD DETAILS  
**TEMPORARY SWALE**  
 FILE NAME: SD504.DWG    DETAIL NUMBER: **504**



**CROSS SECTION**  
NTS

|               | DIKE A<br>(50c or less) | DIKE B<br>(5-100c) |
|---------------|-------------------------|--------------------|
| a-DIKE HEIGHT | 18"                     | 36"                |
| b-DIKE WIDTH  | 24"                     | 36"                |
| c-FLOW WIDTH  | 4'                      | 6'                 |
| d-FLOW DEPTH  | 8"                      | 15"                |



**PLAN VIEW**  
NTS

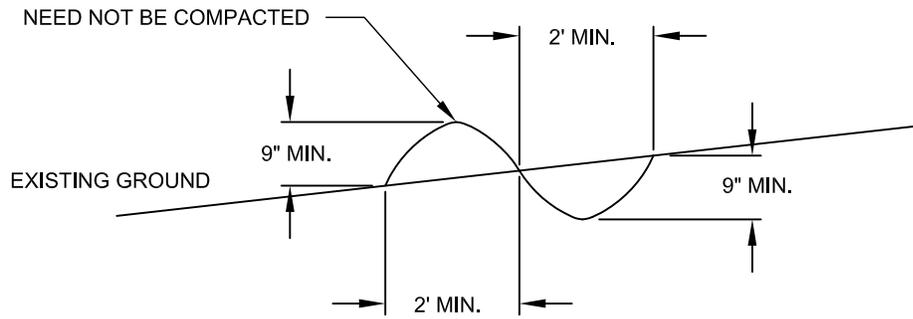
**NOTES:**

1. ALL DIKES SHALL BE COMPACTED SUFFICIENT TO PREVENT EROSION.
2. ALL DIKES SHALL HAVE POSITIVE DRAINAGE TO AN OUTLET.
3. TOP WIDTH MAY BE WIDER AND SIDE SLOPES MAY BE FLATTER IF DESIRED TO FACILITATE CROSSING BY CONSTRUCTION TRAFFIC.
4. FIELD LOCATION SHOULD BE ADJUSTED AS NEEDED TO UTILIZE A STABILIZED SAFE OUTLET.
5. EARTH DIKES SHALL HAVE AN OUTLET THAT FUNCTIONS WITH A MINIMUM OF EROSION. RUNOFF SHALL BE CONVEYED TO A SEDIMENT TRAPPING DEVICE SUCH AS A SEDIMENT TRAP OR SEDIMENT BASIN WHERE EITHER THE DIKE CHANNEL OR THE DRAINAGE AREA ABOVE THE DIKE ARE NOT ADEQUATELY STABILIZED.
6. STABILIZATION SHALL BE: (A) IN ACCORDANCE WITH STANDARD SPECIFICATIONS FOR SEED AND STRAW MULCH OR STRAW MULCH IF NOT IN SEEDING SEASON. (B) FLOW CHANNEL AS PER THE CHART BELOW.

| TYPE OF TREATMENT | CHANNEL GRADE | DIKE A                           | DIKE B                                       |
|-------------------|---------------|----------------------------------|--|
| 1                 | 0.5-3.0%      | SEED AND STRAW MULCH             | SEED AND STRAW MULCH                         |
| 2                 | 3.1-5.0%      | SEED AND STRAW MULCH             | SEED USING JUTE, OR EXCELSIOR; SOD; 2" STONE |
| 3                 | 5.1-8.0%      | SEED WITH JUTE, OR SOD; 2" STONE | LINED RIP-RAP 4-8"                           |
| 4                 | 8.1-20%       | LINED RIP-RAP 4-8"               | ENGINEERING DESIGN                           |

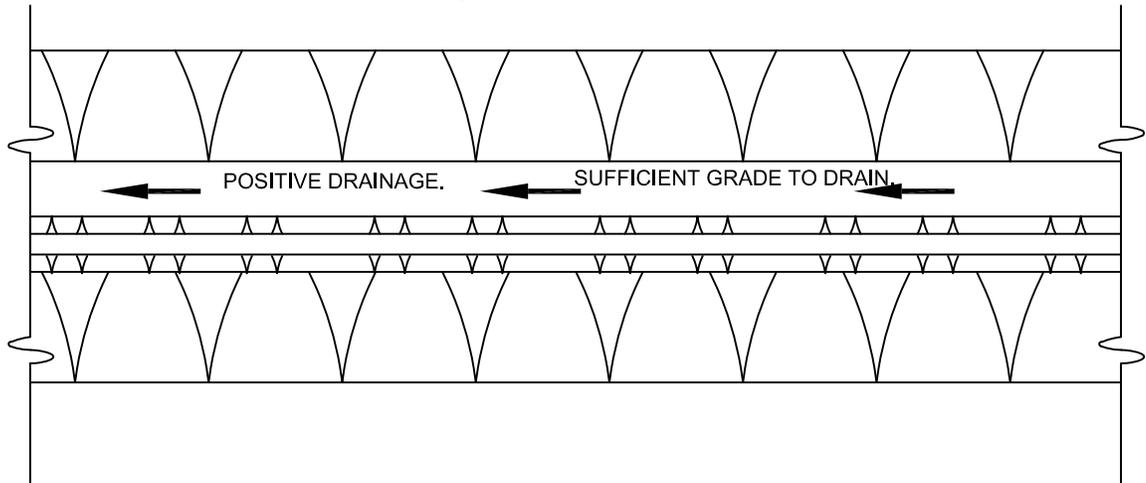
- A. STONE TO BE 2" STONE, OR RECYCLED CONCRETE EQUIVALENT, IN A LAYER AT LEAST 3" IN THICKNESS AND BE PRESSED INTO THE SOIL WITH EQUIPMENT.
  - B. RIP-RAP TO BE 4-8 INCHES IN A LAYER AT LEAST 8 INCHES AND PRESSED INTO THE SOIL.
  - C. APPROVED EQUIVALENTS CAN BE SUBSTITUTED FOR ANY OF THE ABOVE MATERIALS.
7. PERIODIC INSPECTION AND REQUIRED MAINTENANCE MUST BE PROVIDED AFTER EACH RAIN EVENT.

|  |                                       |   |
|--|---------------------------------------|---|
| <p>APPROVED BY: GARY M. SCHIMEK<br/>NATURAL RESOURCES/STORMWATER ENGINEERING MANAGER</p> | <p>City of Redmond<br/>WASHINGTON</p> | <p>STANDARD DETAILS</p> <p><b>EARTH DIKE</b></p> <p>FILE NAME: SD505.DWG    DETAIL NUMBER: <b>505</b></p> |
| <p>REVISION DATE: JULY 01, 2015</p>  |                                       |   |



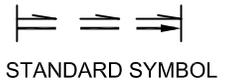
**CROSS SECTION**

NTS



**PLAN VIEW**

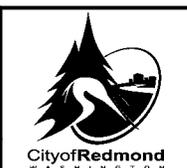
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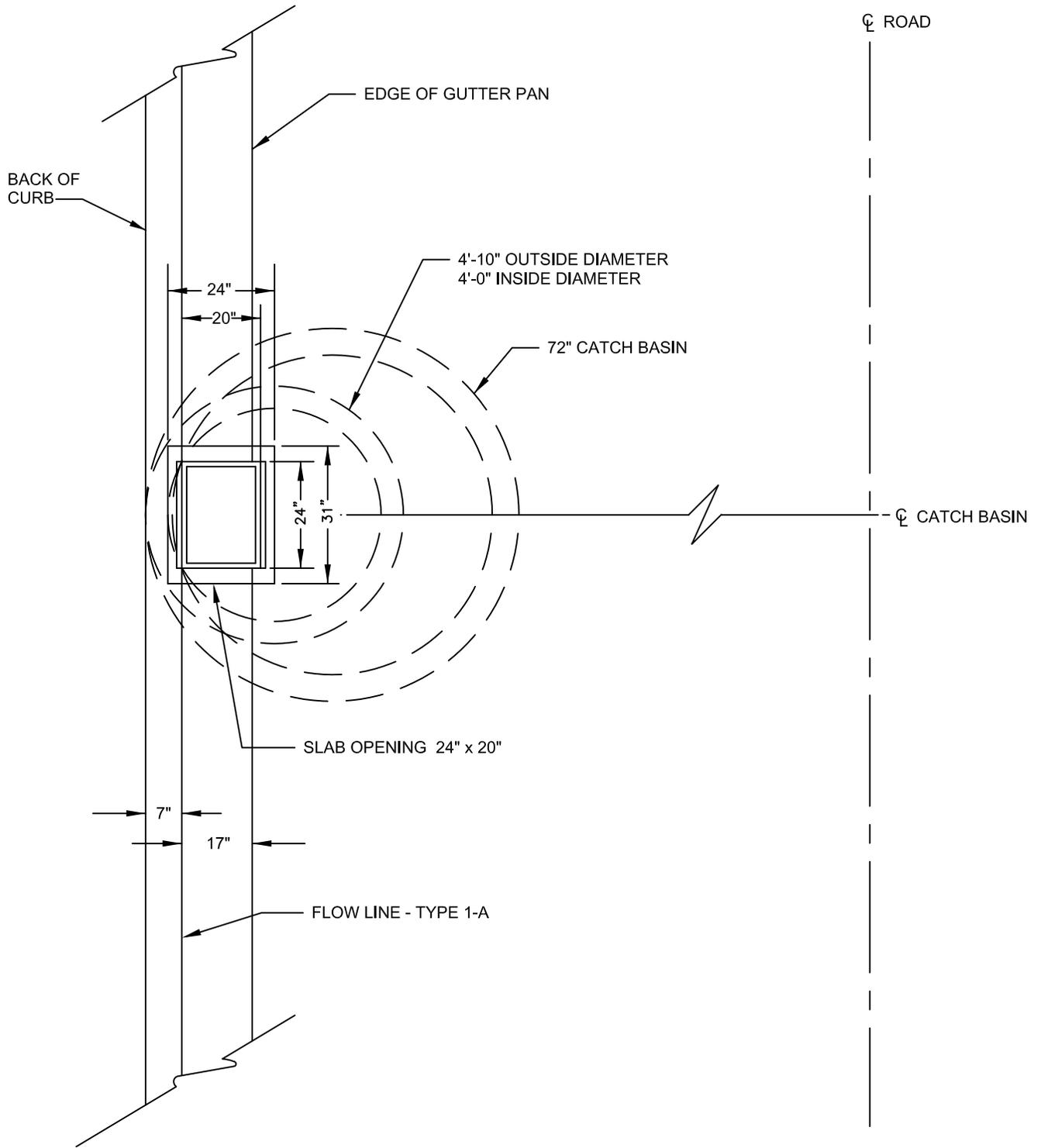
NOTES:

1. ALL PERIMETER DIKE/SWALE HAVE UNINTERRUPTED POSITIVE GRADE TO AN OUTLET.
2. DIVERTED RUNOFF FROM A DISTURBED AREA SHALL BE CONVEYED TO A SEDIMENT TRAPPING DEVICE.
3. DIVERTED RUNOFF FROM A UNDISTURBED AREA SHALL OUTLET DIRECTLY INTO AN UNDISTURBED STABILIZED AREA AT NON-EROSIVE VELOCITY.
4. THE SWALE SHALL BE EXCAVATED OR SHAPED TO LINE, GRADE, AND CROSS SECTION AS REQUIRED TO MEET THE CRITERIA SPECIFIED IN THE STANDARD.
5. STABILIZATION OF THE AREA DISTURBED BY THE DIKE AND SWALE SHALL BE DONE IN ACCORDANCE WITH THE STANDARD AND SPECIFICATION FOR SEED AND STRAW MULCH, AND SHALL BE DONE WITHIN 10 DAYS.
6. PERIODIC INSPECTION AND REQUIRED MAINTENANCE MUST BE PROVIDED AFTER EACH RAIN EVENT.

*Gary M. Schimek*  
 APPROVED BY: GARY M. SCHIMEK  
 NATURAL RESOURCES/STORMWATER ENGINEERING MANAGER  
 REVISION DATE: JULY 01, 2015



STANDARD DETAILS  
**PERIMETER DIKE/SWALE**  
 FILE NAME: SD506.DWG    DETAIL NUMBER: **506**



**PLAN**  
NTS

*Gary M. Schimek*

APPROVED BY: GARY M. SCHIMEK  
NATURAL RESOURCES/STORMWATER ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015

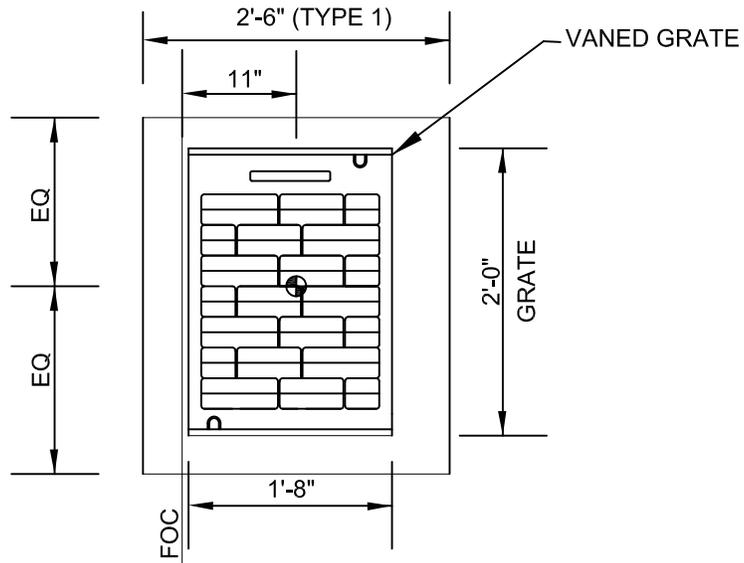


City of Redmond  
WASHINGTON

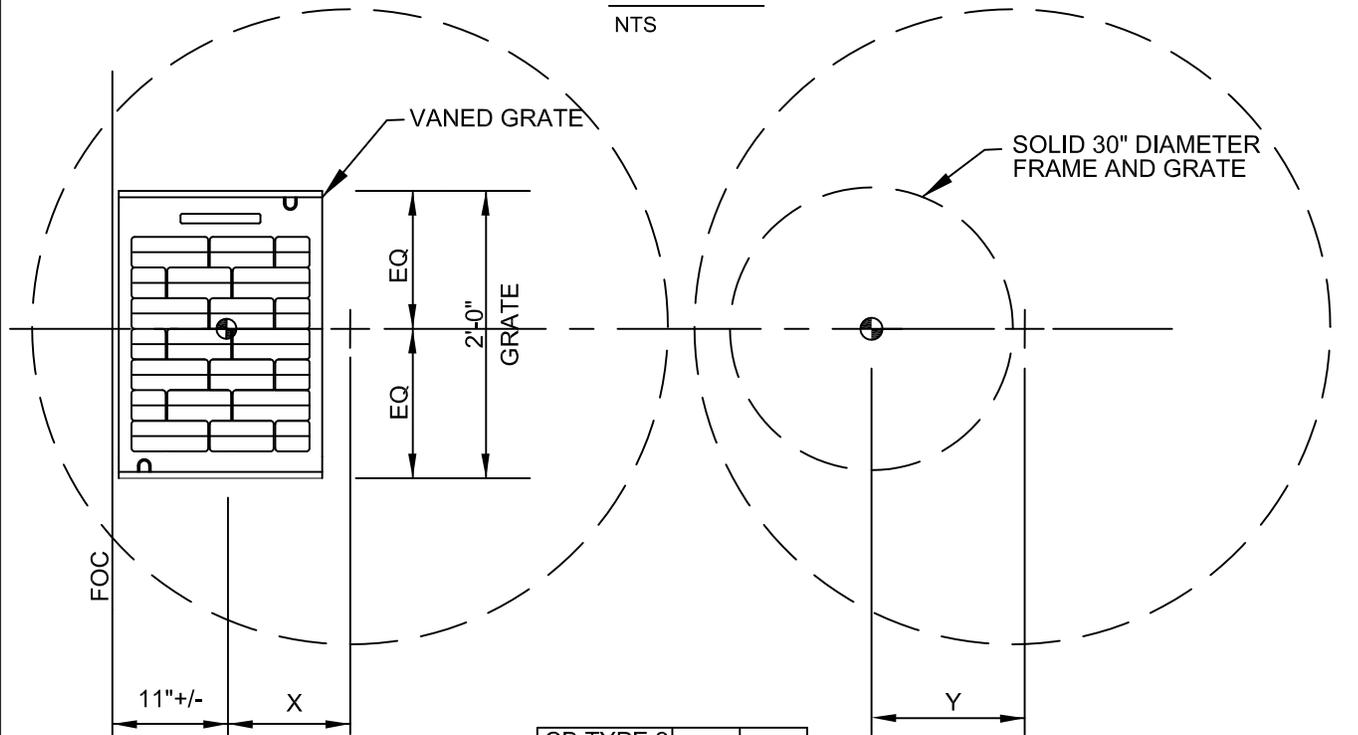
STANDARD DETAILS

**TYPICAL ORIENTATION OF TYPE 2  
CB IN CITY STREET SECTION**

FILE NAME: SD609A.DWG    DETAIL NUMBER: **609A**



**TYPE 1/1L**  
NTS



| CB TYPE 2 |      | X   | Y   |
|-----------|------|-----|-----|
| I.D.      | O.D. |     |     |
| 48"       | 58"  | 11" | 12" |
| 54"       | 65"  | 14" | 15" |
| 60"       | 72"  | 17" | 18" |
| 72"       | 86"  | 24" | 24" |

**KEY**

⊙ CENTER OF GRATE

⊕ CENTER OF STRUCTURE

**NOTES:**

1. CENTER OF GRATE = CENTER OF STRUCTURE FOR TYPE 1 AND TYPE 1L STRUCTURES ONLY.
2. SPECIFIED STATION AND OFFSET REFERENCE CENTER OF STRUCTURE FOR ALL DRAINAGE FACILITIES UNLESS OTHERWISE NOTED ON PLANS.
3. ALL GRATES SHALL BE VANED UNLESS OTHERWISE SHOWN.

**TYPE 2/3**  
NTS

*Gary M. Schimek*

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NATURAL RESOURCES/STORMWATER ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



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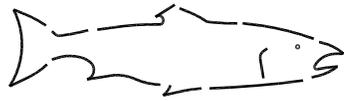
STANDARD DETAILS

**TYPICAL CATCH  
BASIN PLACEMENT**

FILE NAME: SD609B.DWG | DETAIL NUMBER: **609B**



DUMP NO WASTE



DRAINS TO LAKE

ONLY RAIN



DOWN THE DRAIN

DUMP NO WASTE



DRAINS TO STREAM

DUMP NO WASTE

PROTECT YOUR

GROUND WATER



NOTES:

- 1. 2" LETTERS, ACTUAL SIZE 12" X 27".
- 2. STENCILS ARE AVAILABLE AT THE STORM WATER CONSTRUCTION DIVISION.
- 3. ALL STORM DRAINAGE CONSTRUCTION SHALL REQUIRE STENCILING PAVED SURFACE AREA NEAR CATCH BASIN.

STENCILING DETAIL

NTS

*Gary M. Schimek*

APPROVED BY: GARY M. SCHIMEK  
NATURAL RESOURCES/STORMWATER ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



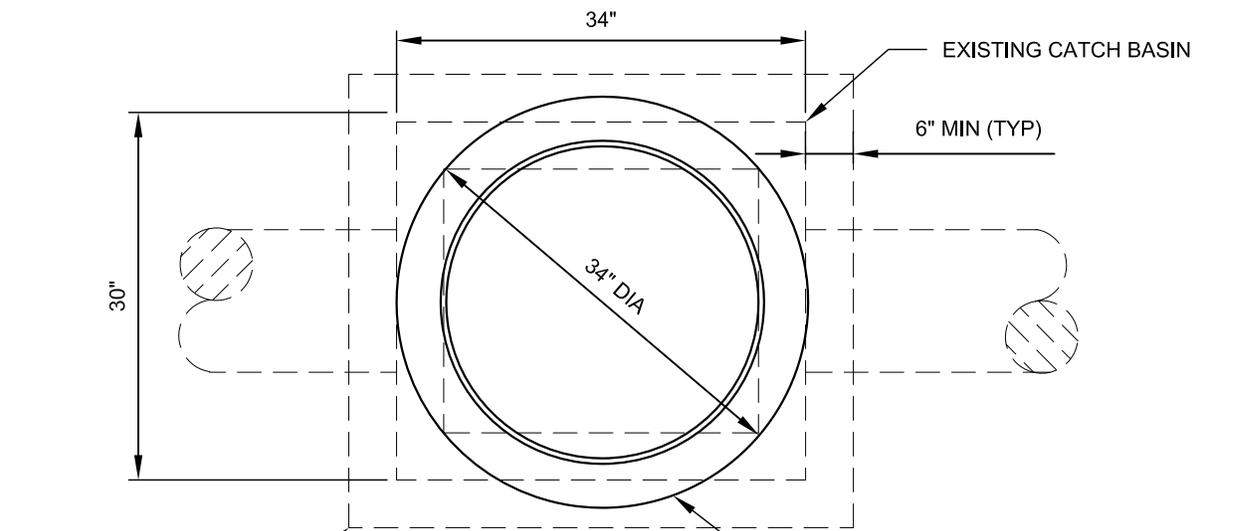
City of Redmond  
WASHINGTON

STANDARD DETAILS

**STENCILING DETAIL**

FILE NAME: SD622.DWG

DETAIL NUMBER: **622**



**PLAN VIEW**  
NTS

3000 PSI  
CONCRETE  
COLLAR

EXISTING CATCH BASIN

6" MIN (TYP)

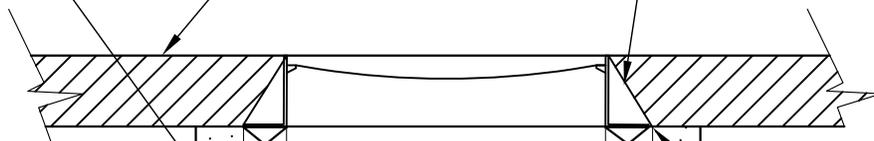
34"

30"

34" DIA

REMOVE SQUARE FRAME AND  
GRATE REPLACE WITH ROUND  
SOLID LOCKING COVER AND FRAME

EXISTING PAVEMENT



**PROFILE VIEW**  
NTS

(SEE STD DETAIL 615)

EXISTING CATCH BASIN

REMOVE BRICKS  
AND ADJUST AS  
NEEDED PER  
STD DETAIL 615

6" MIN

STANDARD DETAILS

**REPLACE FRAME & GRATE  
WITH RING & COVER**

*Gary M. Schimek*

APPROVED BY: GARY M. SCHIMEK  
NATURAL RESOURCES/STORMWATER ENGINEERING MANAGER

REVISION DATE: JULY 01, 2015



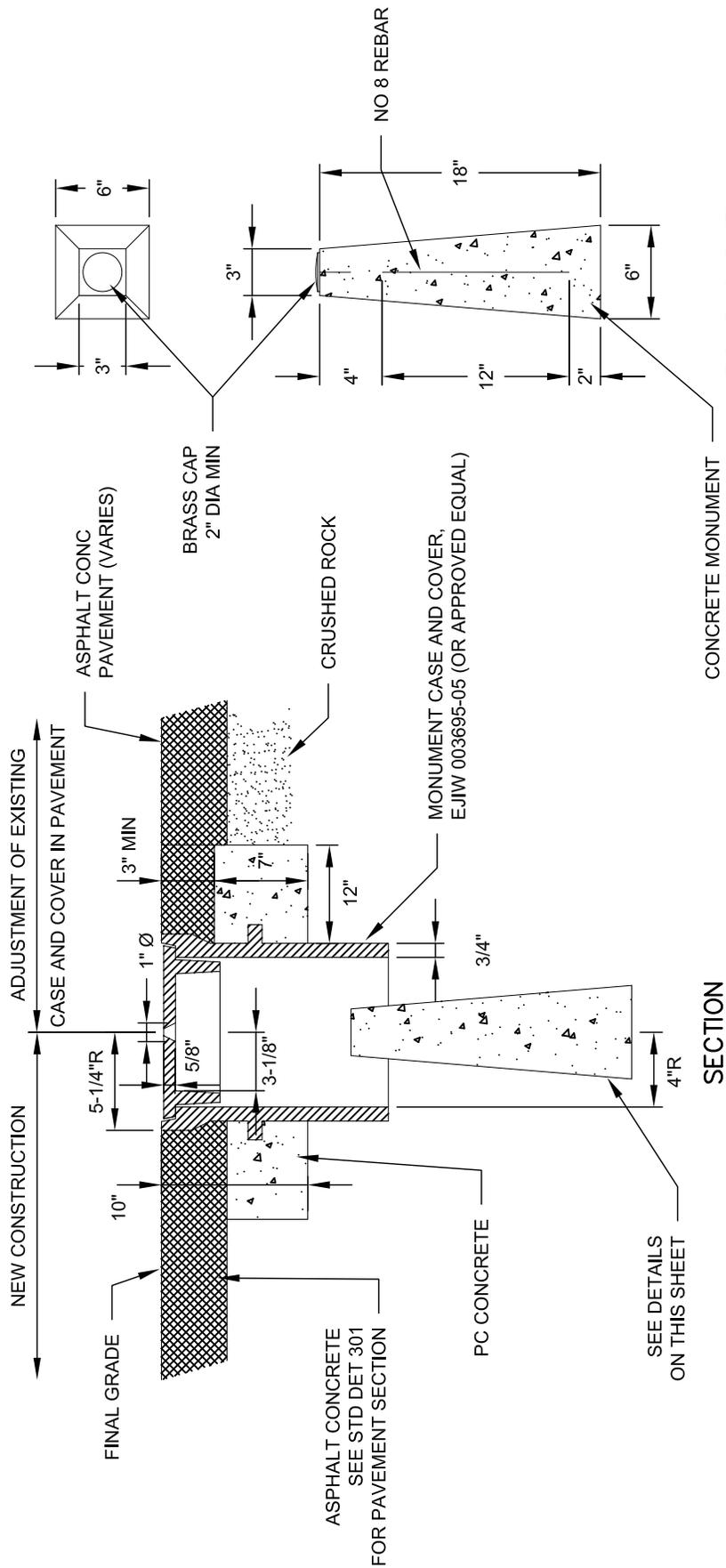
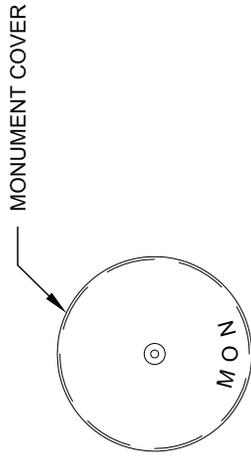
City of Redmond  
WASHINGTON

FILE NAME: SD631.DWG

DETAIL NUMBER: **631**

**NOTES:**

1. CASTINGS SHALL BE GRAY IRON AASHTO M360, CLASS 35B.
2. COVER SEAT SHALL BE MACHINED FOR PERFECT CONTACT AROUND CIRCUMFERENCE AND FULL WIDTH OF BEARING SURFACE.
3. APPROXIMATE WEIGHTS STANDARD:  
 CASE 60 LBS  
 COVER 19 LBS  
 TOTAL 79 LBS
4. BRASS CAP 2" DIA MINIMUM.
5. IF MONUMENT IS LOCATED IN A SOFT AREA, INSTALL A 2' X 2' 4" THICK, CONCRETE PAD WITH CL. 3000 CONCRETE.



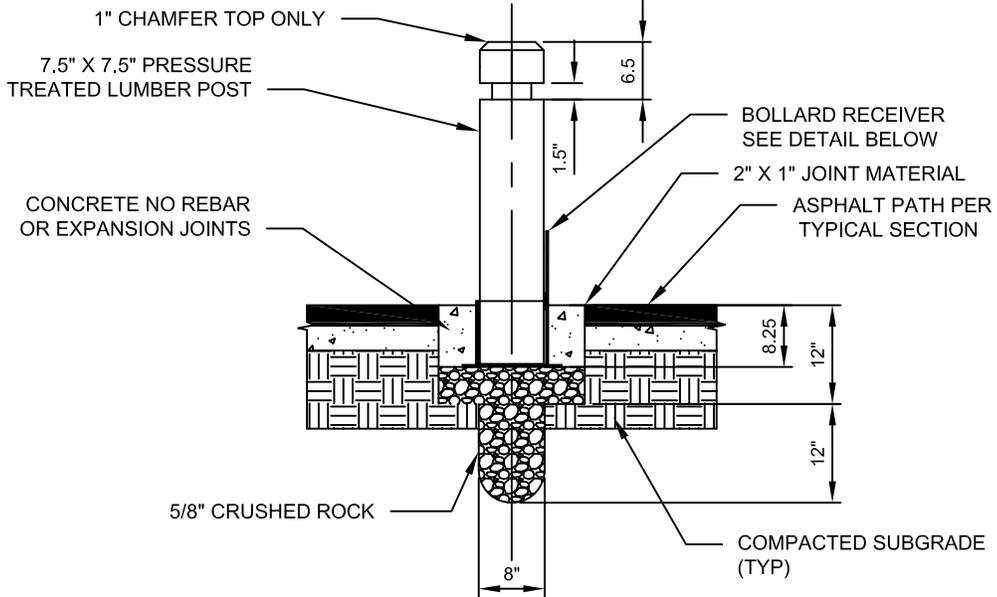
**SURVEY MONUMENT CASE AND COVER**

NTS

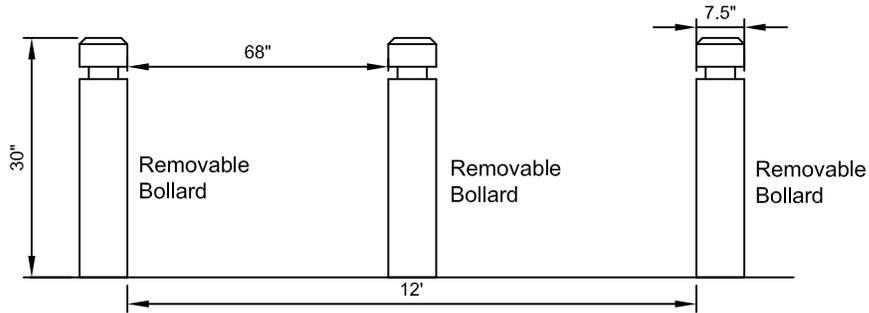
*Rob Crittenden*  
 APPROVED BY: ROB CRITTENDEN  
 TRAFFIC OPERATIONS SAFETY AND ENGINEERING MANAGER  
 REVISION DATE: JULY 01, 2015



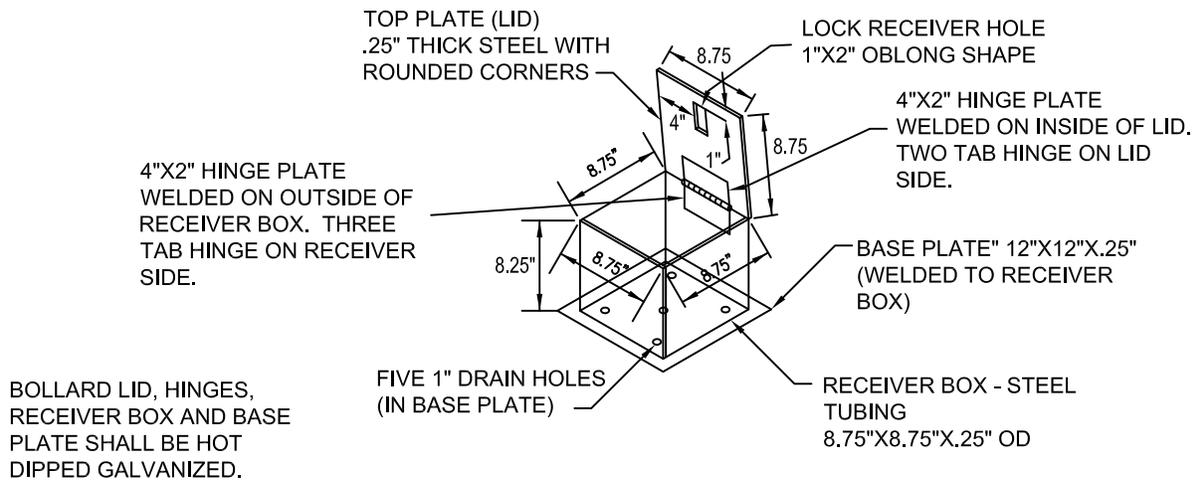
STANDARD DETAILS  
**SURVEY MONUMENT CASE AND COVER**  
 FILE NAME: SD903.DWG    DETAIL NUMBER: **903**



**BOLLARD ELEVATION**  
NTS



**BOLLARD SPACING**  
NTS



**REMOVABLE BOLLARD RECEIVER**  
NTS

*Dave Tucek*

APPROVED BY: DAVE TUCHEK  
PARKS MAINTENANCE AND OPERATIONS MANAGER

REVISION DATE: JULY 01, 2015



City of Redmond  
WASHINGTON

STANDARD DETAILS

**PARKS BOLLARD**

FILE NAME: SD911.DWG

DETAIL NUMBER: **911**

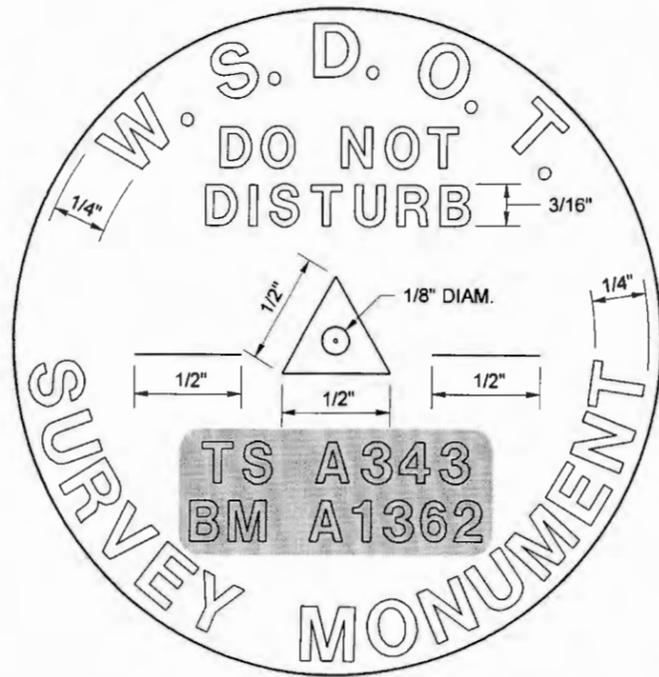
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## WSDOT Standard Plans

|            |  |
|------------|--|
| A-10.20-00 | Survey Monument Types 1 and 2  |
| B-5.20-01  | Catch Basin Type 1   |
| B-10.20-01 | Catch Basin Type 2   |
| B-25.20-01 | Combination Inlet  |
| B-25.60-00 | Concrete Inlet   |
| B-30.10-01 | Rectangular Frame (Reversible)   |
| B-30.20-02 | Rectangular Solid Metal Cover  |
| B-30.30-01 | Rectangular Vaned Grate  |
| B-30.40-01 | Rectangular Bi-Directional Vaned Grate                                 |
| B-30.50-01 | Rectangular Herringbone Grate  |
| B-30.70-03 | Circular Frame (Ring) and Cover  |
| B-30.90-01 | Miscellaneous Details for Drainage Structures                          |
| B-55.20-00 | Pipe Zone Bedding and Backfill   |
| I-10.10-01 | High Visibility Fence  |
| I-30.17-00 | High Visibility Silt Fence   |
| I-30.20-00 | Erosion Control at Culvert Ends  |
| I-30.30-01 | Wattle Installation on Slope   |
| I-40.20-00 | Storm Drain Inlet Protection   |
| I-60.10-01 | Biodegradable Erosion Control Blanket Placement for Slopes             |
| J-21.10-04 | Type PS, Type 1, RM & FB Signal Standard Foundation Details (2 sheets) |
| J-40.10-03 | Locking Lid Standard Duty Junction Box Types 1 & 2 (2 sheets)          |
| J-40.30-03 | Locking Lid Standard Duty Junction Box Type 8 (2 sheets)               |

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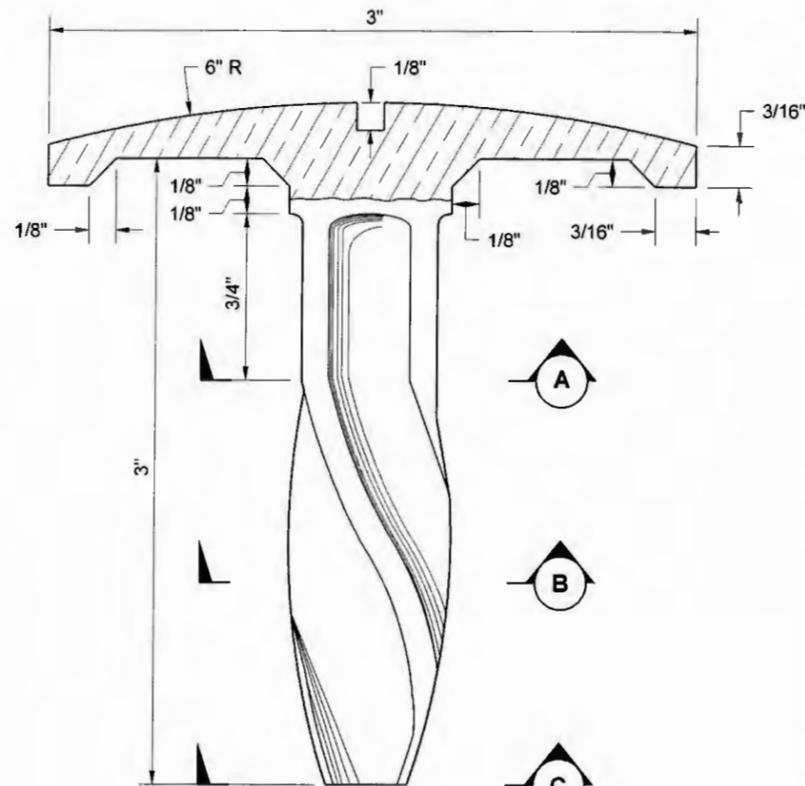
DRAWN BY: MARK SUJKA



TYPE 1

TOP VIEW

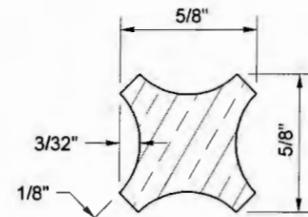
BRASS DISC



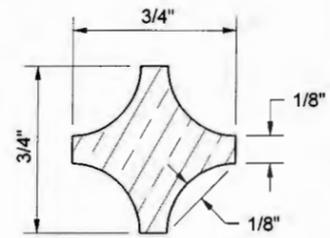
SIDE VIEW



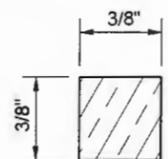
TYPE 2



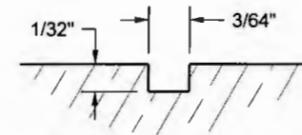
SECTION A



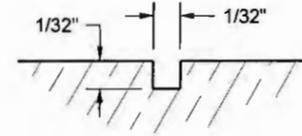
SECTION B



SECTION C



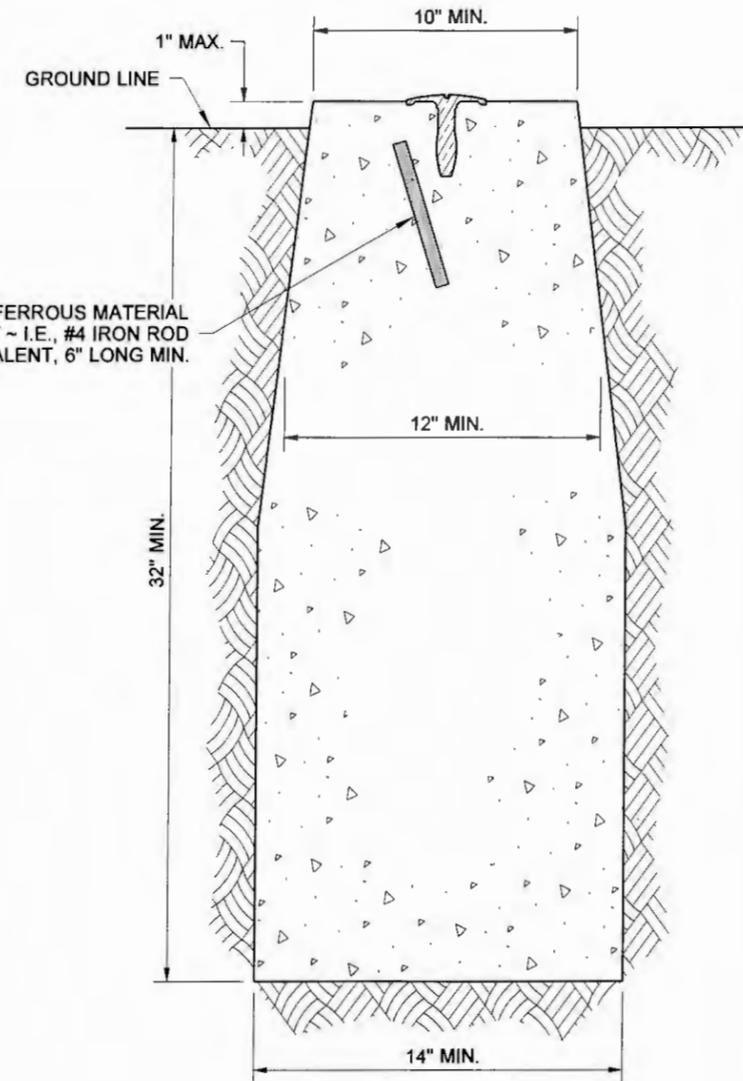
SECTION OF GROOVE FOR 1/4" LETTERS



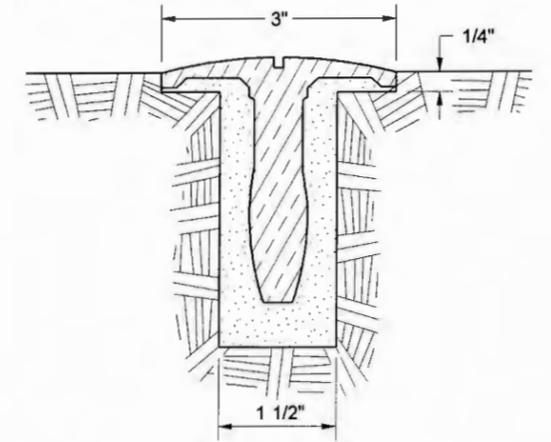
SECTION OF GROOVE FOR 3/16" LETTERS

NOTES

1. The Brass Disc will be furnished by the State.
2. The text in the shaded area (see TOP VIEW) shall be 3/16" high and will be stamped by WSDOT personnel prior to setting the cap. Only the assigned identification letters and numbers are to be placed on the Brass Disc.
3. The hole shall be 32" minimum in depth or 6" below the deepest recorded frost line. All loose material shall be removed from the bottom of the hole so that the concrete is placed on firm undisturbed earth.
4. The top of the concrete shall be troweled smooth and the Brass Disc set in the center with top flush and level. The top of the monument may be recessed or protruding, depending on conditions.
5. The Brass Disc shall be rotated so it can be read while the observer is facing north.
6. When the concrete is set, cover the entire monument with moist earth and leave for three days.
7. To replace a Public Land Survey System (PLSS) corner, consult a licensed Professional Land Surveyor (PLS).



SECTION VIEW  
GENERAL INSTALLATION



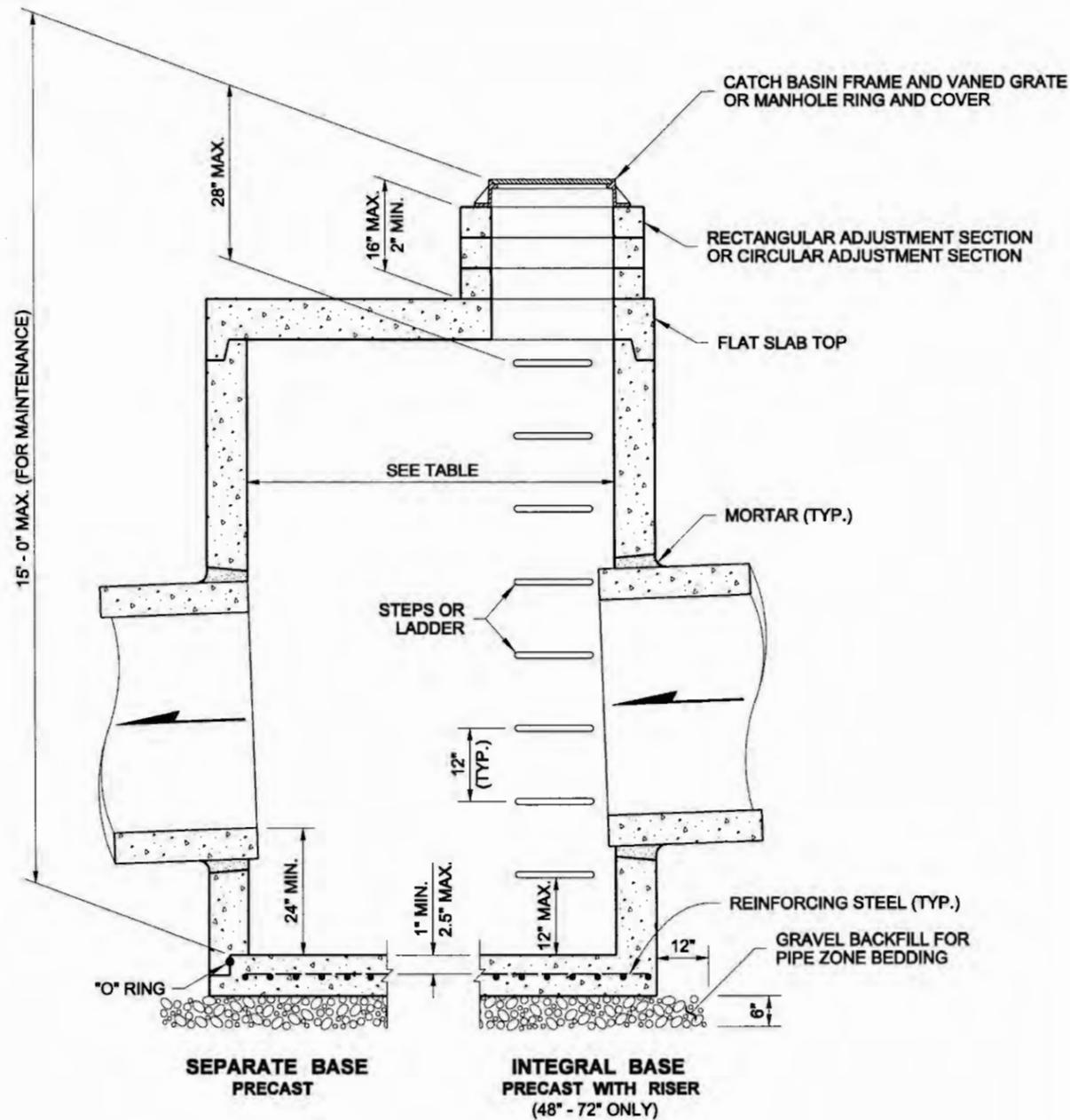
SECTION VIEW  
LEDGE ROCK, CONCRETE,  
OR ASPHALT INSTALLATION



**SURVEY MONUMENT  
TYPES 1 AND 2  
STANDARD PLAN A-10.20-00**

SHEET 1 OF 1 SHEET

APPROVED FOR PUBLICATION  
*Peter Belcher* 10/5/07  
 STATE DESIGN ENGINEER DATE  
 Washington State Department of Transportation



**NOTES**

1. No steps are required when height is 4' or less.
2. The bottom of the precast catch basin may be sloped to facilitate cleaning.
3. The rectangular frame and grate may be installed with the flange up or down. The frame may be cast into the adjustment section.
4. Knockouts shall have a wall thickness of 2" minimum to 2.5" maximum. Provide a 1.5" minimum gap between the knockout wall and the outside of the pipe. After the pipe is installed, fill the gap with joint mortar in accordance with **Standard Specification 9-04.3**.

| CATCH BASIN DIMENSIONS |                     |                     |                       |                                    |
|------------------------|---------------------|---------------------|-----------------------|------------------------------------|
| CATCH BASIN DIAMETER   | MIN. WALL THICKNESS | MIN. BASE THICKNESS | MAXIMUM KNOCKOUT SIZE | MINIMUM DISTANCE BETWEEN KNOCKOUTS |
| 48"                    | 4"                  | 6"                  | 36"                   | 8"                                 |
| 54"                    | 4.5"                | 8"                  | 42"                   | 8"                                 |
| 60"                    | 5"                  | 8"                  | 48"                   | 8"                                 |
| 72"                    | 6"                  | 8"                  | 60"                   | 12"                                |
| 84"                    | 8"                  | 12"                 | 72"                   | 12"                                |
| 96"                    | 8"                  | 12"                 | 84"                   | 12"                                |
| 120"                   | 10"                 | 12"                 | 96"                   | 12"                                |
| 144"                   | 12"                 | 12"                 | 108"                  | 12"                                |

| PIPE ALLOWANCES      |  |           |         |                  |                    |
|----------------------|--|-----------|---------|------------------|--------------------|
| CATCH BASIN DIAMETER | PIPE MATERIAL WITH MAXIMUM INSIDE DIAMETER |           |         |                  |                    |
|                      | CONCRETE                                   | ALL METAL | CPSSP ① | SOLID WALL PVC ② | PROFILE WALL PVC ③ |
| 48"                  | 24"  | 30"       | 24"     | 30"              | 30"                |
| 54"                  | 30"  | 36"       | 30"     | 36"              | 36"                |
| 60"                  | 36"  | 42"       | 36"     | 42"              | 42"                |
| 72"                  | 42"  | 54"       | 42"     | 48"              | 48"                |
| 84"                  | 54"  | 60"       | 54"     | 48"              | 48"                |
| 96"                  | 60"  | 72"       | 60"     | 48"              | 48"                |
| 120"                 | 66"  | 84"       | 60"     | 48"              | 48"                |
| 144"                 | 78"  | 96"       | 60"     | 48"              | 48"                |

① Corrugated Polyethylene Storm Sewer Pipe (Standard Specification 9-05.20)  
 ② (Standard Specification 9-05.12(1))  
 ③ (Standard Specification 9-05.12(2))

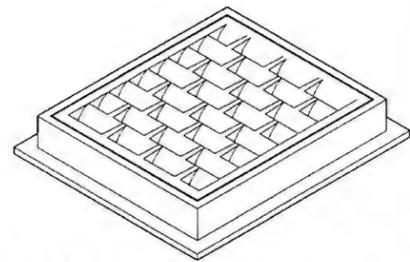


1-3-12

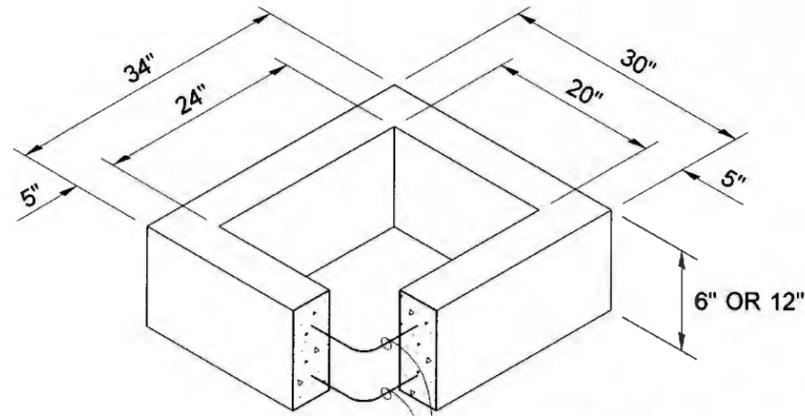
**CATCH BASIN TYPE 2**  
**STANDARD PLAN B-10.20-01**  
 SHEET 1 OF 1 SHEET

APPROVED FOR PUBLICATION  
*Pamela B. [Signature]* 2/7/12  
 STATE DESIGN ENGINEER DATE  
 Washington State Department of Transportation



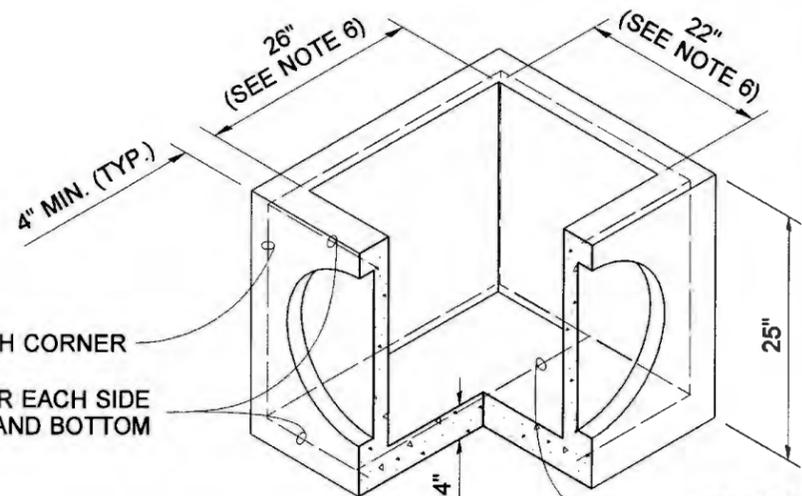


**FRAME AND VANED GRATE**



ONE #3 BAR HOOP FOR 6" HEIGHT  
TWO #3 BAR HOOPS FOR 12" HEIGHT

**RECTANGULAR ADJUSTMENT SECTION**



#3 BAR EACH CORNER

#3 BAR EACH SIDE  
TOP AND BOTTOM

ONE #3 BAR  
ACROSS BOTTOM

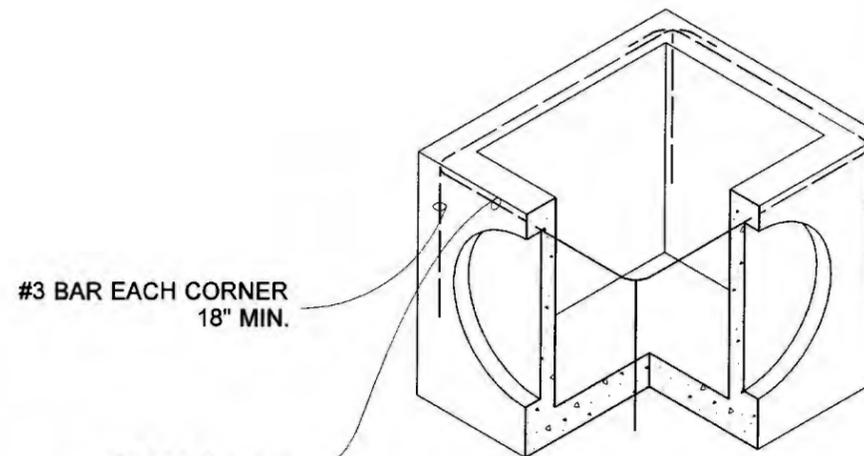
**PRECAST BASE SECTION**

| PIPE ALLOWANCES                             |                         |
|---|-------------------------|
| PIPE MATERIAL                               | MAXIMUM INSIDE DIAMETER |
| REINFORCED OR PLAIN CONCRETE                | 12"                     |
| ALL METAL PIPE                              | 15"                     |
| CPSSP *<br>(STD. SPEC. 9-05.20)             | 12"                     |
| SOLID WALL PVC<br>(STD. SPEC. 9-05.12(1))   | 15"                     |
| PROFILE WALL PVC<br>(STD. SPEC. 9-05.12(2)) | 15"                     |

\* CORRUGATED POLYETHYLENE  
STORM SEWER PIPE

**NOTES**

1. As acceptable alternatives to the rebar shown in the **PRECAST BASE SECTION**, fibers (placed according to the Standard Specifications), or wire mesh having a minimum area of 0.12 square inches per foot shall be used with the minimum required rebar shown in the **ALTERNATIVE PRECAST BASE SECTION**. Wire mesh shall not be placed in the knockouts.
2. The knockout diameter shall not be greater than 18". Knockouts shall have a wall thickness of 2" minimum to 2.5" maximum. Provide a 1.5" minimum gap between the knockout wall and the outside of the pipe. After the pipe is installed, fill the gap with joint mortar in accordance with Standard Specification 9-04.3.
3. The maximum depth from the finished grade to the lowest pipe invert shall be 5'.
4. The frame and grate may be installed with the flange up or down. The frame may be cast into the adjustment section.
5. The Precast Base Section may have a rounded floor, and the walls may be sloped at a rate of 1:24 or steeper.
6. The opening shall be measured at the top of the precast base section.
7. All pickup holes shall be grouted full after the inlet has been placed.



#3 BAR EACH CORNER  
18" MIN.

#3 BAR HOOP

SEE NOTE 1

**ALTERNATIVE PRECAST BASE SECTION**



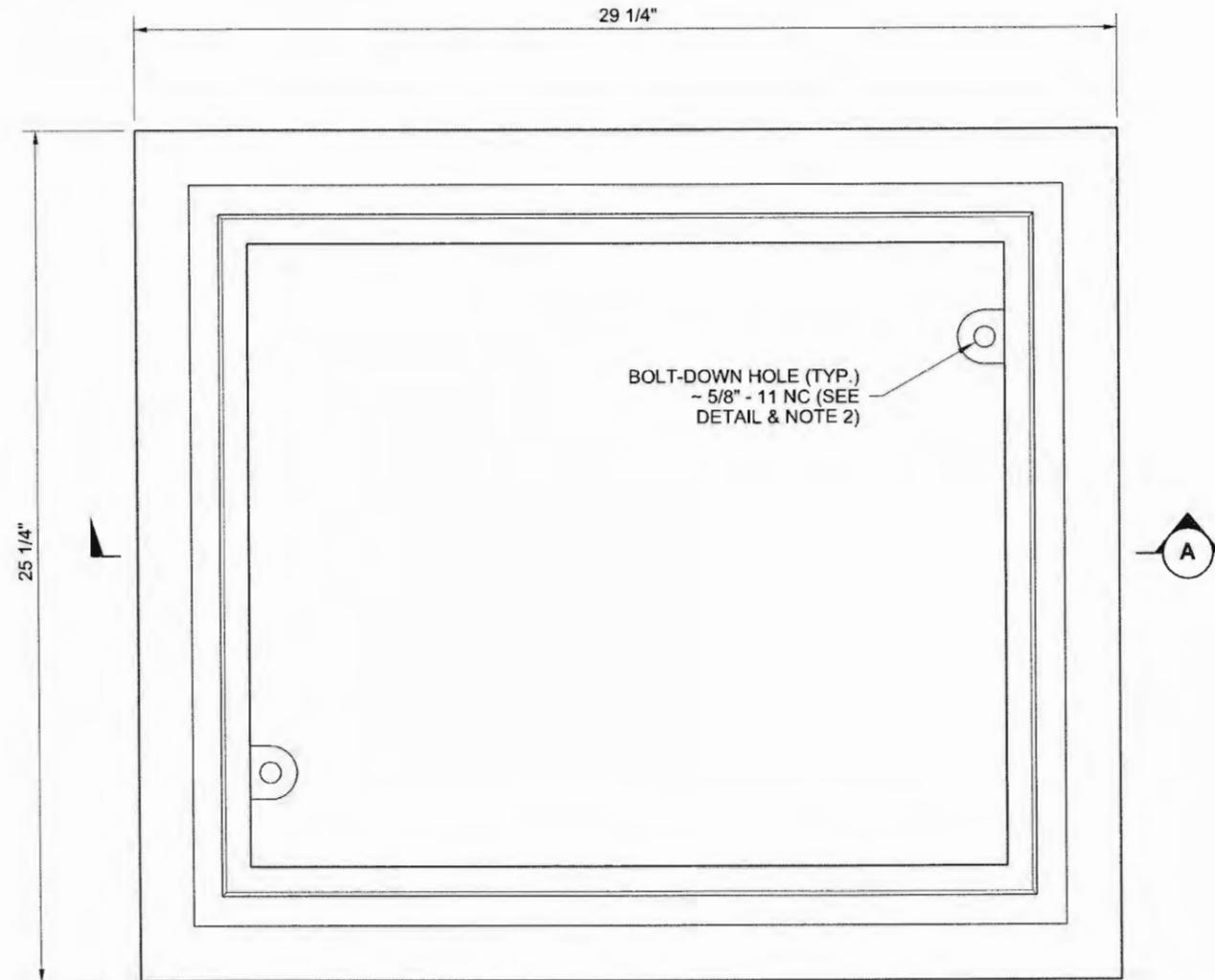
**CONCRETE INLET**

**STANDARD PLAN B-25.60-00**

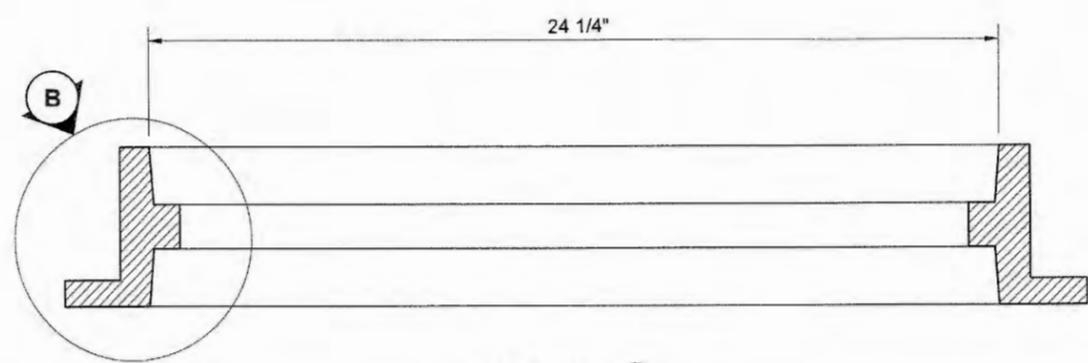
SHEET 1 OF 1 SHEET

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*David Bluff* 6.1.06  
 STATE DESIGN ENGINEER DATE  
 Washington State Department of Transportation

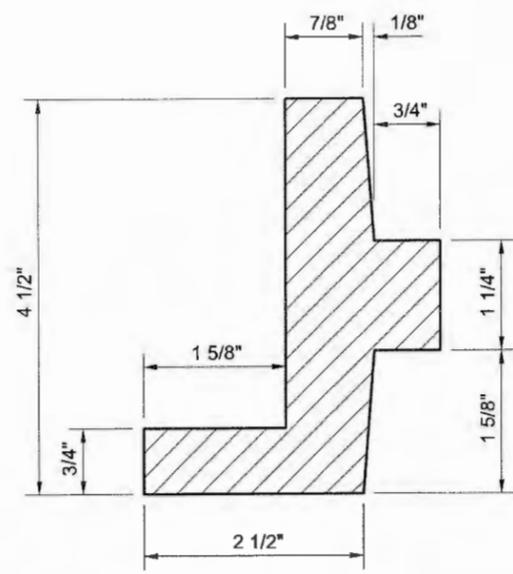
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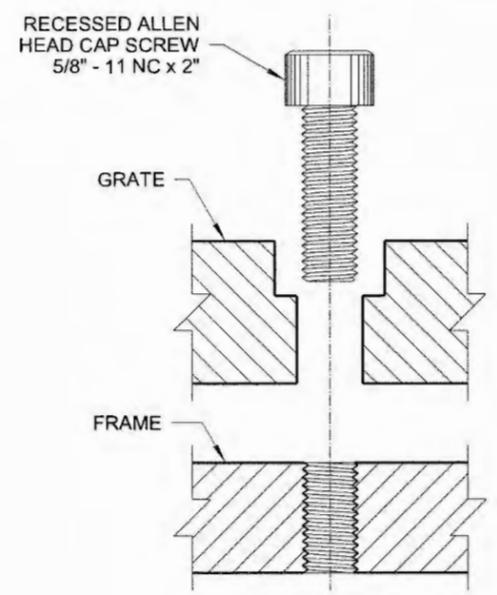
TOP



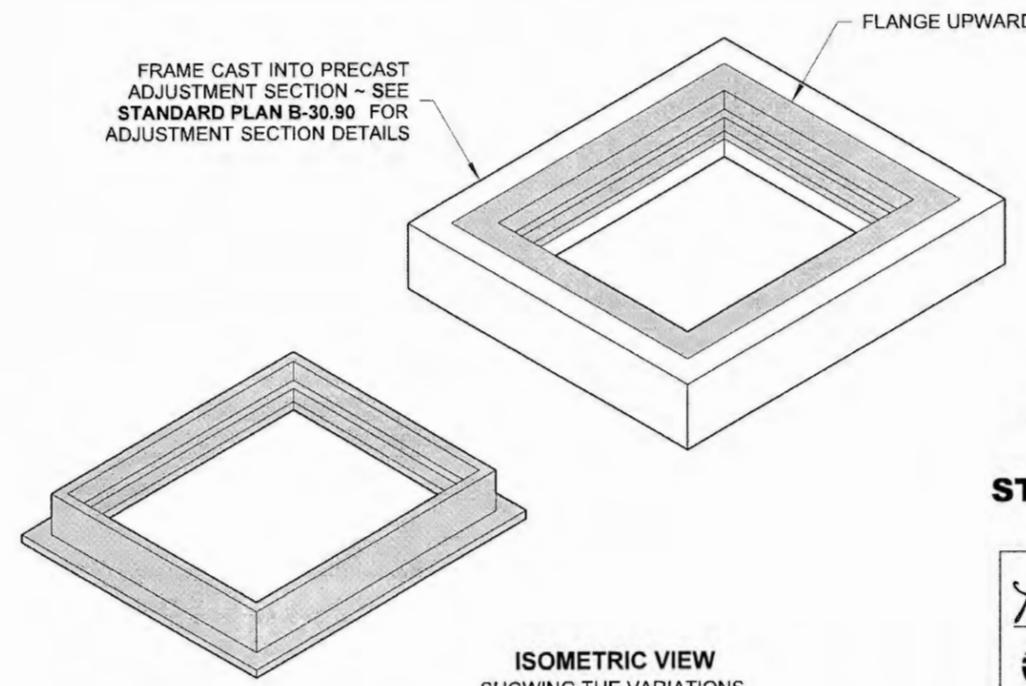
SECTION A



DETAIL B



SECTION  
BOLT-DOWN DETAIL  
SEE NOTE 2



**RECTANGULAR FRAME  
(REVERSIBLE)**

**STANDARD PLAN B-30.10-01**

SHEET 1 OF 1 SHEET

APPROVED FOR PUBLICATION

*Puro B...* 4/26/12

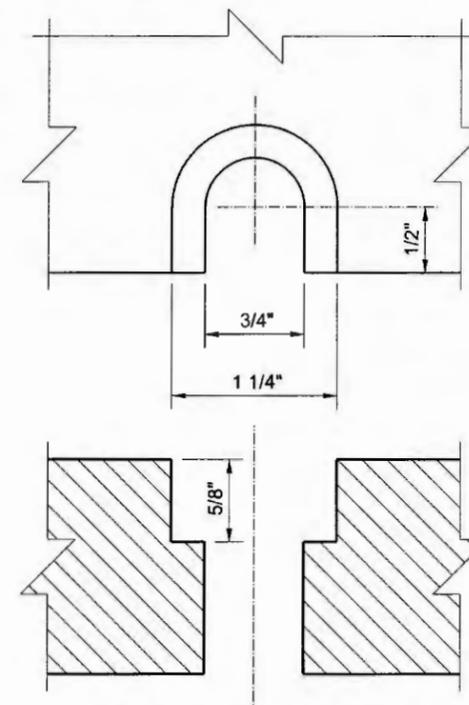
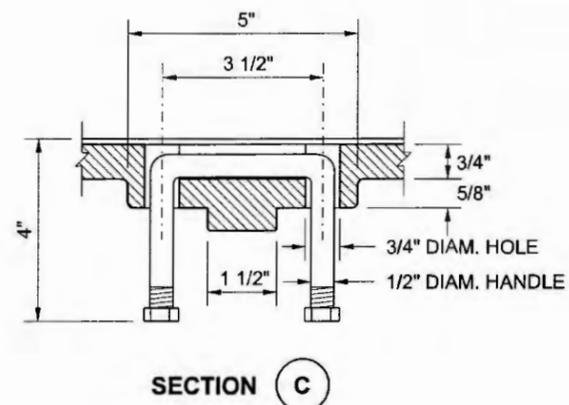
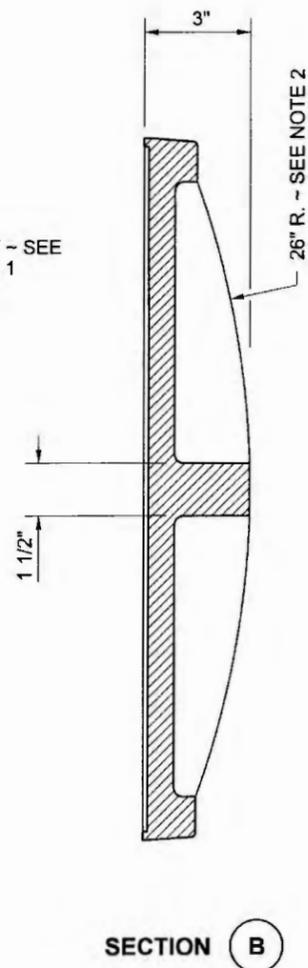
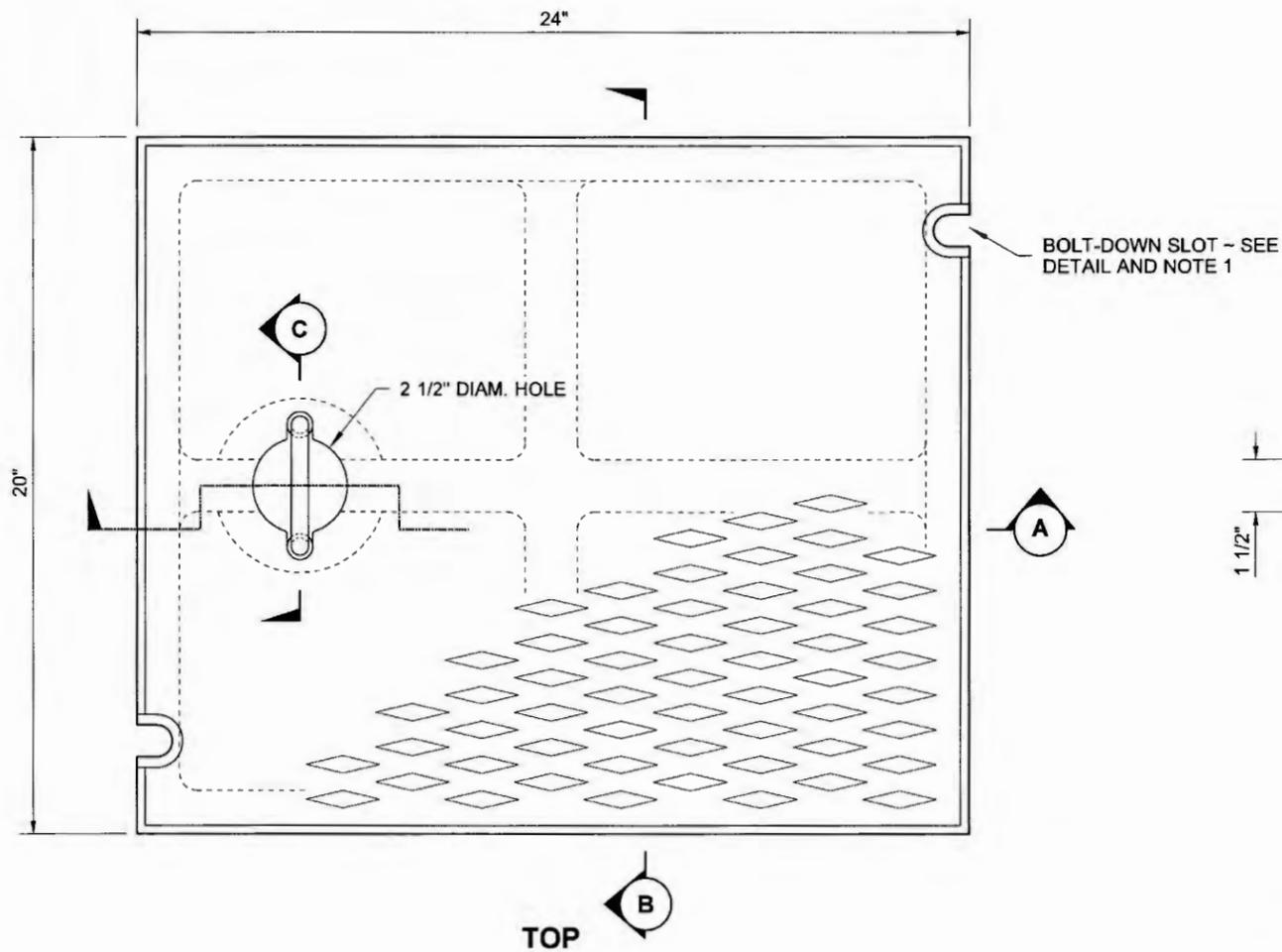
STATE DESIGN ENGINEER DATE

Washington State Department of Transportation

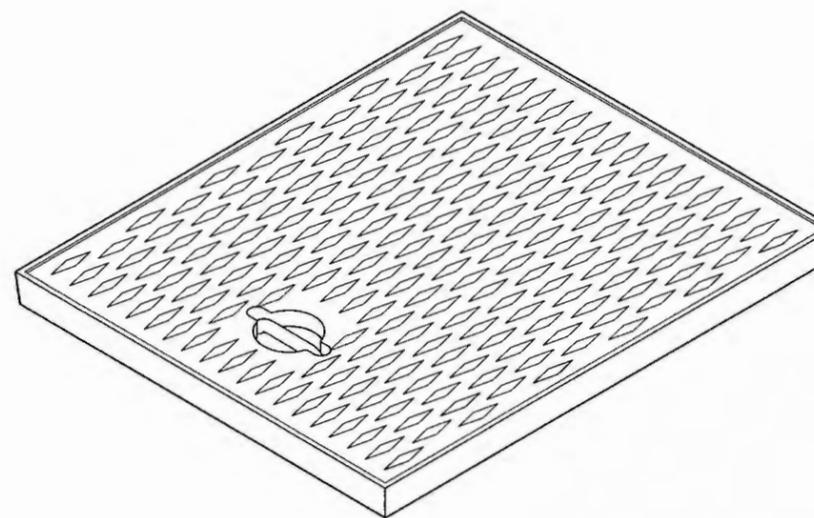
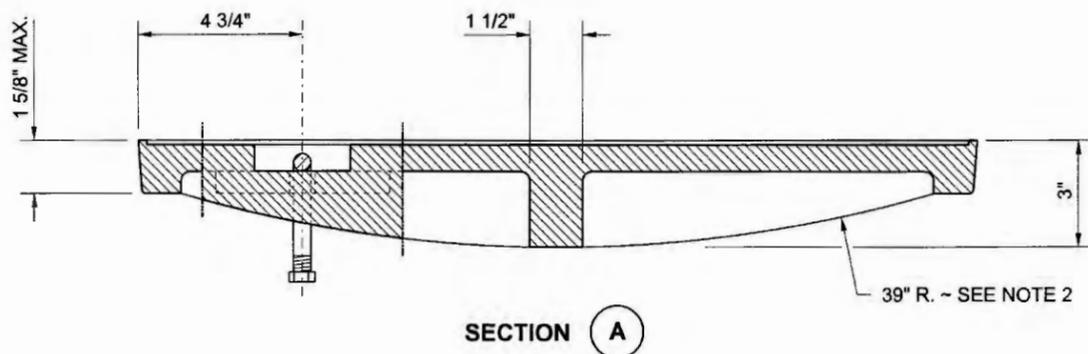
NOTES

1. This frame is designed to accommodate 20" x 24" grates or covers as shown on Standard Plans B-30.20, B-30.30, B-30.40, and B-30.50.
2. Bolt-down capability is required on all frames, grates, and covers, unless specified otherwise in the Contract. Provide 2 holes in the frame that are vertically aligned with the grate or cover slots. The frame shall accept the 5/8" - 11 NC x 2" Allen head cap screw by being tapped, or other approved mechanism. Location of bolt-down holes varies by manufacturer.
3. Refer to **Standard Specification 9-05.15(2)** for additional requirements.

DRAWN BY: LISA CYFORD



BOLT-DOWN SLOT DETAIL  
SEE NOTE 1



ISOMETRIC

NOTES

1. Bolt-down capability is required on all frames, grates, and covers, unless specified otherwise in the Contract. Provide 2 holes in the frame that are vertically aligned with the grate or cover slots. The frame shall accept the 5/8" - 11 NC x 2" Allen head cap screw by being tapped, or other approved mechanism. Location of bolt-down holes varies by manufacturer.
2. Alternative reinforcing designs are acceptable in lieu of the rib design.
3. Refer to **Standard Specification 9-05.15(2)** for additional requirements.
4. For frame details, see **Standard Plan B-30.10**.



4-25-12

RECTANGULAR SOLID  
METAL COVER

STANDARD PLAN B-30.20-02

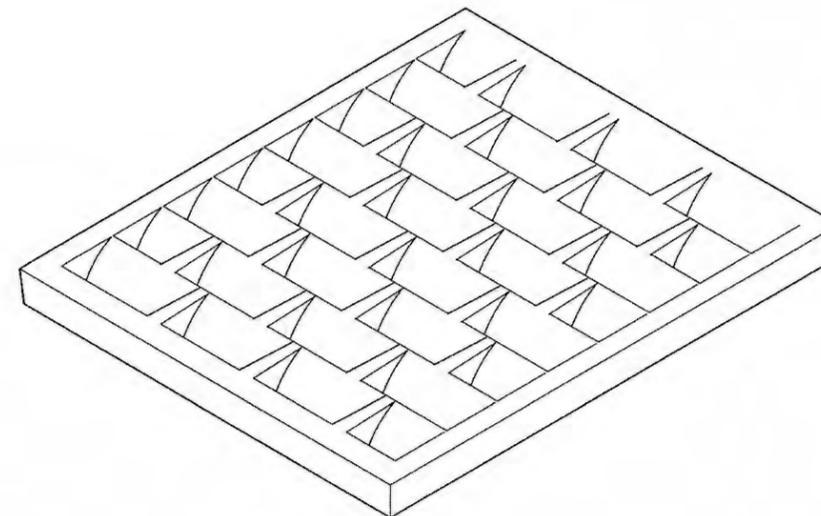
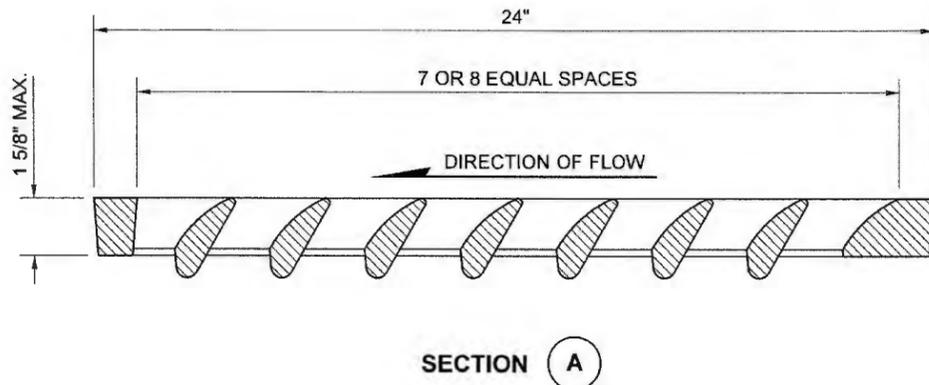
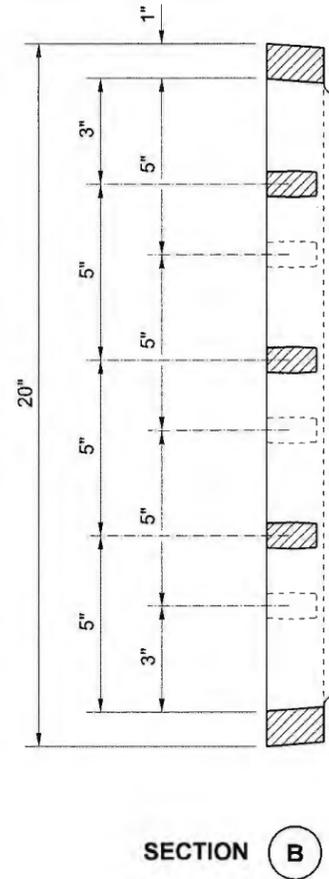
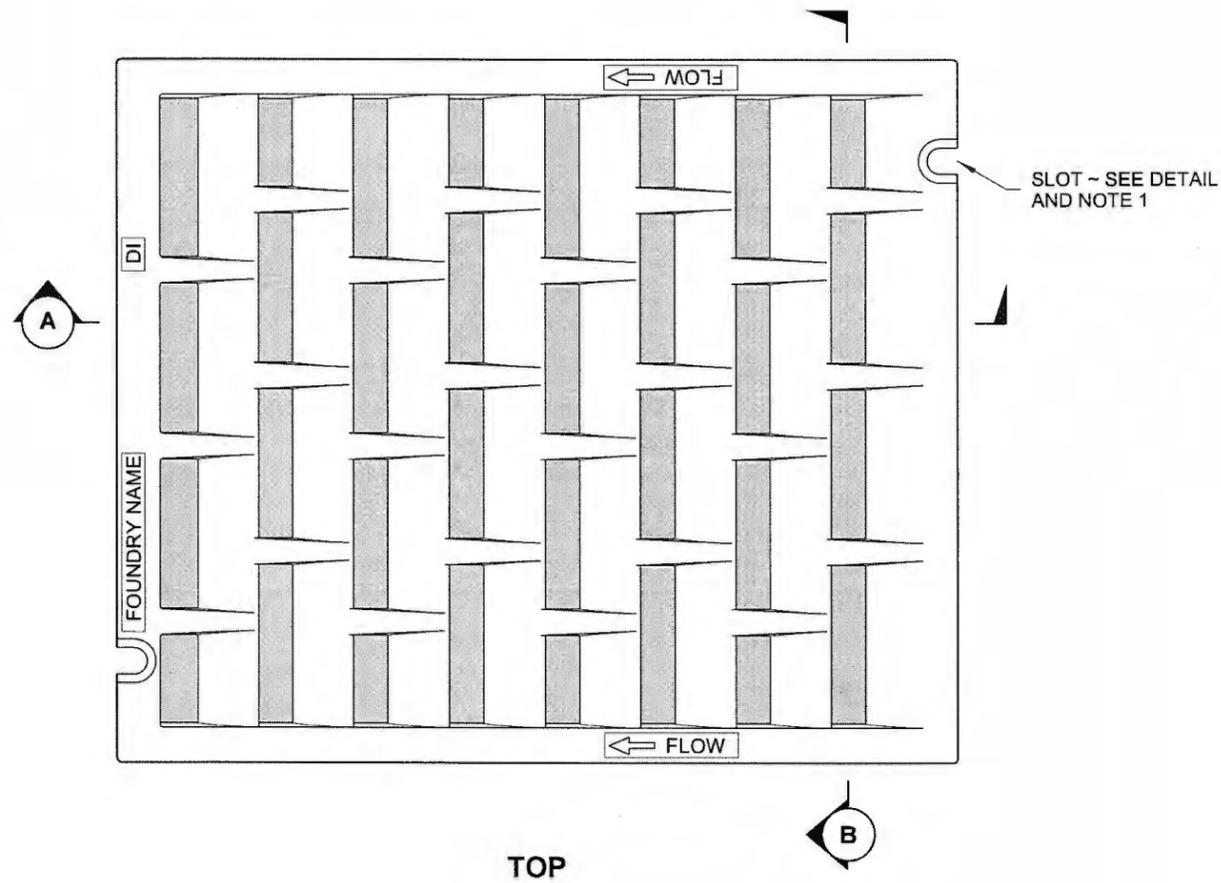
SHEET 1 OF 1 SHEET

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*Paula B...* 4/26/12  
STATE DESIGN ENGINEER DATE

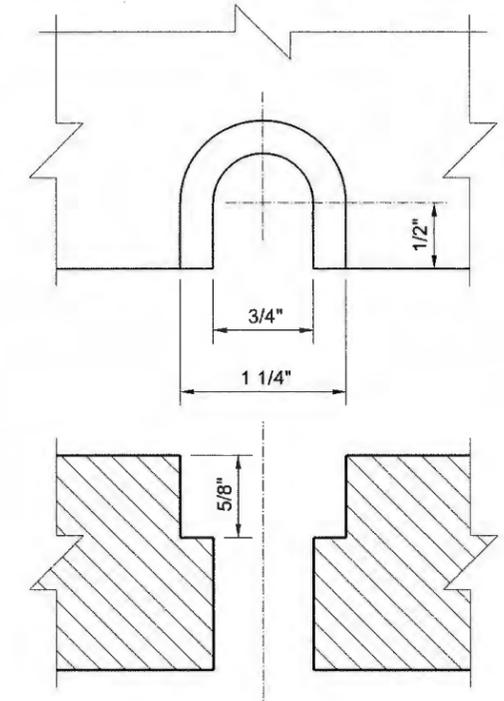
Washington State Department of Transportation

DRAWN BY: LISA CYFORD



NOTES

1. Bolt-down capability is required on all frames, grates, and covers, unless specified otherwise in the Contract. Provide 2 holes in the frame that are vertically aligned with the grate or cover slots. The frame shall accept the 5/8" - 11 NC x 2" Allen head cap screw by being tapped, or other approved mechanism. Location of bolt-down holes varies by manufacturer.
2. For frame details, see **Standard Plan B-30.10**.
3. Refer to **Standard Specification 9-05.15(2)** for additional requirements.



4-25-12

**RECTANGULAR VANED GRATE**

**STANDARD PLAN B-30.30-01**

SHEET 1 OF 1 SHEET

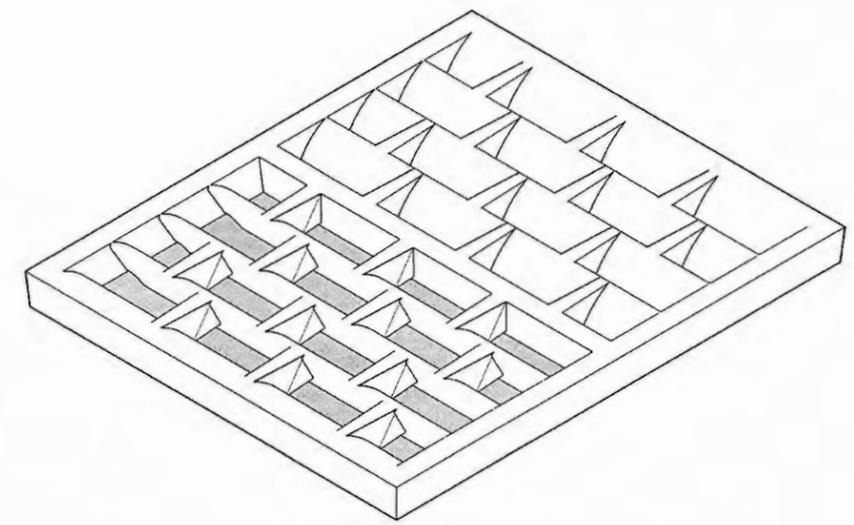
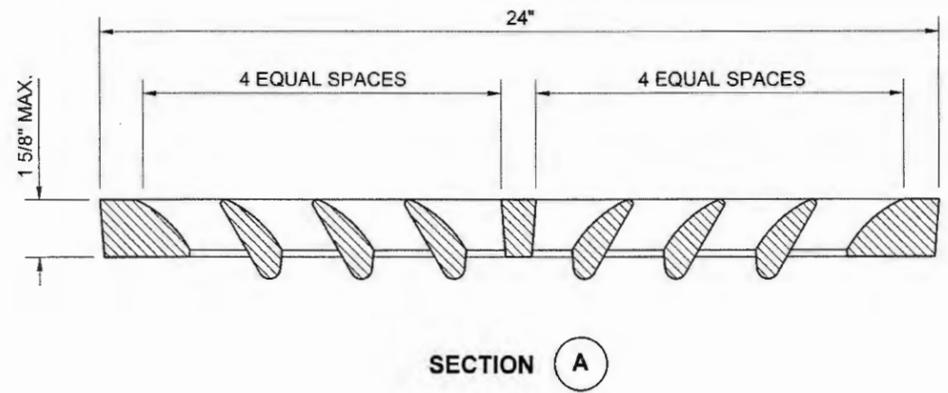
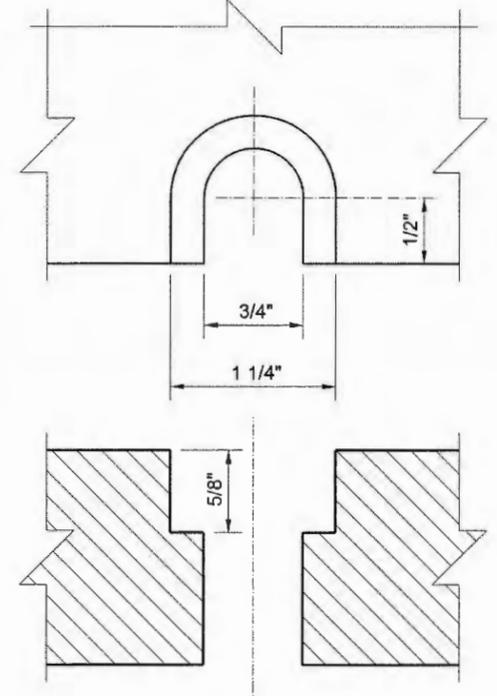
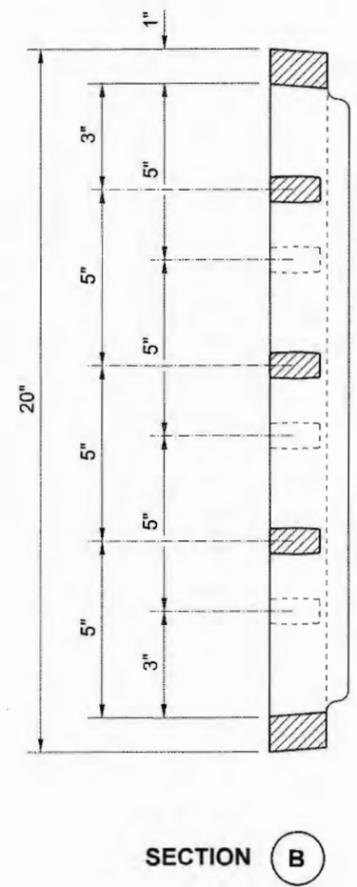
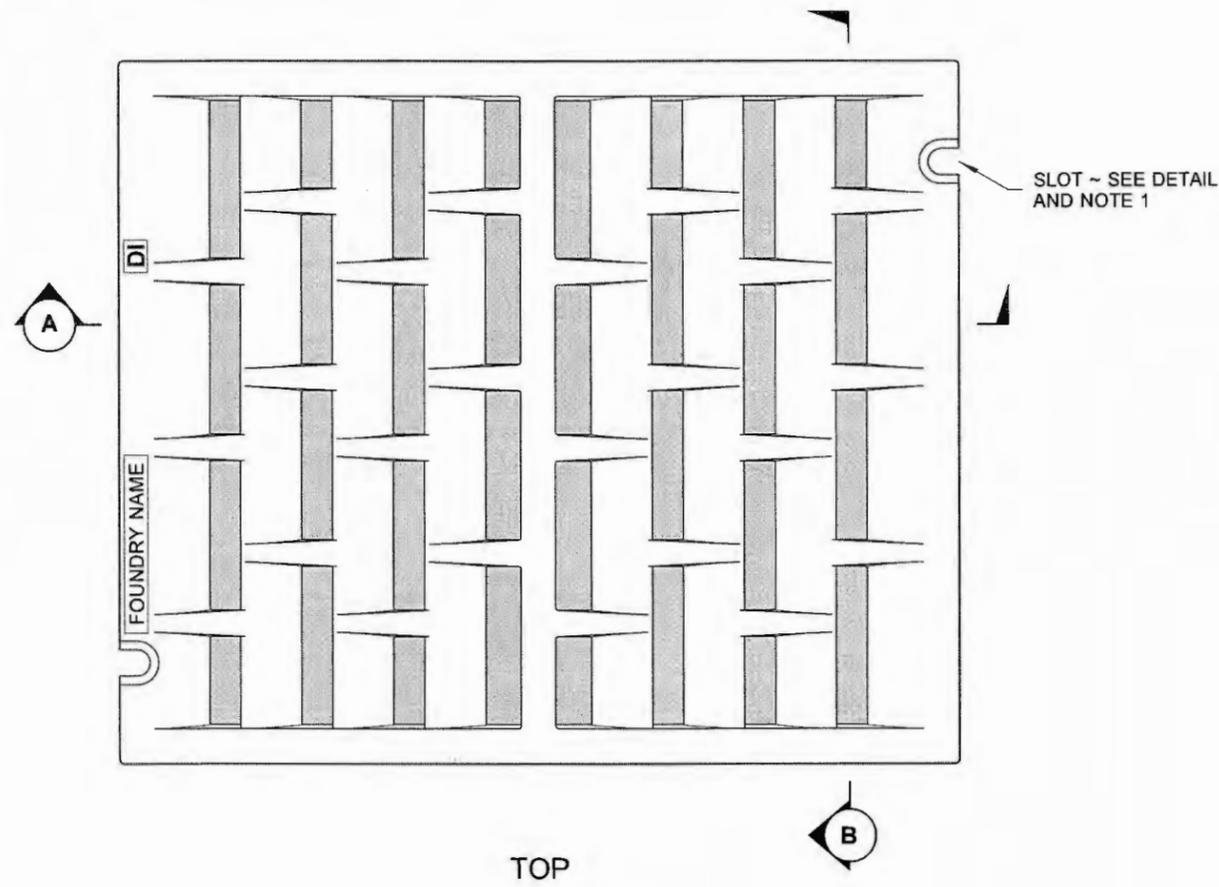
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*Piero B...* 4/26/12 DATE

STATE DESIGN ENGINEER

Washington State Department of Transportation

DRAWN BY: LISA CYFORD



NOTES

1. Bolt-down capability is required on all frames, grates, and covers, unless specified otherwise in the Contract. Provide 2 holes in the frame that are vertically aligned with the grate or cover slots. The frame shall accept the 5/8" - 11 NC x 2" Allen head cap screw by being tapped, or other approved mechanism. Location of bolt-down holes varies by manufacturer.
2. Refer to **Standard Specification 9-05.15(2)** for additional requirements.
3. For frame details, see **Standard Plan B-30.10**.



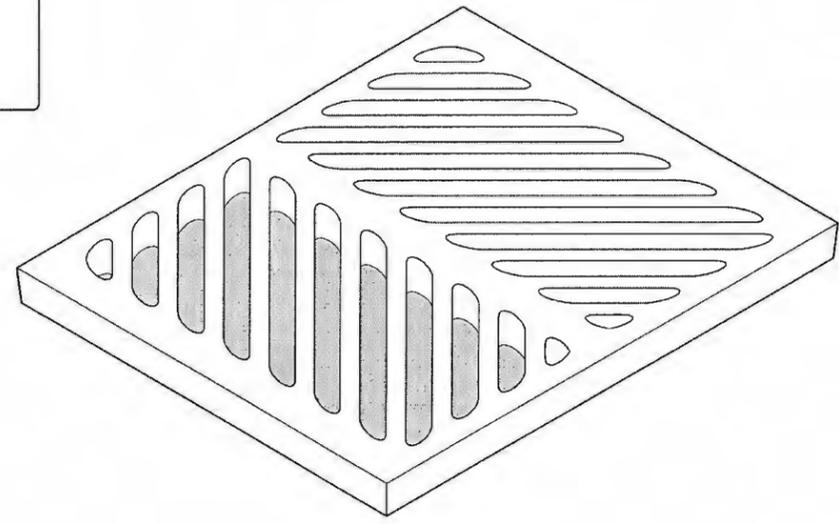
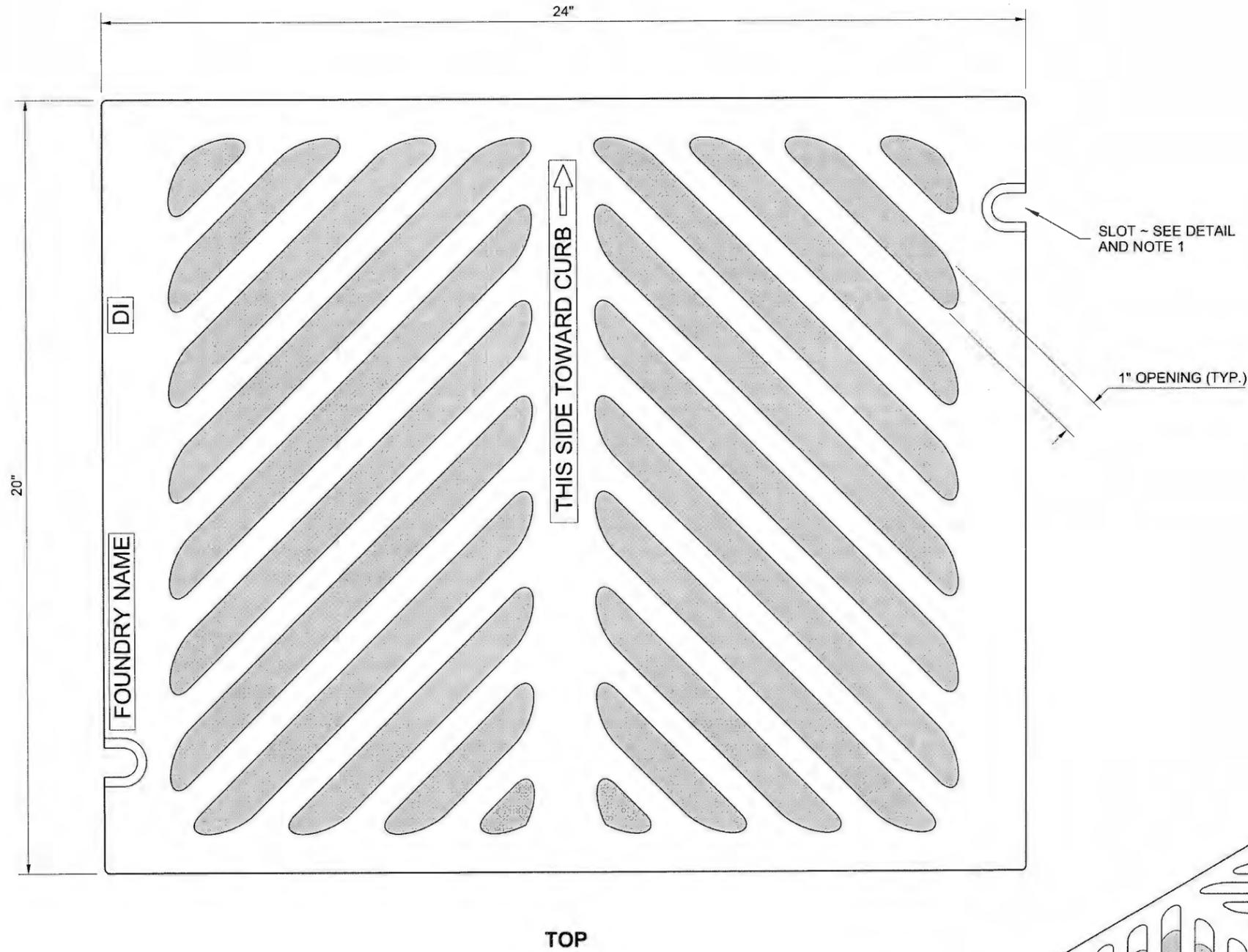
4-25-12

**RECTANGULAR  
BI-DIRECTIONAL  
VANED GRATE  
STANDARD PLAN B-30.40-01**

SHEET 1 OF 1 SHEET

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 Washington State Department of Transportation

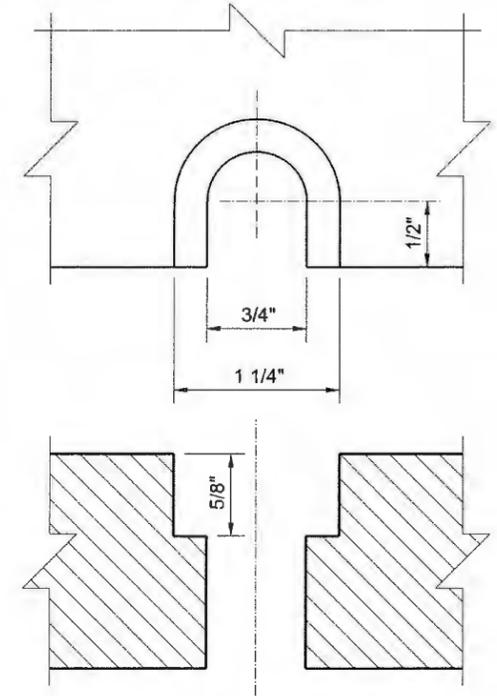
DRAWN BY: LISA CYFORD



ISOMETRIC

NOTES

1. Bolt-down capability is required on all frames, grates, and covers, unless specified otherwise in the Contract. Provide 2 holes in the frame that are vertically aligned with the grate or cover slots. The frame shall accept the 5/8" - 11 NC x 2" Allen head cap screw by being tapped, or other approved mechanism. Location of bolt-down holes varies by manufacturer.
2. Refer to **Standard Specification 9-05.15(2)** for additional requirements.
3. For frame details, see **Standard Plan B-30.10**.
4. The thickness of the grate shall not exceed 1 5/8".



BOLT-DOWN SLOT DETAIL  
SEE NOTE 1



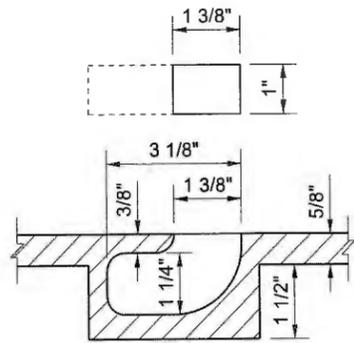
4-25-12

**RECTANGULAR  
HERRINGBONE GRATE**  
**STANDARD PLAN B-30.50-01**

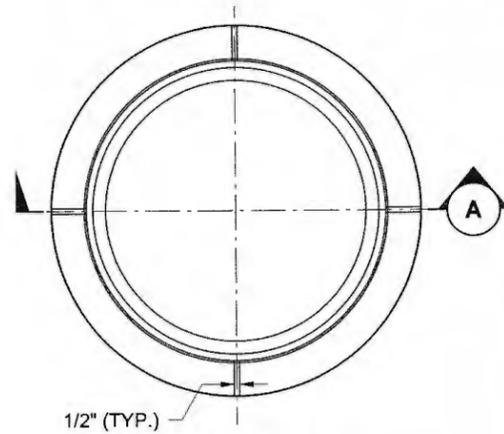
SHEET 1 OF 1 SHEET

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*Paulo B. [Signature]* 4/25/12  
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 Washington State Department of Transportation

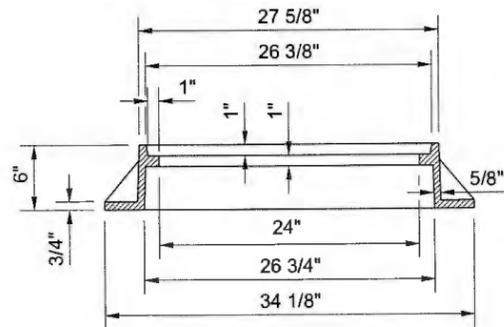
DRAWN BY: LISA CYFORD



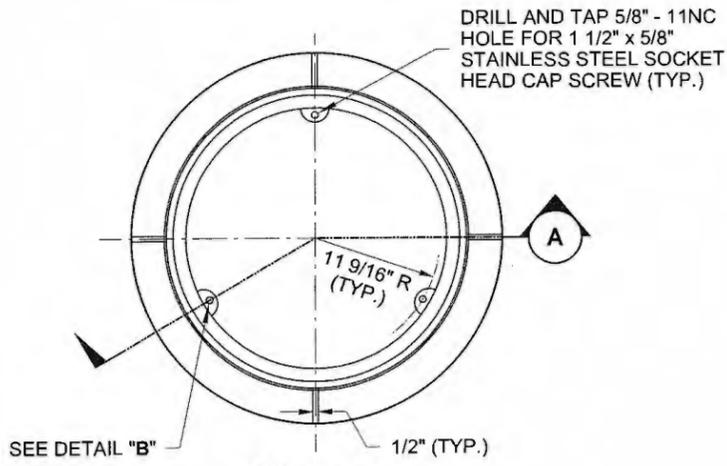
BLIND PICK NOTCH  
DETAIL "A"



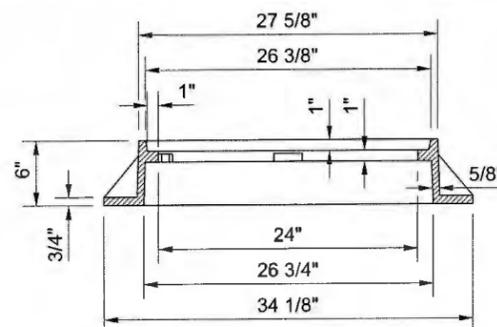
RING PLAN



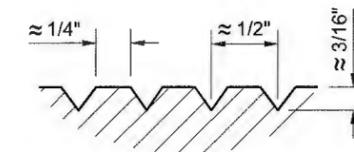
RING SECTION A



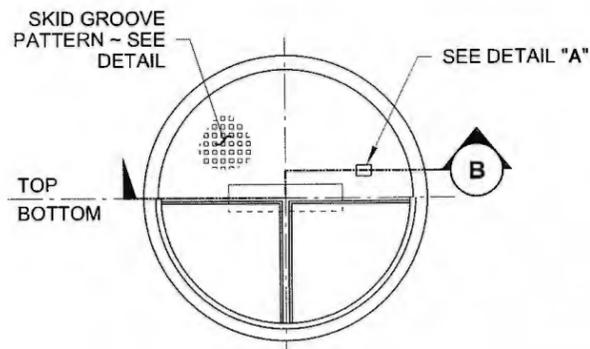
RING PLAN



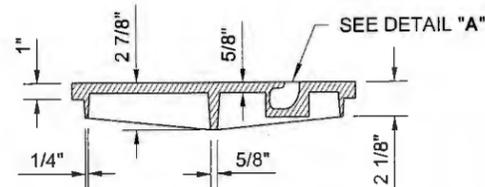
RING SECTION A



SKID GROOVE PATTERN  
DETAIL

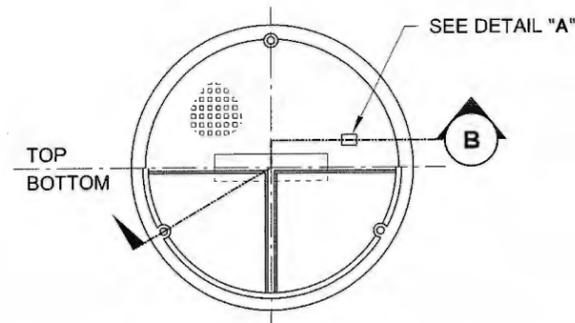


COVER PLAN

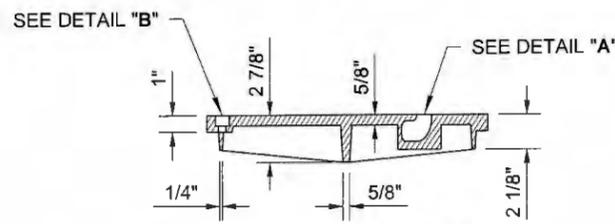


COVER SECTION B

(SEE NOTE 7)  
STANDARD  
TYPE 1

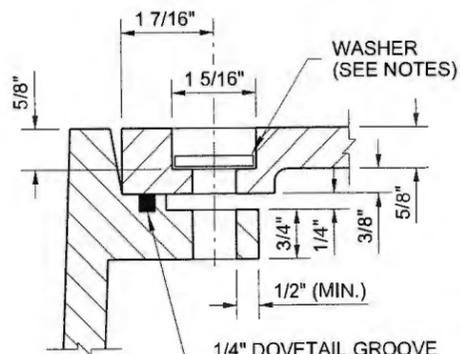


COVER PLAN

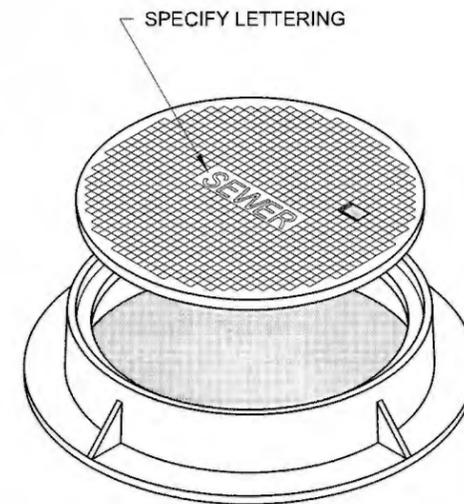


COVER SECTION B

(SEE NOTE 7)  
BOLT-DOWN / WATERTIGHT  
TYPE 2



BOLT-DOWN / WATERTIGHT  
DETAIL "B"



ISOMETRIC VIEW

NOTES

1. The gasket and groove may be in the seat (frame) or in the underside of the cover. The gasket may be "T" shaped in section. The groove may be cast or machined.
2. Bolt-down capability is required on all frames, grates, and covers, unless specified otherwise in the Contract. Provide 3 holes in the frame that are vertically aligned with the grate or cover slots. The frame shall accept the 5/8" - 1 NC x 2" Allen head cap screw by being tapped, or other approved mechanism. Location of bolt down holes varies by manufacturer.
3. For bolt-down manhole ring and covers that are not designated "Watertight," the neoprene gasket, groove, and washer are not required.
4. Washer shall be neoprene (Detail "B").
5. In lieu of blind pick notch for manhole covers, a single 1" pick hole is acceptable. Hole location and number of holes may vary by manufacturer.
6. Alternative reinforcing designs are acceptable in lieu of the rib design.
7. For clarity, the vertical scale of the Cover Section has been exaggerated, it is 1.5 times the horizontal scale (1H:1.5V).



4-25-12

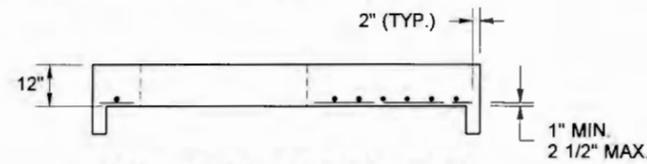
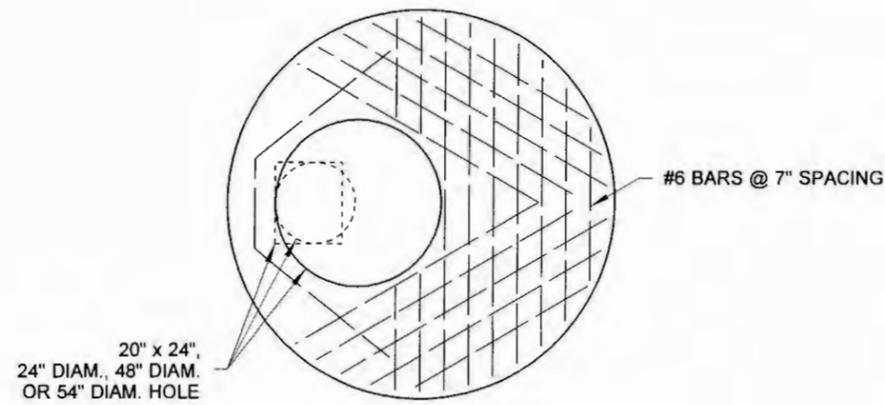
**CIRCULAR FRAME (RING)  
AND COVER**  
**STANDARD PLAN B-30.70-03**

SHEET 1 OF 1 SHEET

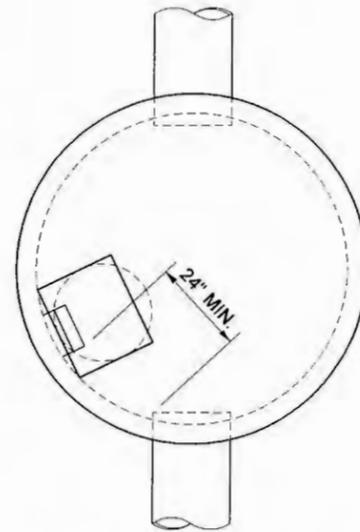
APPROVED FOR PUBLICATION  
*Pavel Babitsky* 4/25/12  
 STATE DESIGN ENGINEER DATE  
 Washington State Department of Transportation

**NOTE**

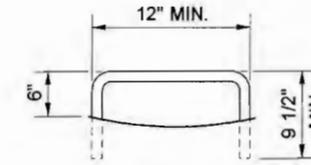
Ladder rungs for manholes and catch basins shall meet the requirements of AASHTO M 199.



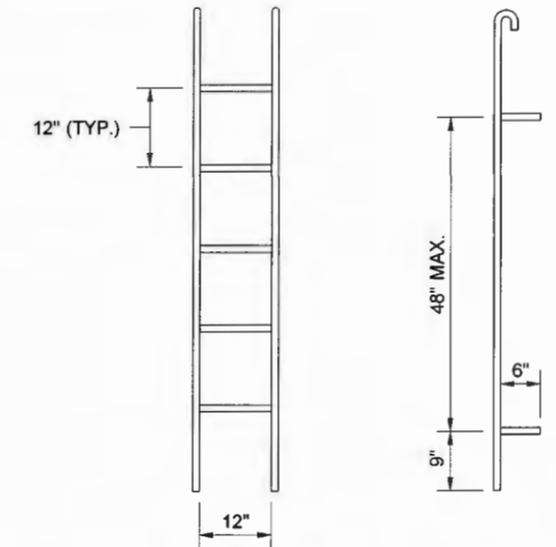
**84" or 96" FLAT SLAB TOP**



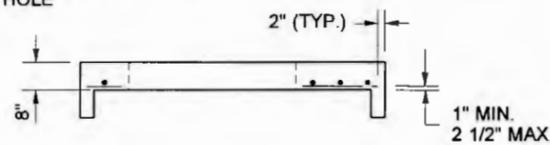
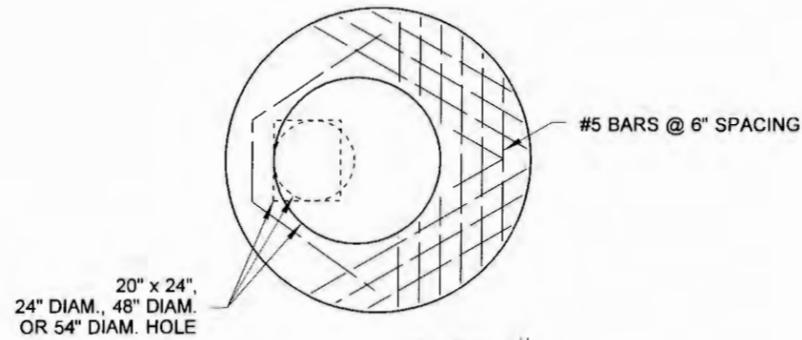
**TYPICAL ORIENTATION FOR ACCESS AND STEPS**



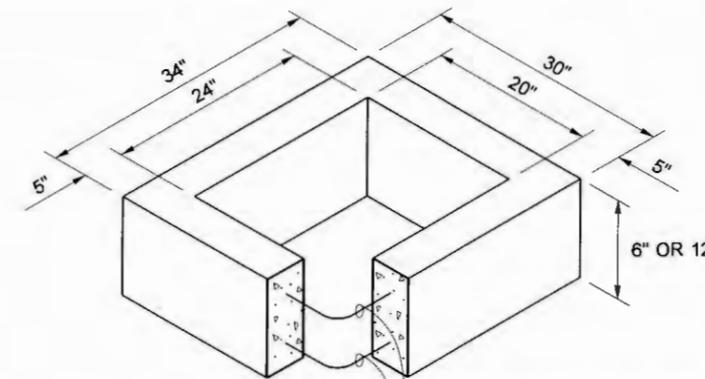
**STEP**



**PREFABRICATED LADDER**



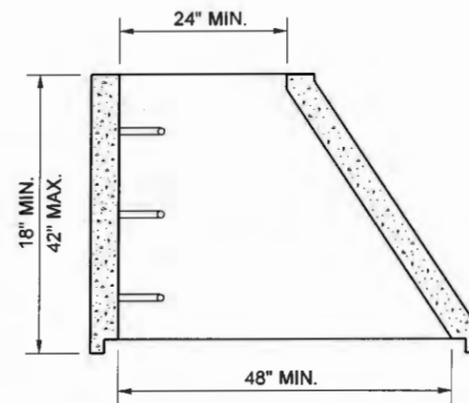
**72" FLAT SLAB TOP**



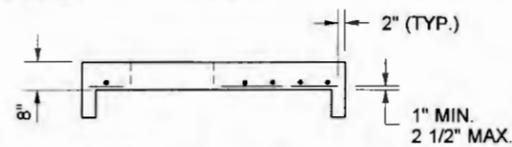
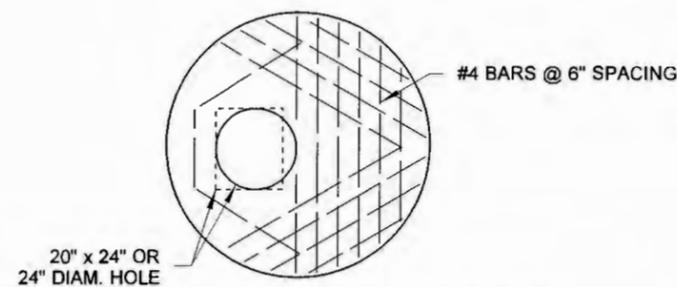
ONE #3 BAR HOOP FOR 6"  
TWO #3 BAR HOOPS FOR 12"

**RECTANGULAR ADJUSTMENT SECTION**

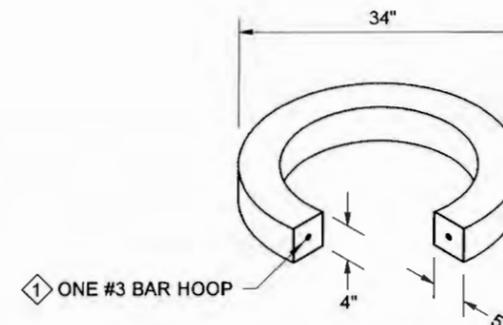
As an acceptable alternative to rebar, wire mesh having a minimum area of 0.12 square inches per foot may be used for adjustment sections.



**ECCENTRIC CONE SECTION**



**48", 54", or 60" FLAT SLAB TOP**



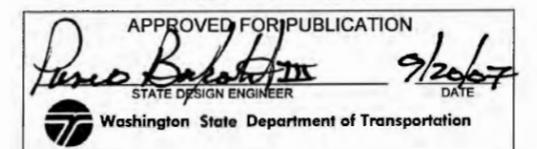
**CIRCULAR ADJUSTMENT SECTION**



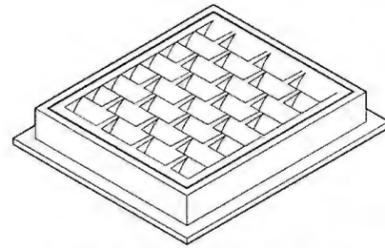
EXPIRES JULY 1, 2009

**MISCELLANEOUS DETAILS FOR DRAINAGE STRUCTURES STANDARD PLAN B-30.90-01**

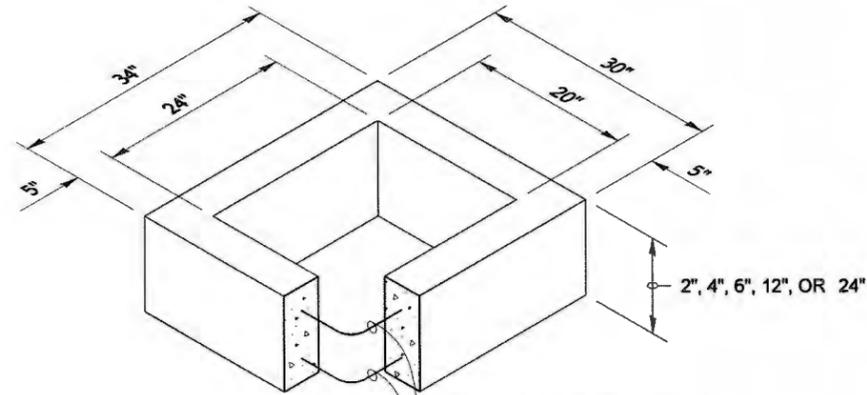
SHEET 1 OF 1 SHEET



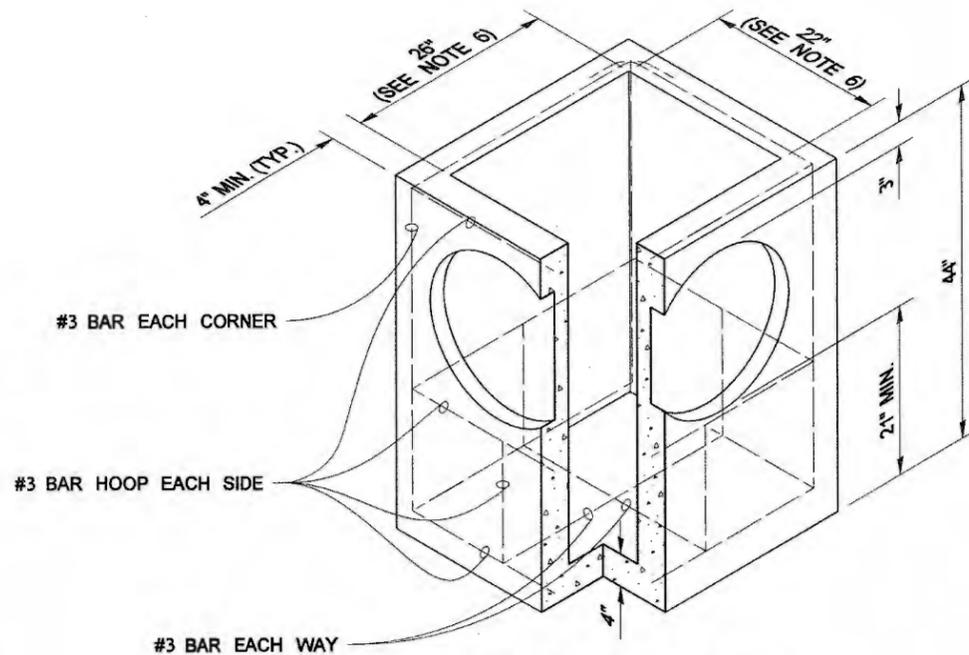
DRAWN BY: LISA CYFORD



FRAME AND VANED GRATE



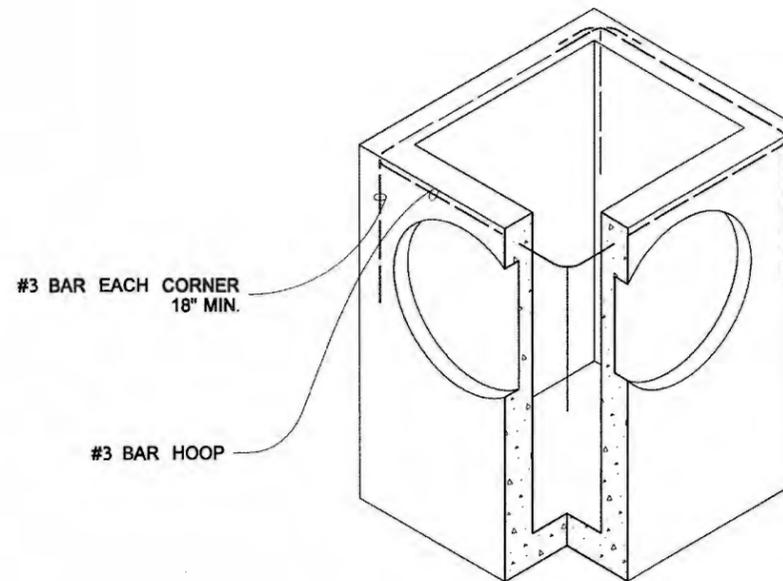
RECTANGULAR ADJUSTMENT SECTION



PRECAST BASE SECTION

| PIPE ALLOWANCES                          |                         |
|--|-------------------------|
| PIPE MATERIAL                            | MAXIMUM INSIDE DIAMETER |
| REINFORCED OR PLAIN CONCRETE             | 12"                     |
| ALL METAL PIPE                           | 15"                     |
| CPSSP * (STD. SPEC. 9-05.20)             | 12"                     |
| SOLID WALL PVC (STD. SPEC. 9-05.12(1))   | 15"                     |
| PROFILE WALL PVC (STD. SPEC. 9-05.12(2)) | 15"                     |

\* CORRUGATED POLYETHYLENE STORM SEWER PIPE



(SEE NOTE 1)  
ALTERNATIVE PRECAST BASE SECTION

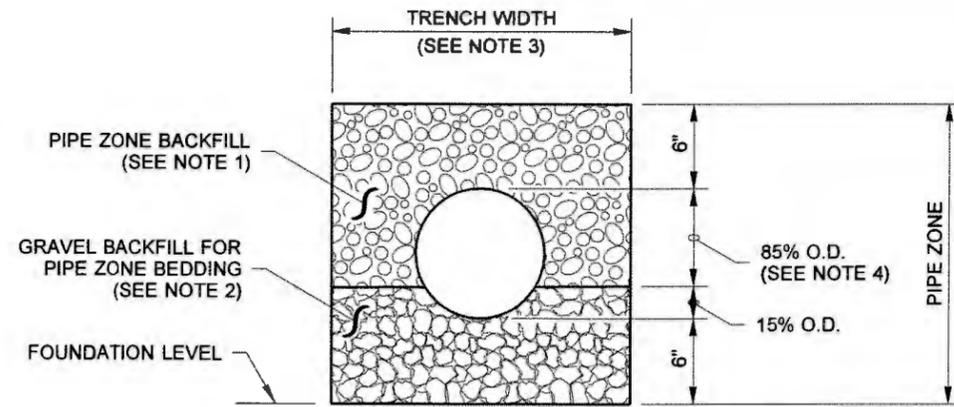
NOTES

1. As acceptable alternatives to the rebar shown in the PRECAST BASE SECTION, fibers (placed according to the Standard Specifications), or wire mesh having a minimum area of 0.12 square inches per foot shall be used with the minimum required rebar shown in the ALTERNATIVE PRECAST BASE SECTION. Wire mesh shall not be placed in the knockouts.
2. The knockout diameter shall not be greater than 20". Knockouts shall have a wall thickness of 2" minimum to 2.5" maximum. Provide a 1.5" minimum gap between the knockout wall and the outside of the pipe. After the pipe is installed, fill the gap with joint mortar in accordance with Standard Specification 9-04.3.
3. The maximum depth from the finished grade to the lowest pipe invert shall be 5'.
4. The frame and grate may be installed with the flange down, or integrally cast into the adjustment section with flange up.
5. The Precast Base Section may have a rounded floor, and the walls may be sloped at a rate of 1:24 or steeper.
6. The opening shall be measured at the top of the Precast Base Section.
7. All pickup holes shall be grouted full after the basin has been placed.

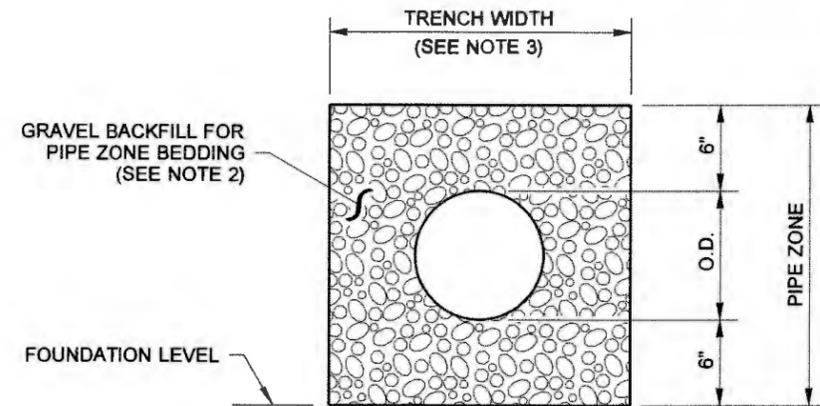


**CATCH BASIN TYPE 1**  
**STANDARD PLAN B-5.20-01**  
 SHEET 1 OF 1 SHEET

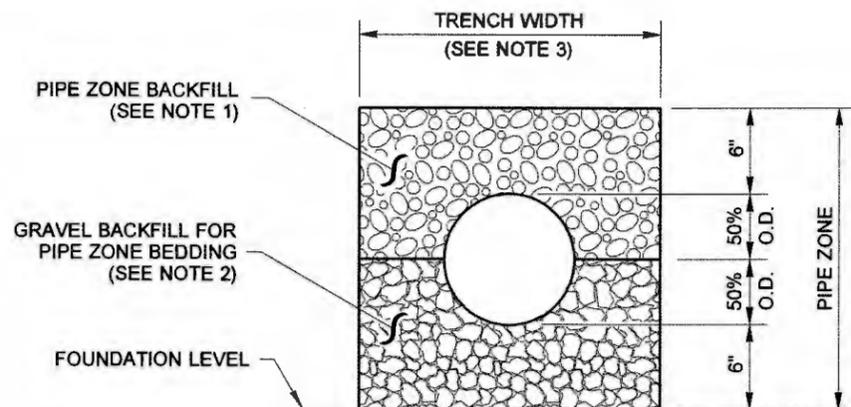
APPROVED FOR PUBLICATION  
*Paulo B. [Signature]* 6/16/11  
 STATE DESIGN ENGINEER DATE  
 Washington State Department of Transportation



**CONCRETE AND DUCTILE IRON PIPE**



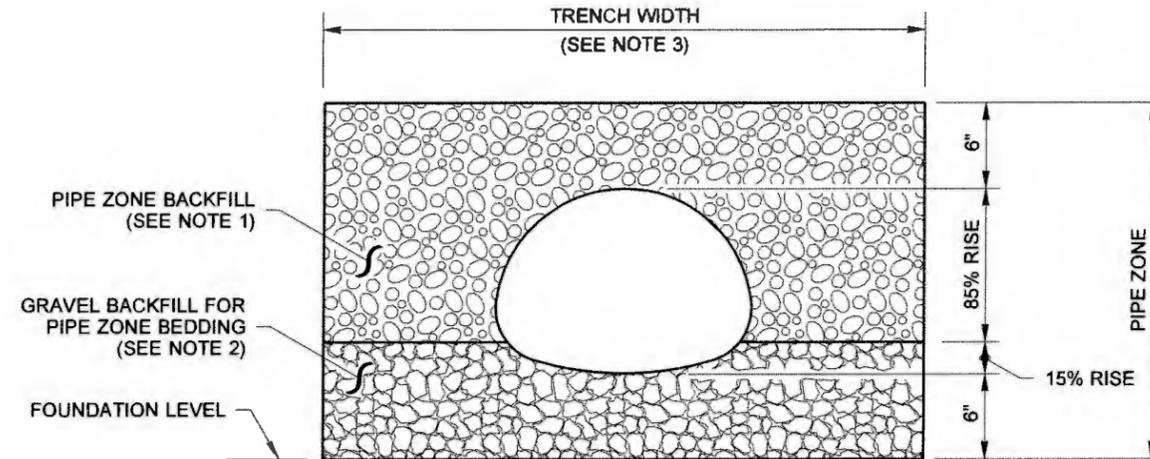
**THERMOPLASTIC PIPE**



**METAL PIPE**

**NOTES**

1. See Standard Specifications Section 7-08.3(3) for Pipe Zone Backfill.
2. See Standard Specifications Section 9-03.12(3) for Gravel Backfill for Pipe Zone Bedding.
3. See Standard Specifications Section 2-09.4 for Measurement of Trench Width.
4. For sanitary sewer installation, concrete pipe shall be bedded to spring line.



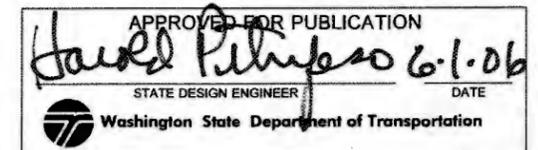
**PIPE ARCHES**

| CLEARANCE BETWEEN PIPES FOR MULTIPLE INSTALLATIONS |              |                                  |
|--|--------------|----------------------------------|
| PIPE   | SIZE         | MINIMUM DISTANCE BETWEEN BARRELS |
| CIRCULAR PIPE (DIAMETER)                           | 12" to 24"   | 12"                              |
|  | 30" to 96"   | DIAM. /2                         |
|  | 102" to 180" | 48"                              |
| PIPE ARCH (SPAN) METAL ONLY                        | 18" to 36"   | 12"                              |
|  | 43" to 142"  | SPAN /3                          |
|  | 148" to 200" | 48"                              |

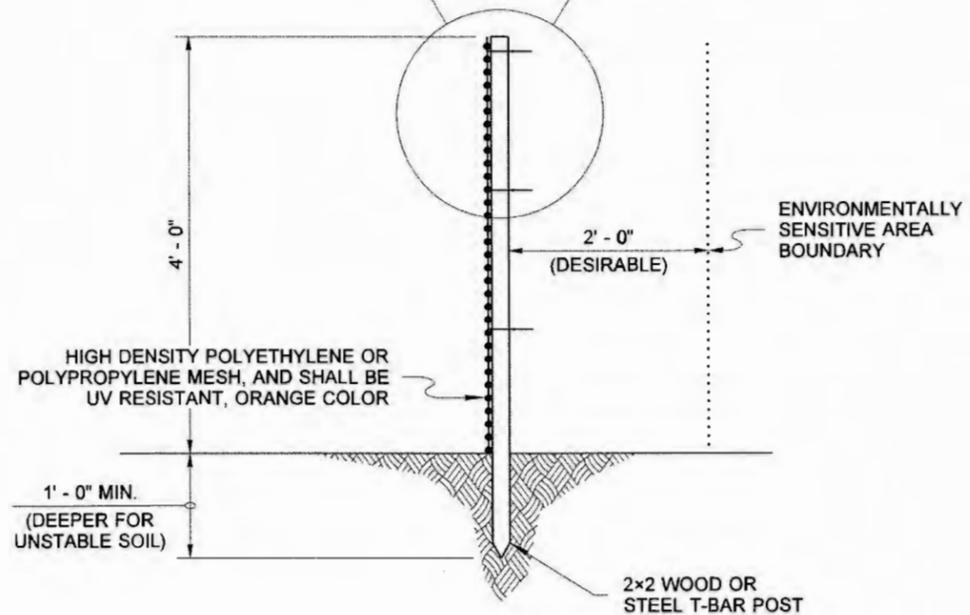
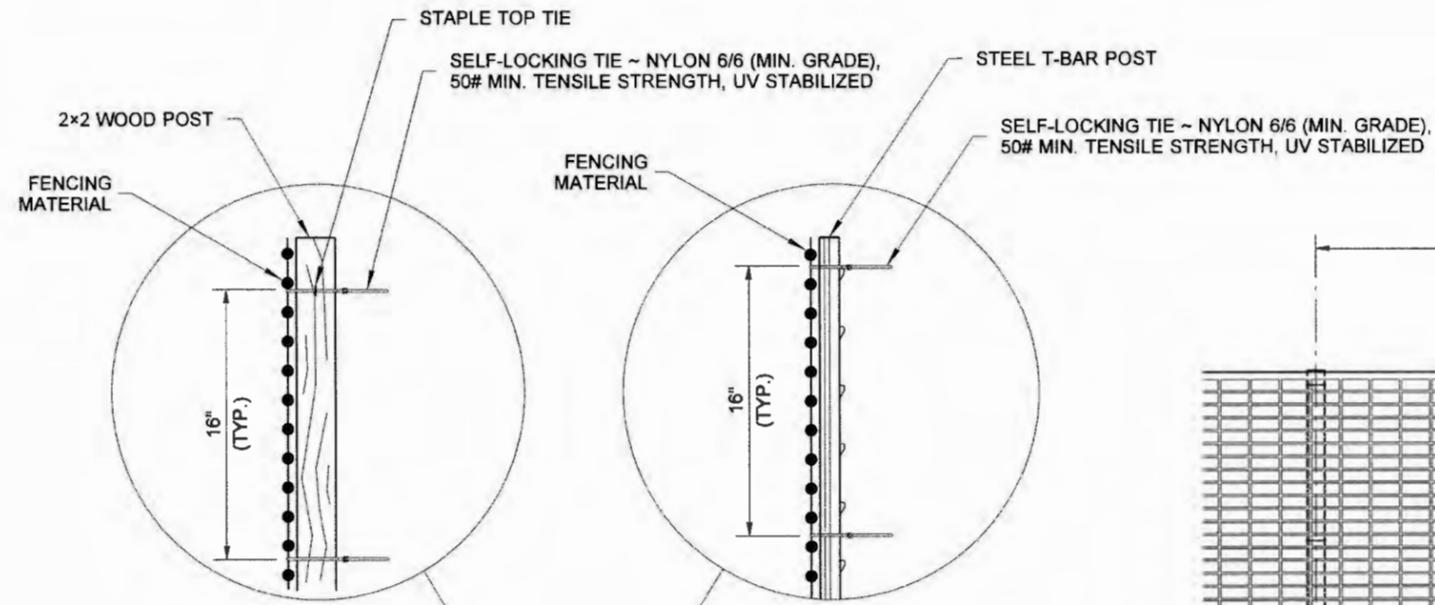


**PIPE ZONE BEDDING AND BACKFILL**  
**STANDARD PLAN B-55.20-00**

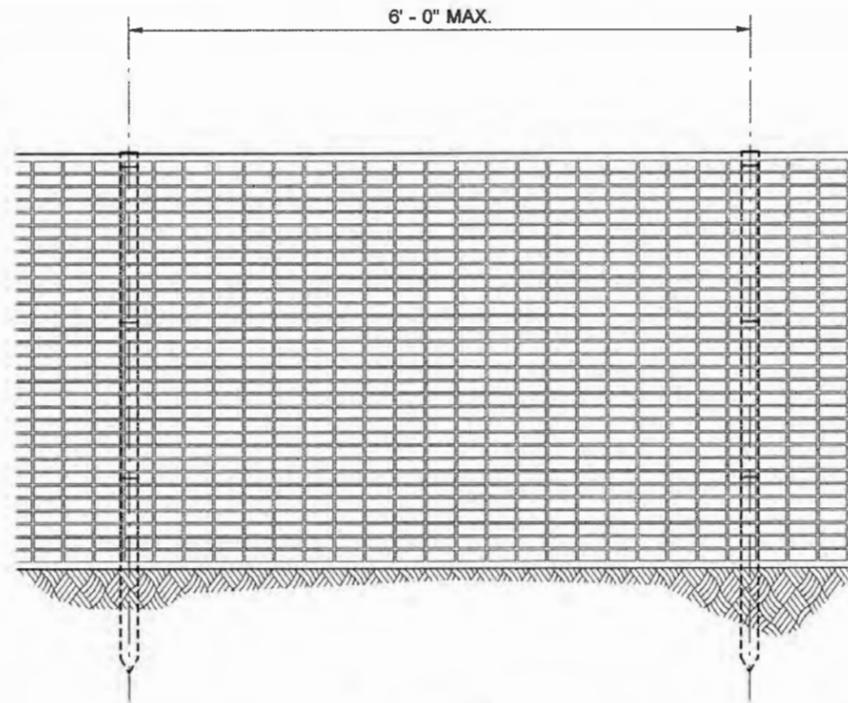
SHEET 1 OF 1 SHEET



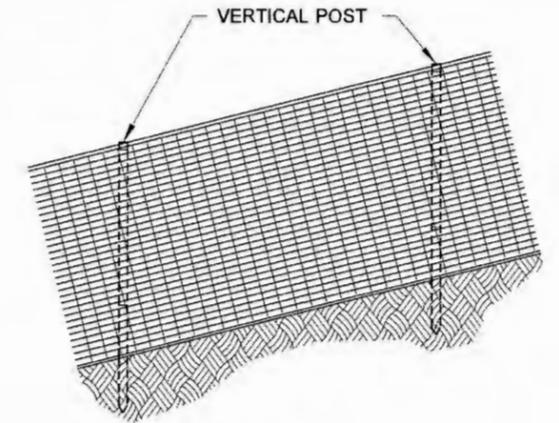
DRAWN BY: BILL BERENS



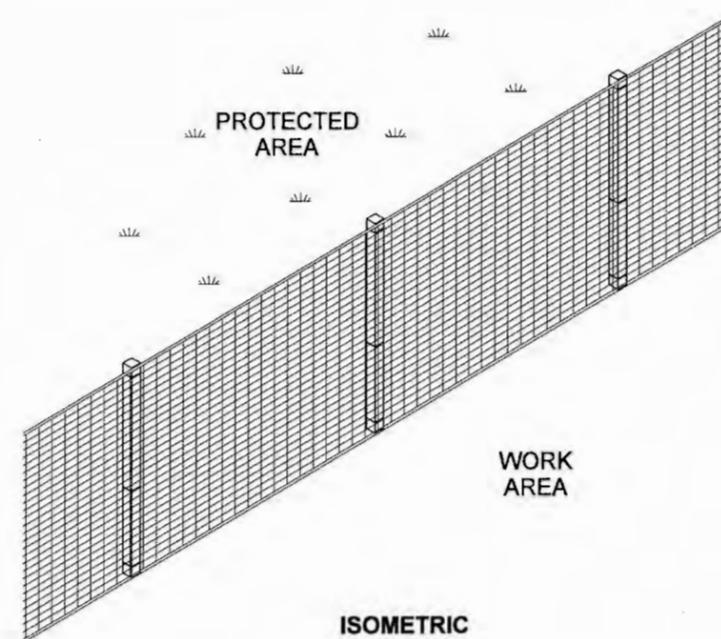
TYPICAL SECTION



ELEVATION



ELEVATION  
FENCE ON SLOPE



ISOMETRIC

NOTE

1. Post shall have sufficient strength and durability to support the fence through the life of the project.



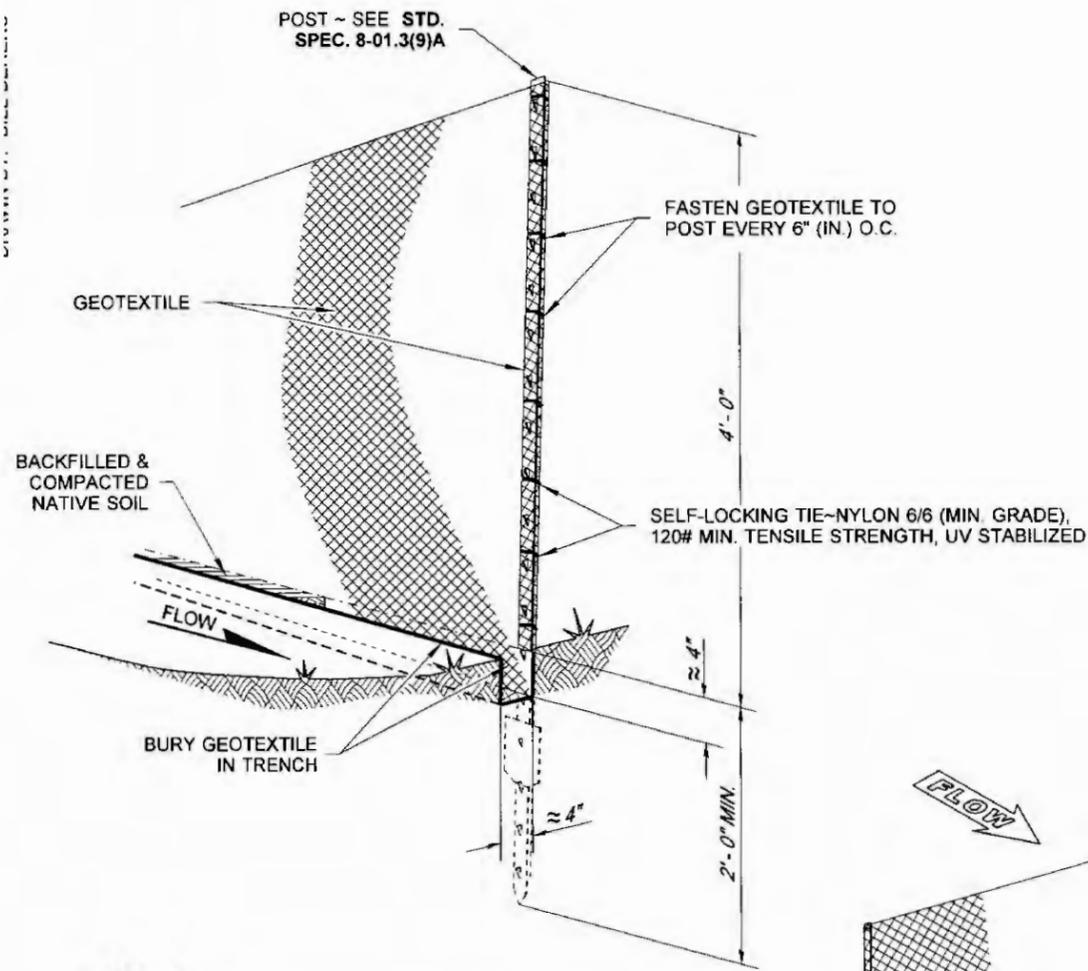
STATE OF WASHINGTON  
 REGISTERED  
 LANDSCAPE ARCHITECT  
*Mark W. Maurer*  
 MARK W. MAURER  
 CERTIFICATE NO. 000598  
 8/10/2009

HIGH VISIBILITY FENCE

STANDARD PLAN I-10.10-01

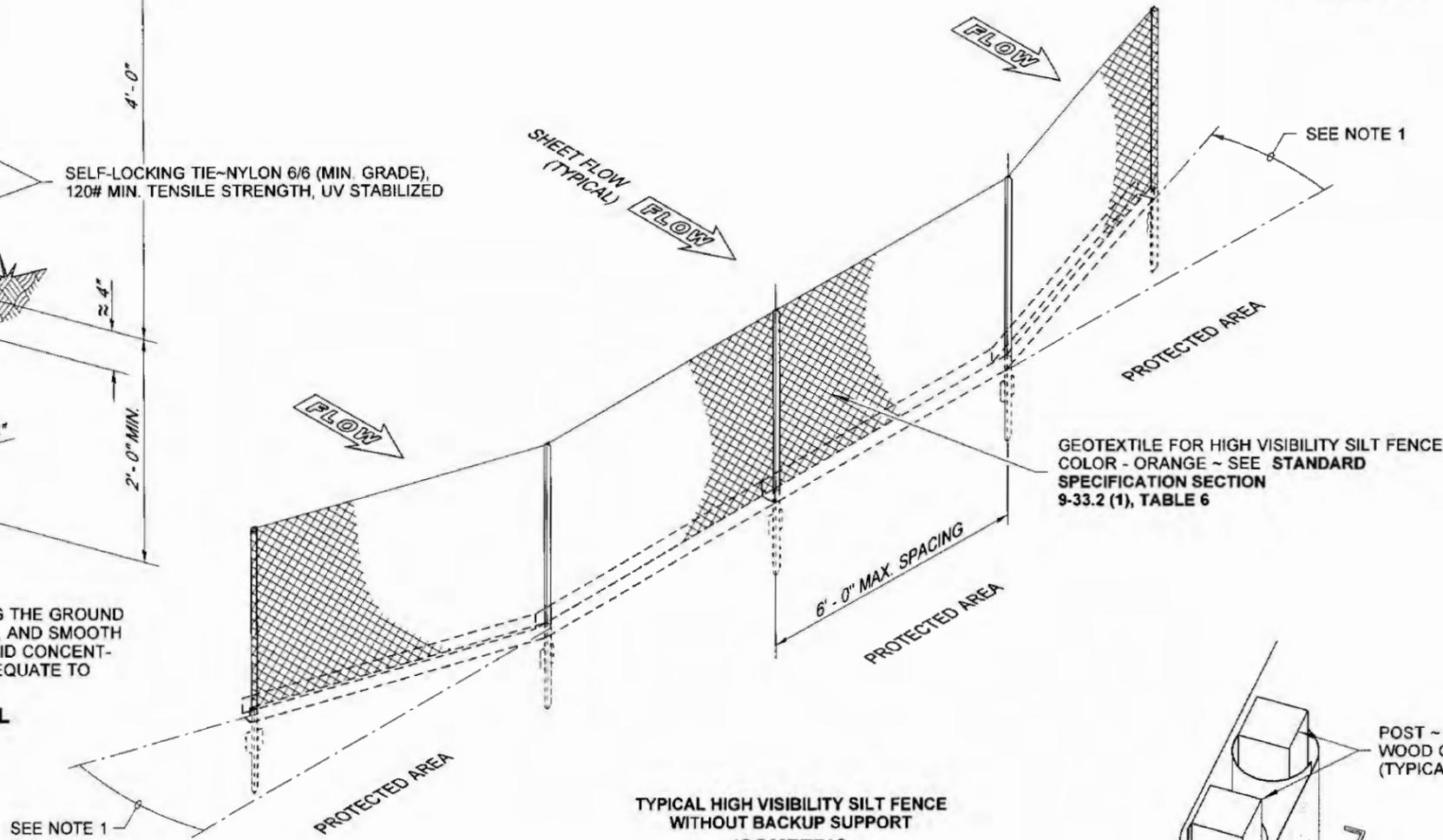
SHEET 1 OF 1 SHEET

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*Bill Berens* 8/10/09  
 STATE DESIGN ENGINEER DATE  
 Washington State Department of Transportation

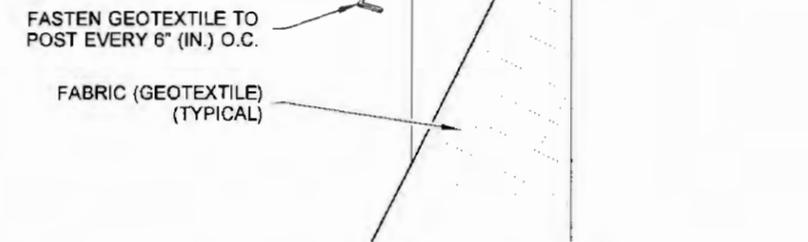


**NOTE**  
 DURING EXCAVATION, MINIMIZE DISTURBING THE GROUND AROUND TRENCH AS MUCH AS IS FEASIBLE, AND SMOOTH SURFACE FOLLOWING EXCAVATION TO AVOID CONCENTRATING FLOWS. COMPACTION MUST BE ADEQUATE TO PREVENT UNDERCUTTING FLOWS.

**TYPICAL INSTALLATION DETAIL**  
 (STEEL POSTS SHOWN)



**TYPICAL HIGH VISIBILITY SILT FENCE WITHOUT BACKUP SUPPORT ISOMETRIC**  
 (STEEL POSTS SHOWN)



**SPLICE DETAIL**  
 (WOOD POSTS SHOWN)

SPLICED FENCE SECTIONS SHALL BE CLOSE ENOUGH TOGETHER TO PREVENT SILT LADEN WATER FROM ESCAPING THROUGH THE FENCE AT THE OVERLAP. JOINING SECTIONS SHALL NOT BE PLACED IN LOW SPOTS OR IN SUMP LOCATIONS.

**NOTES**

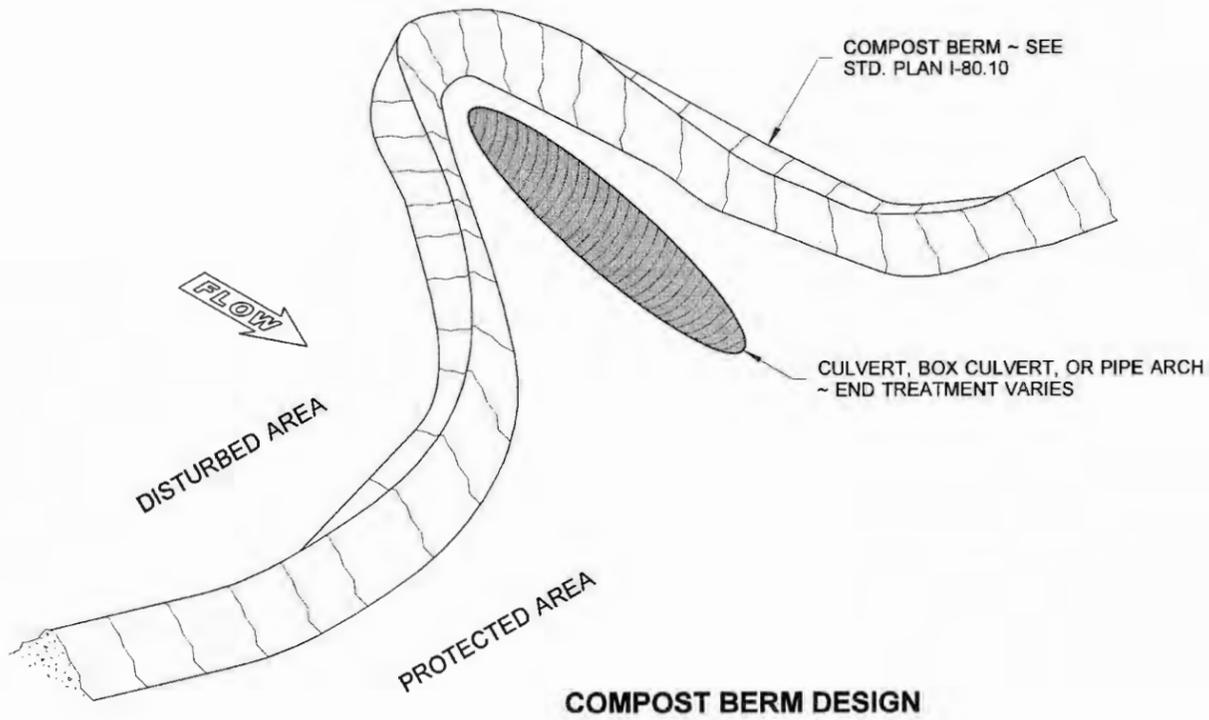
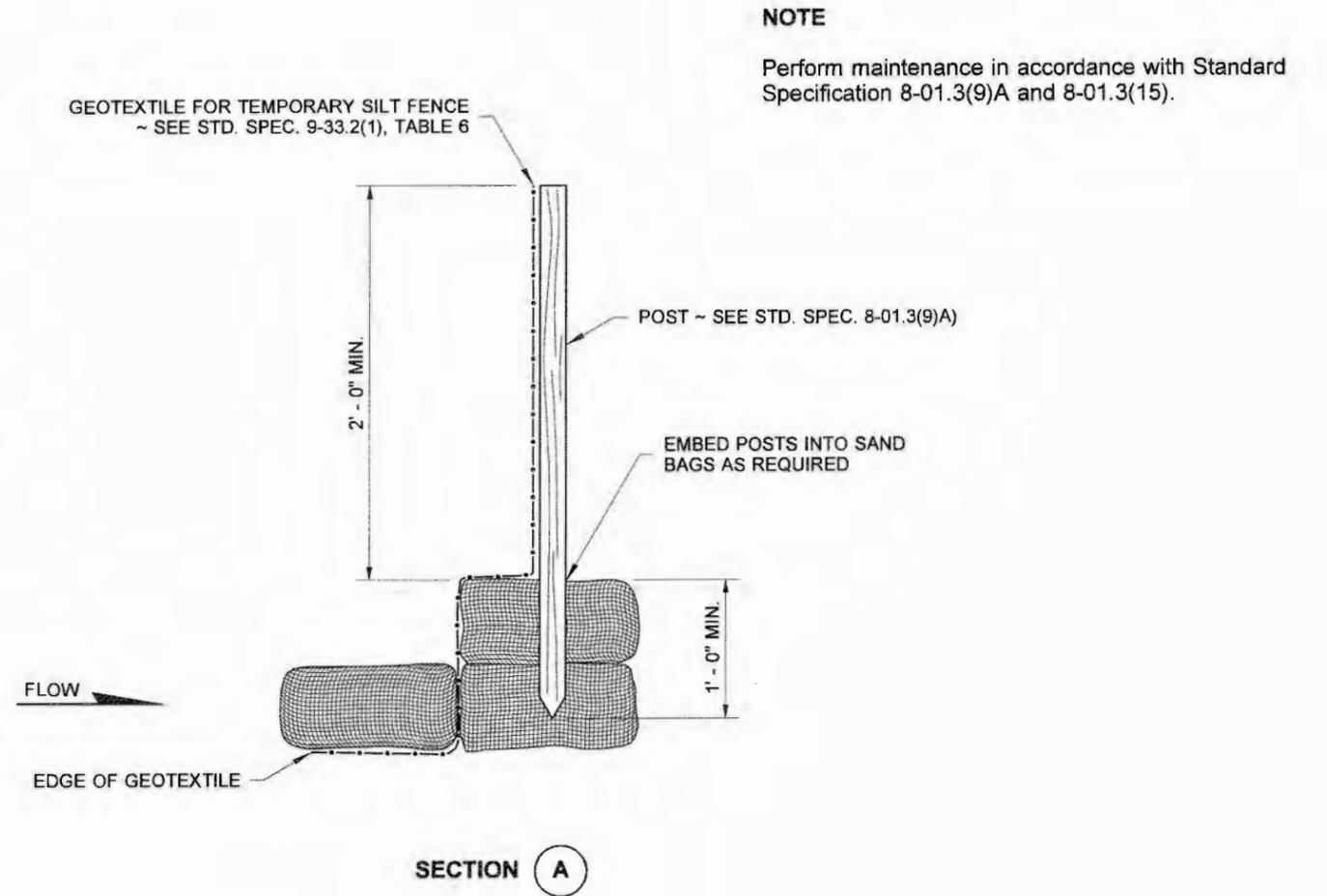
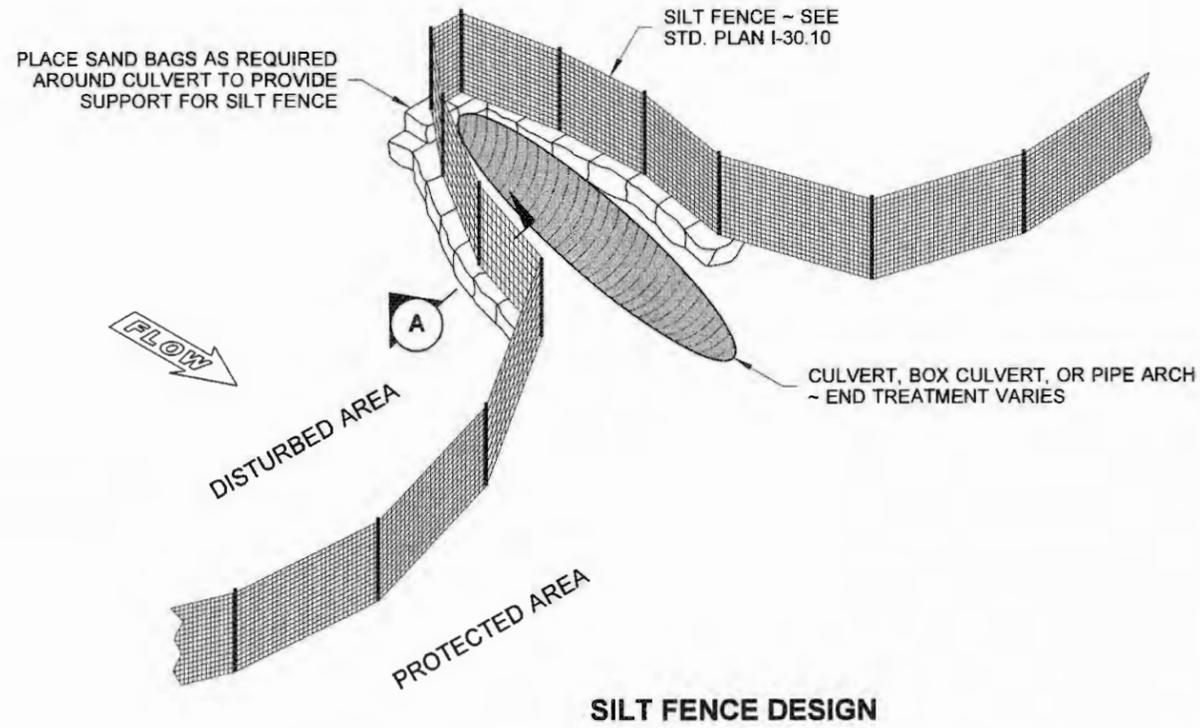
1. Install the ends of the high visibility silt fence to point slightly upslope to prevent sediment from flowing around the ends of the fence.
2. Perform maintenance in accordance with **Standard Specifications 8-01.3(9)A and 8-01.3(15)**.
3. Splices shall never be placed in low spots or sump locations. If splices are located in low or sump areas, the fence may need to be reinstalled unless the Project Engineer approves the installation.
4. Install silt fencing parallel to mapped contour lines.



STATE OF WASHINGTON  
 REGISTERED  
 LANDSCAPE ARCHITECT  
*Sandra L. Salisbury*  
 SANDRA L. SALISBURY  
 CERTIFICATE NO. 000860  
 March 11, 2013

**HIGH VISIBILITY SILT FENCE**  
**STANDARD PLAN I-30.17-00**  
 SHEET 1 OF 1 SHEET

APPROVED FOR PUBLICATION  
*Passo Bogatchev* 3/22/13  
 STATE DESIGN ENGINEER DATE  
 Washington State Department of Transportation



STATE OF WASHINGTON  
REGISTERED  
LANDSCAPE ARCHITECT

*Mark W. Maurer*  
MARK W. MAURER  
CERTIFICATE NO. 000598  
9/20/07

**EROSION CONTROL  
AT CULVERT ENDS**

**STANDARD PLAN I-30.20-00**

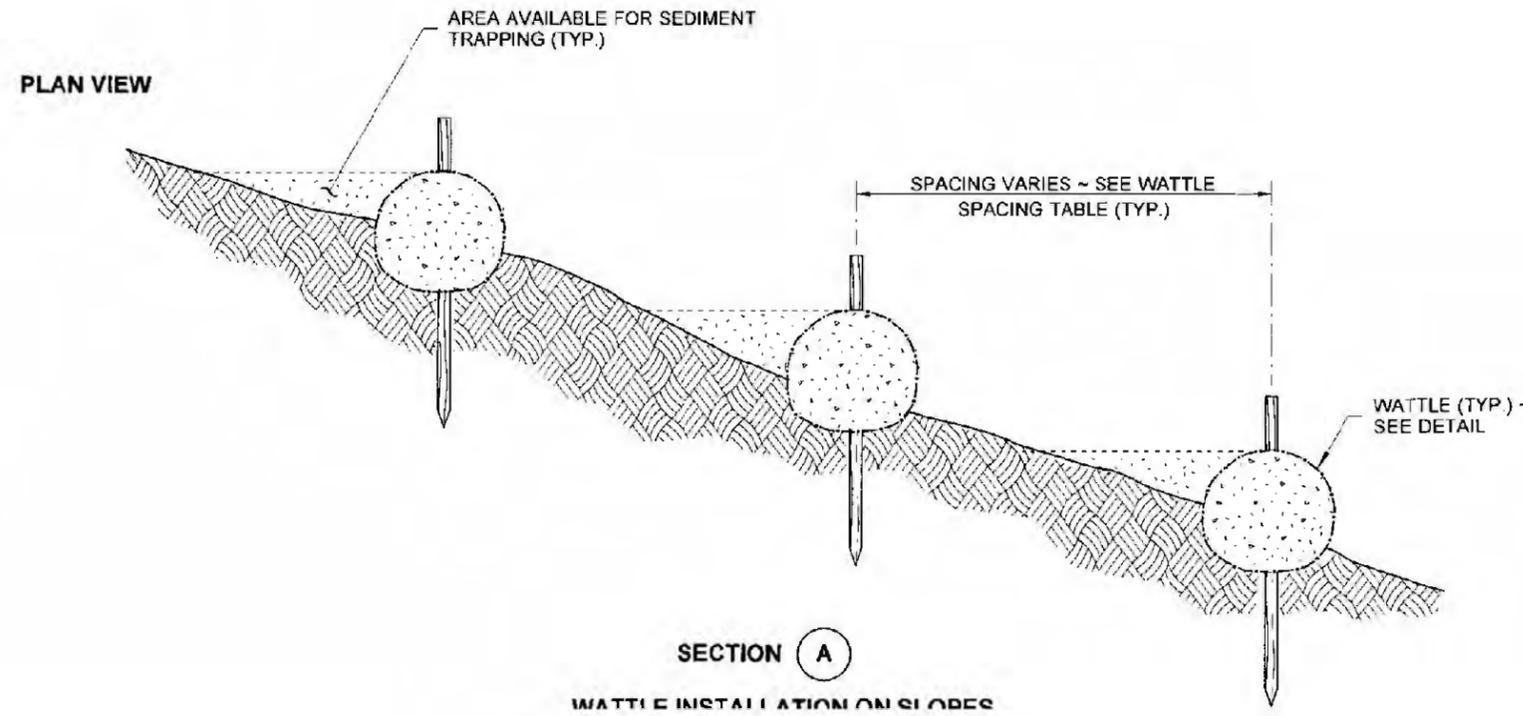
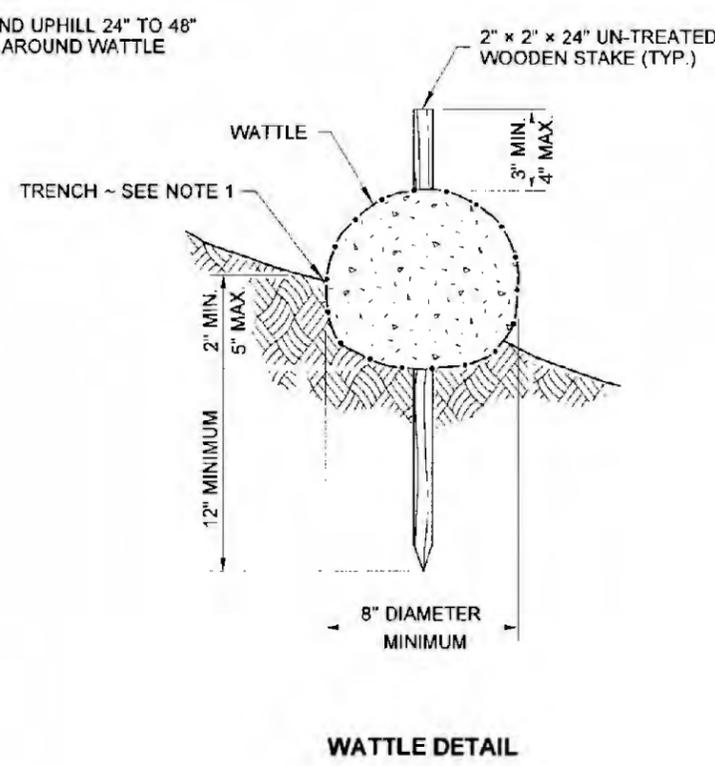
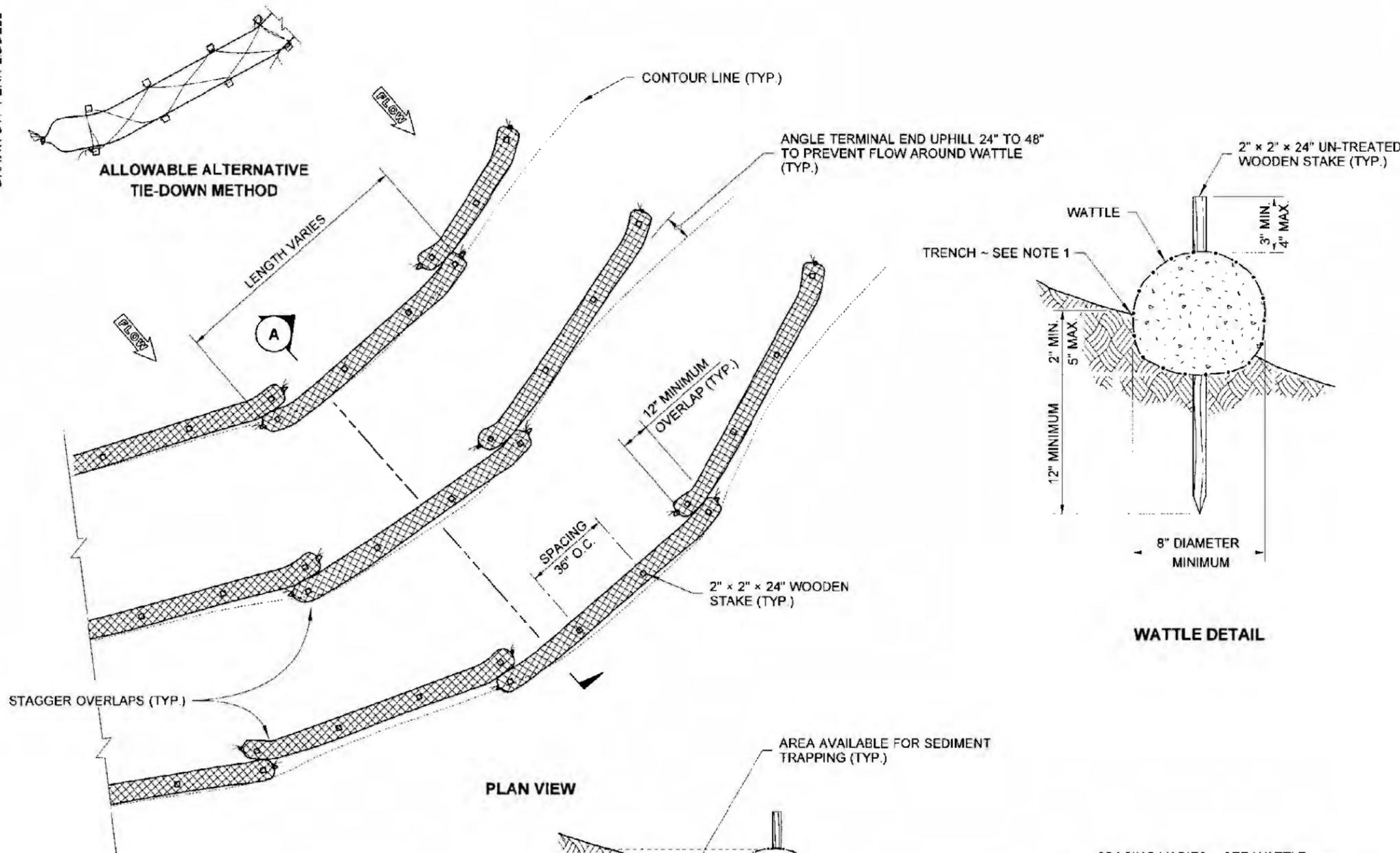
SHEET 1 OF 1 SHEET

APPROVED FOR PUBLICATION

*David Balchunas* 9/20/07  
STATE DESIGN ENGINEER DATE

Washington State Department of Transportation

DRAWN BY: FERN LIDDELL



| 8" DIAMETER WATTLE SPACING TABLE |                 |
|----------------------------------|-----------------|
| SLOPE                            | MAXIMUM SPACING |
| 1H : 1V                          | 10' - 0"        |
| 2H : 1V                          | 20' - 0"        |
| 3H : 1V                          | 30' - 0"        |
| 4H : 1V                          | 40' - 0"        |

**NOTES**

1. Wattles shall be in accordance with **Standard Specification 9-14.5(5)**. Install Wattles along contours. Installation shall be in accordance with **Standard Specification 8-01.3(10)**.
2. Securely knot each end of Wattle. Overlap adjacent Wattle ends 12" behind one another and securely tie together.
3. Compact excavated soil and trenches to prevent undercutting. Additional staking may be necessary to prevent undercutting.
4. Install Wattle perpendicular to flow along contours.
5. Wattles shall be inspected regularly, and immediately after a rainfall produces runoff, to ensure they remain thoroughly entrenched and in contact with the soil.
6. Perform maintenance in accordance with **Standard Specification 8-01.3(15)**.
7. Refer to **Standard Specification 8-01.3(16)** for removal.

STATE OF WASHINGTON  
REGISTERED  
LANDSCAPE ARCHITECT  
*Sandra L. Salisbury*  
SANDRA L. SALISBURY  
LICENSE NO. 860  
DATE: 10 June 2013

NOTE: THIS PLAN IS NOT A LEGAL ENGINEERING DOCUMENT BUT AN ELECTRONIC DUPLICATE. THE ORIGINAL, SIGNED BY THE ENGINEER AND APPROVED FOR PUBLICATION, IS KEPT ON FILE AT THE WASHINGTON STATE DEPARTMENT OF TRANSPORTATION. A COPY MAY BE OBTAINED UPON REQUEST.

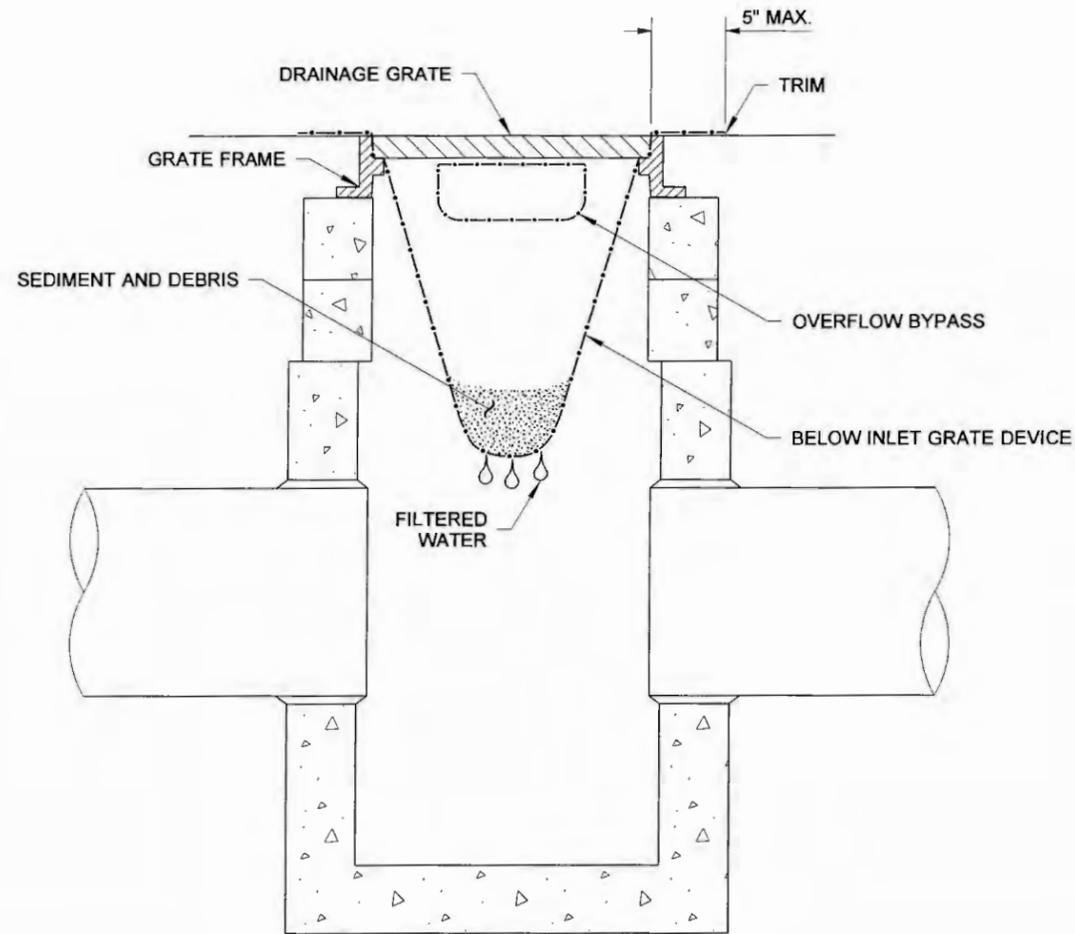
**WATTLE INSTALLATION ON SLOPE**  
**STANDARD PLAN I-30.30-01**  
SHEET 1 OF 1 SHEET

APPROVED FOR PUBLICATION  
*Pavel B. Bolshakov* 4/10/13  
STATE DESIGN ENGINEER DATE  
Washington State Department of Transportation

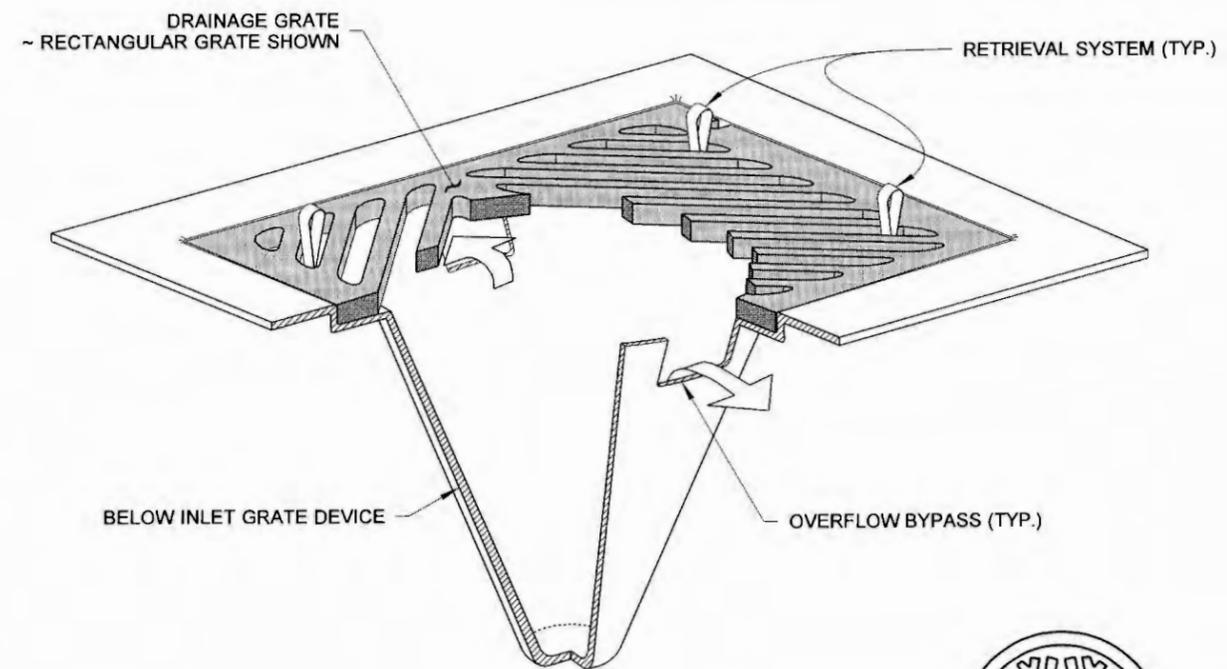
SECTION A  
WATTLE INSTALLATION ON SLOPES

**NOTES**

1. Size the Below Inlet Grate Device (BIGD) for the storm water structure it will service.
2. The BIGD shall have a built-in high-flow relief system (overflow bypass).
3. The retrieval system must allow removal of the BIGD without spilling the collected material.
4. Perform maintenance in accordance with Standard Specification 8-01.3(15).



**SECTION VIEW**  
NOT TO SCALE



**ISOMETRIC VIEW**

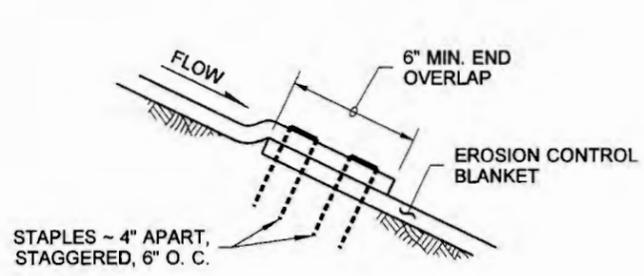


STATE OF WASHINGTON  
REGISTERED  
LANDSCAPE ARCHITECT  
*Mark W. Maurer*  
MARK W. MAURER  
CERTIFICATE NO. 000598  
9/20/07

**STORM DRAIN  
INLET PROTECTION  
STANDARD PLAN I-40.20-00**

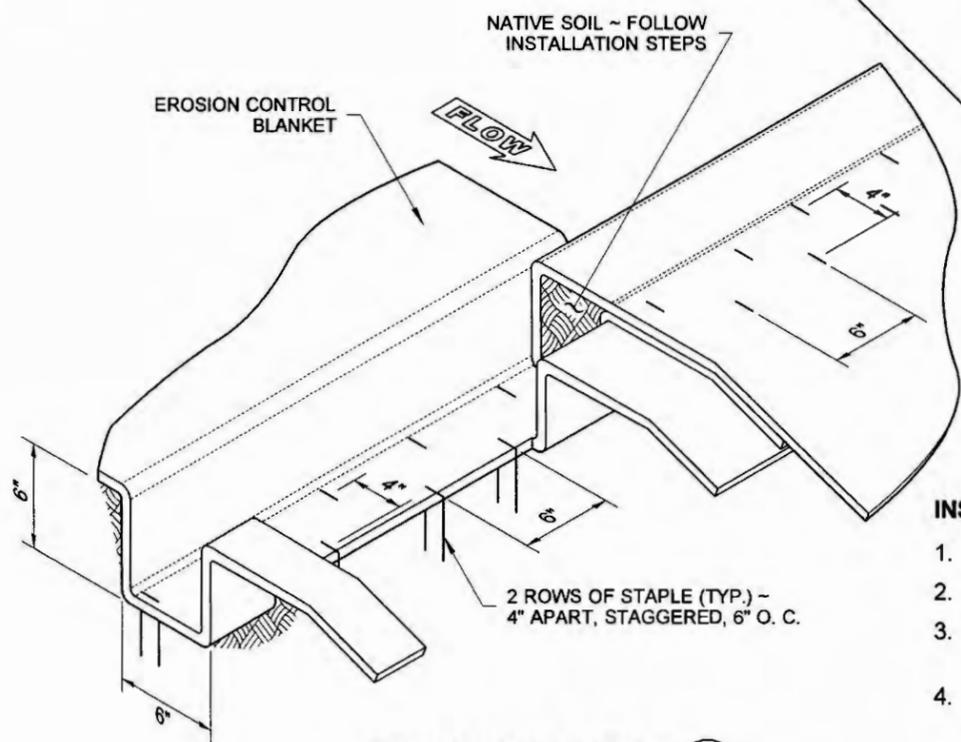
SHEET 1 OF 1 SHEET

APPROVED FOR PUBLICATION  
*Dino Balatras* 9/20/07  
STATE DESIGN ENGINEER DATE  
Washington State Department of Transportation



SHINGLE SPLICE - SECTION A

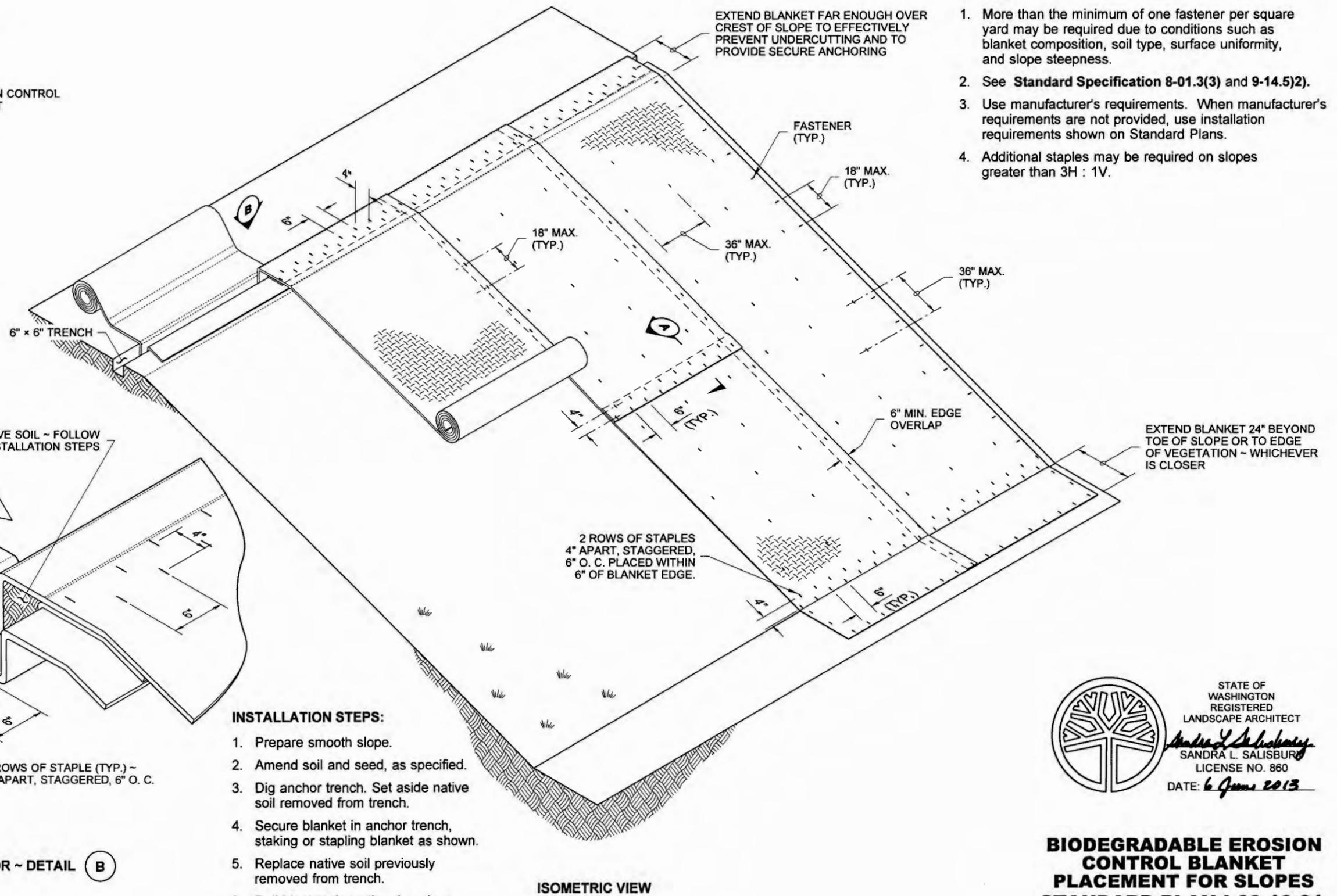
DRAWN BY: LISA CYFORD



INITIAL ANCHOR - DETAIL B

**INSTALLATION STEPS:**

1. Prepare smooth slope.
2. Amend soil and seed, as specified.
3. Dig anchor trench. Set aside native soil removed from trench.
4. Secure blanket in anchor trench, staking or stapling blanket as shown.
5. Replace native soil previously removed from trench.
6. Roll blanket down the slope in a controlled manner, taking care to remove excess slack, and taking care not to stretch blanket.
7. Stake or staple blanket as shown so there are no gaps between the blanket and the soil. Staple while unrolling blanket to minimize walking on blanket.



ISOMETRIC VIEW

**NOTES**

1. More than the minimum of one fastener per square yard may be required due to conditions such as blanket composition, soil type, surface uniformity, and slope steepness.
2. See **Standard Specification 8-01.3(3) and 9-14.5(2)**.
3. Use manufacturer's requirements. When manufacturer's requirements are not provided, use installation requirements shown on Standard Plans.
4. Additional staples may be required on slopes greater than 3H : 1V.

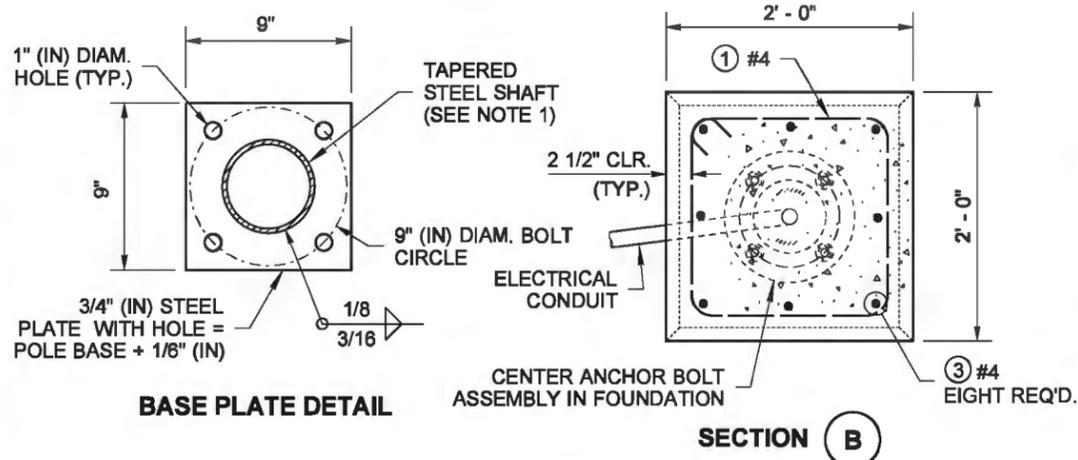
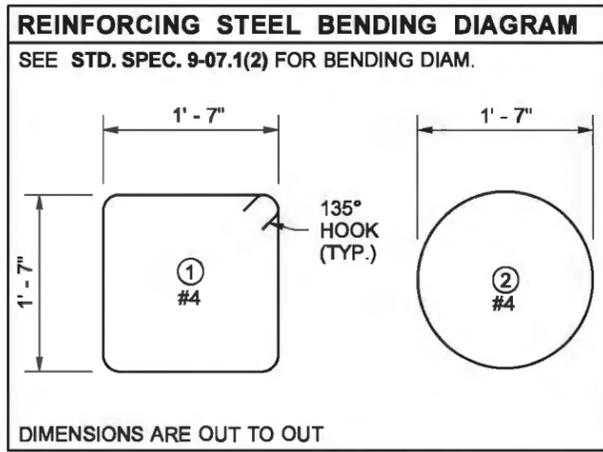


STATE OF WASHINGTON  
REGISTERED  
LANDSCAPE ARCHITECT  
*Sandra L. Salisbury*  
SANDRA L. SALISBURY  
LICENSE NO. 860  
DATE: 6 June 2013

**BIODEGRADABLE EROSION CONTROL BLANKET PLACEMENT FOR SLOPES STANDARD PLAN I-60.10-01**

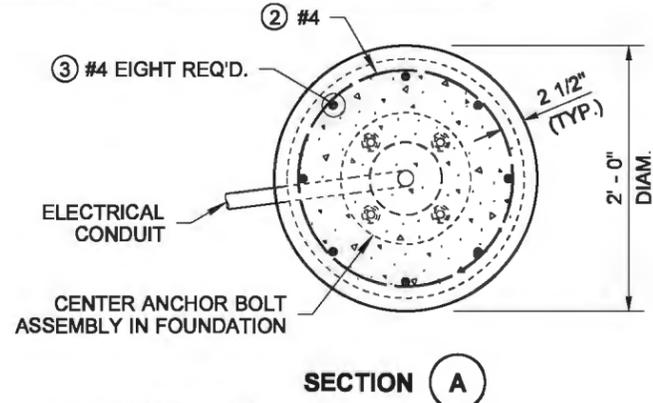
SHEET 1 OF 1 SHEET

APPROVED FOR PUBLICATION  
*Pamela R. [Signature]* 6/19/13  
STATE DESIGN ENGINEER DATE  
Washington State Department of Transportation

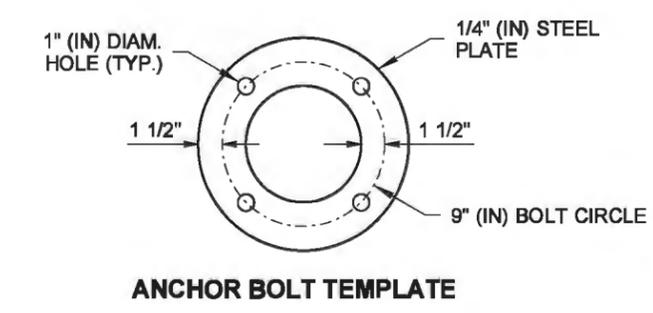
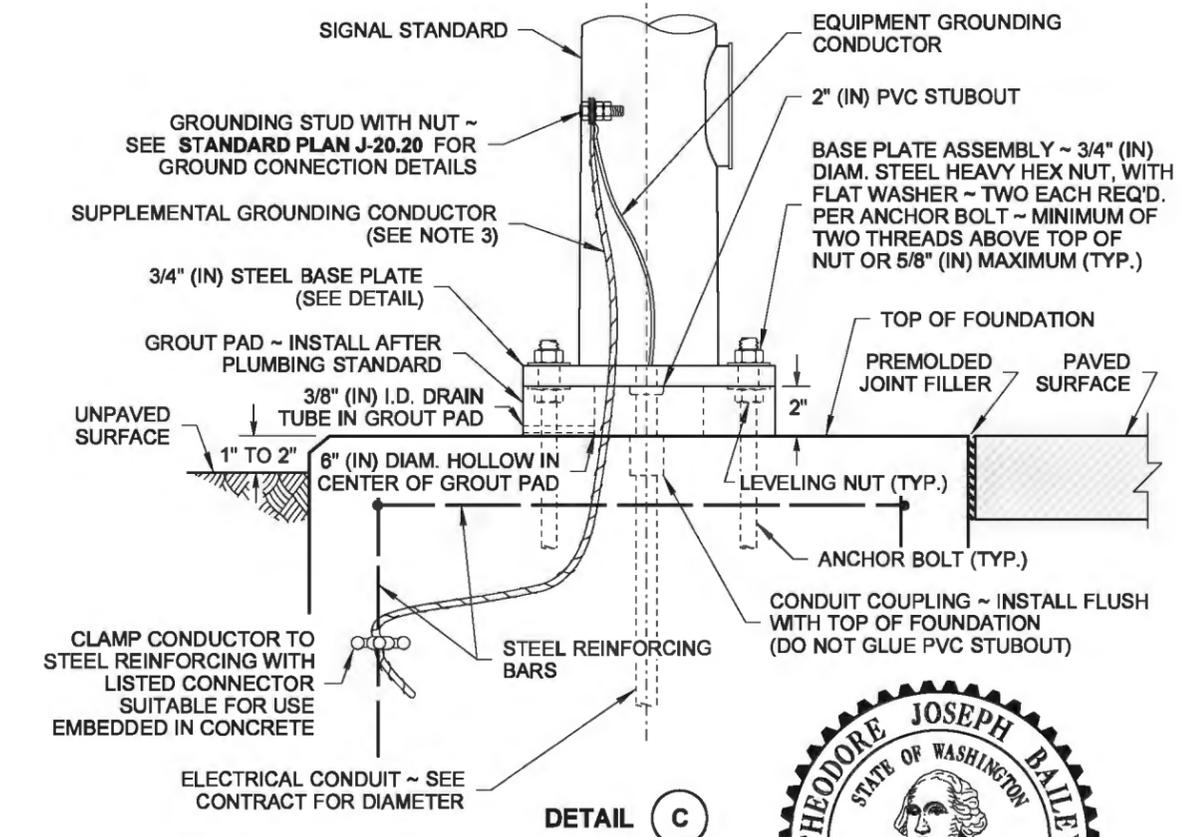
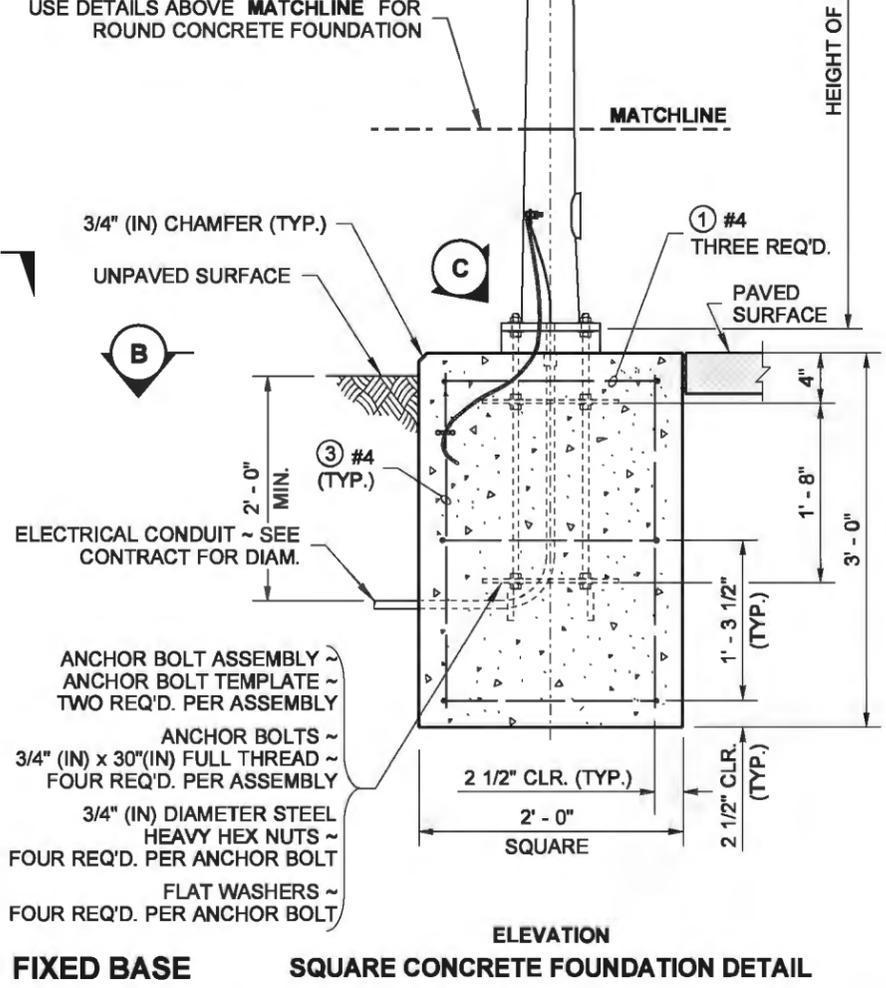
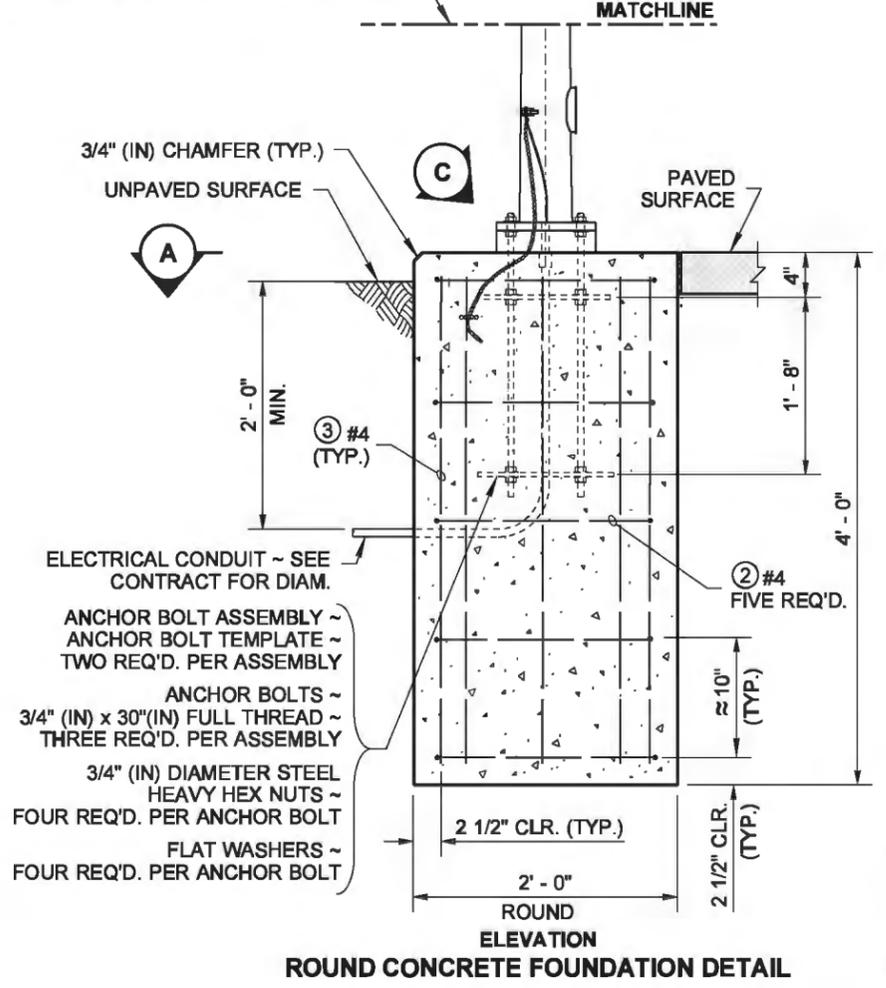


- NOTES**
- Clamping bolts shall be tightened to 50 ft-lbs max. torque. After state inspection, burr threads to prevent nut rotation. **DO NOT OVERTIGHTEN.**
  - The final height of the Anchor Bolts shall be below the top of the slip plate assembly to ensure proper function of the slip base.
  - Supplemental grounding conductor shall be non-insulated #4 AWG stranded copper and shall be clamped to vertical rebar with a connector suitable for use embedded in concrete; Provide 3' - 0" min. slack. Attach to pole grounding stud with a full circle crimp-on connector (crimped with a manufacturer recommended crimper).
  - Junction box serving the Standard shall preferably be located 5' - 0" (10' - 0" Max.) from the Standard.
  - Provide cable tie at wiring entering the junction box (for slip base installations only) ~ See **Detail A, Standard Plan J-28.70.**
  - Keeper Plate shall not extend beyond the edges of the pole base plate.

DRAWN BY: FERN LIDDELL



SEE DETAILS ABOVE **MATCHLINE** FOR SQUARE CONCRETE FOUNDATION



THEODORE JOSEPH BAILEY  
STATE OF WASHINGTON  
REGISTERED ENGINEER  
39820

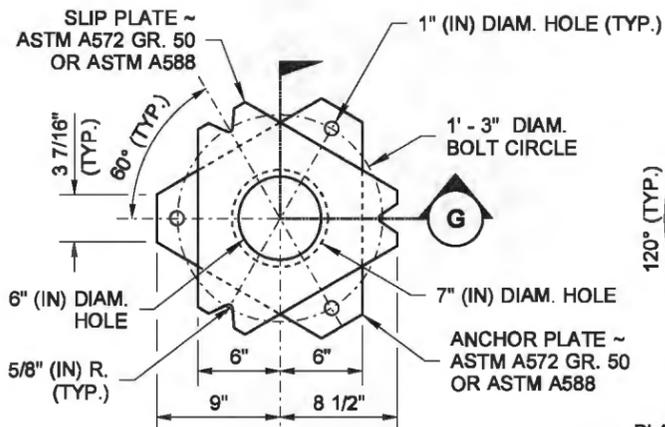
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Jun 26 2014 4:29 PM

**TYPE PS, TYPE 1, RM & FB SIGNAL STANDARD FOUNDATION DETAILS**  
**STANDARD PLAN J-21.10-04**

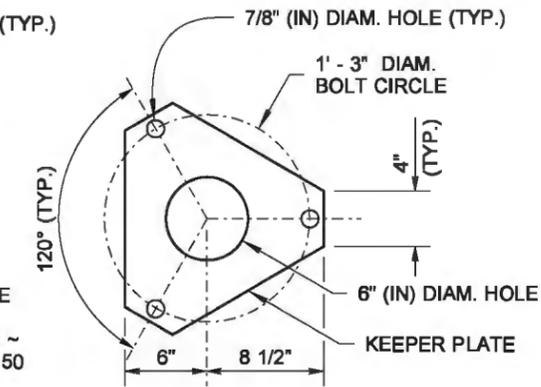
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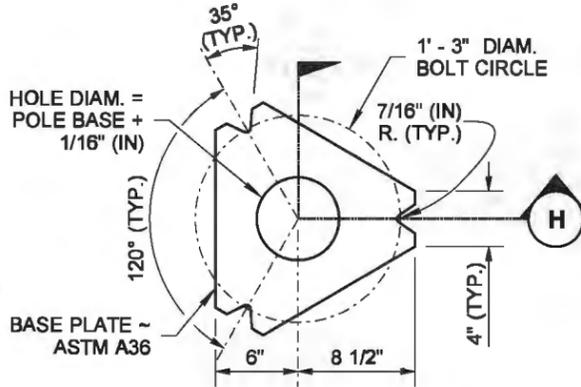
STATE DESIGN ENGINEER  
Washington State Department of Transportation



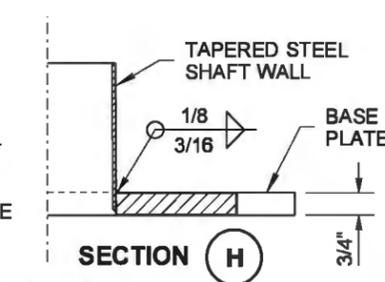
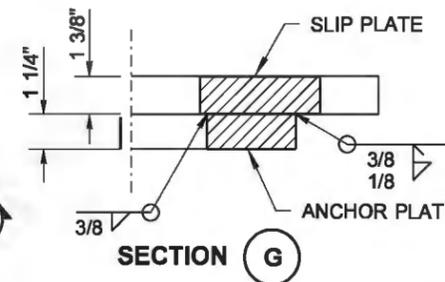
SLIP AND ANCHOR PLATES DETAIL



KEEPER PLATE DETAIL



BASE PLATE DETAIL



DRAWN BY: FERN LIDDELL

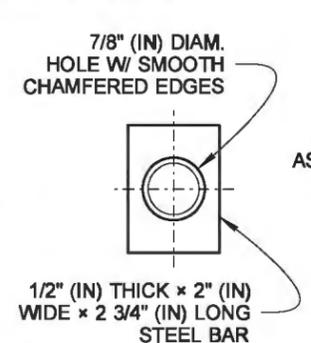
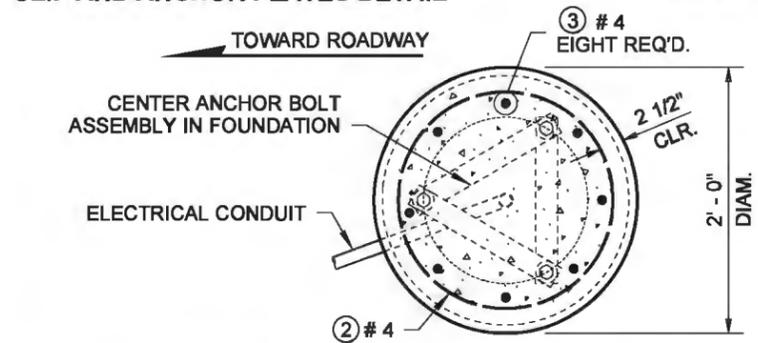
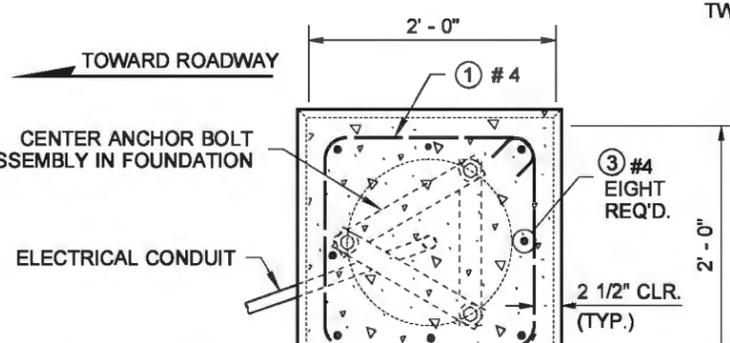
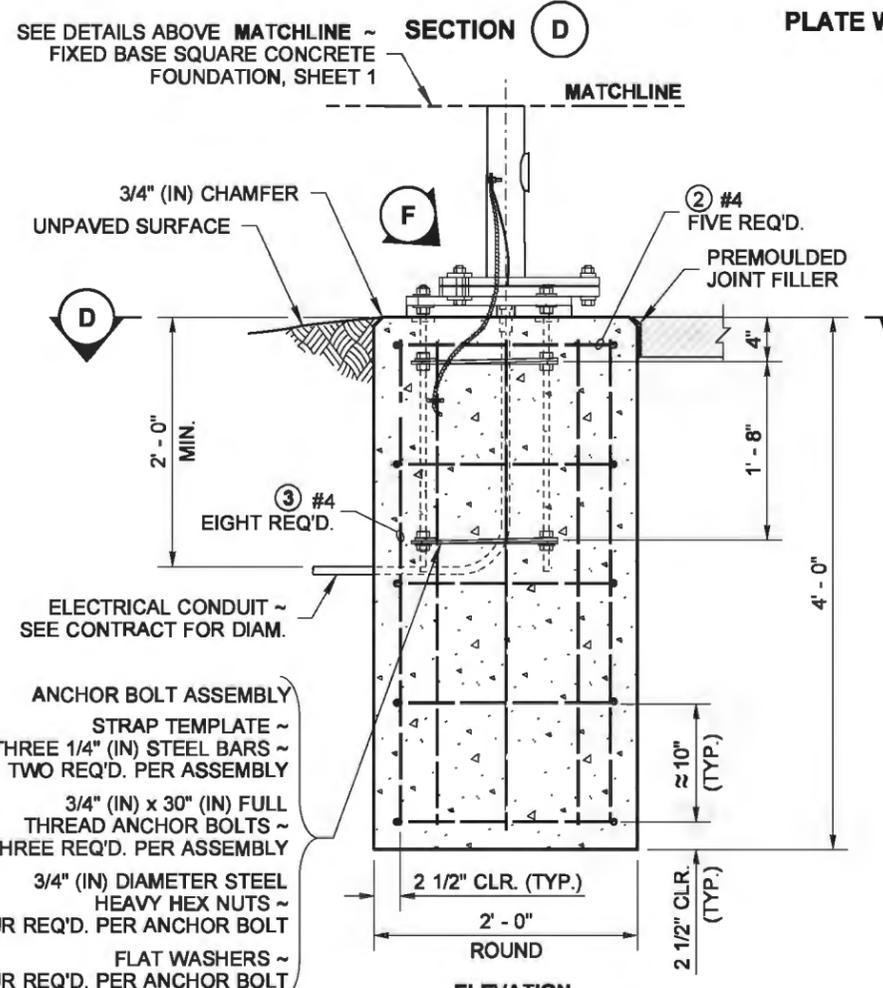
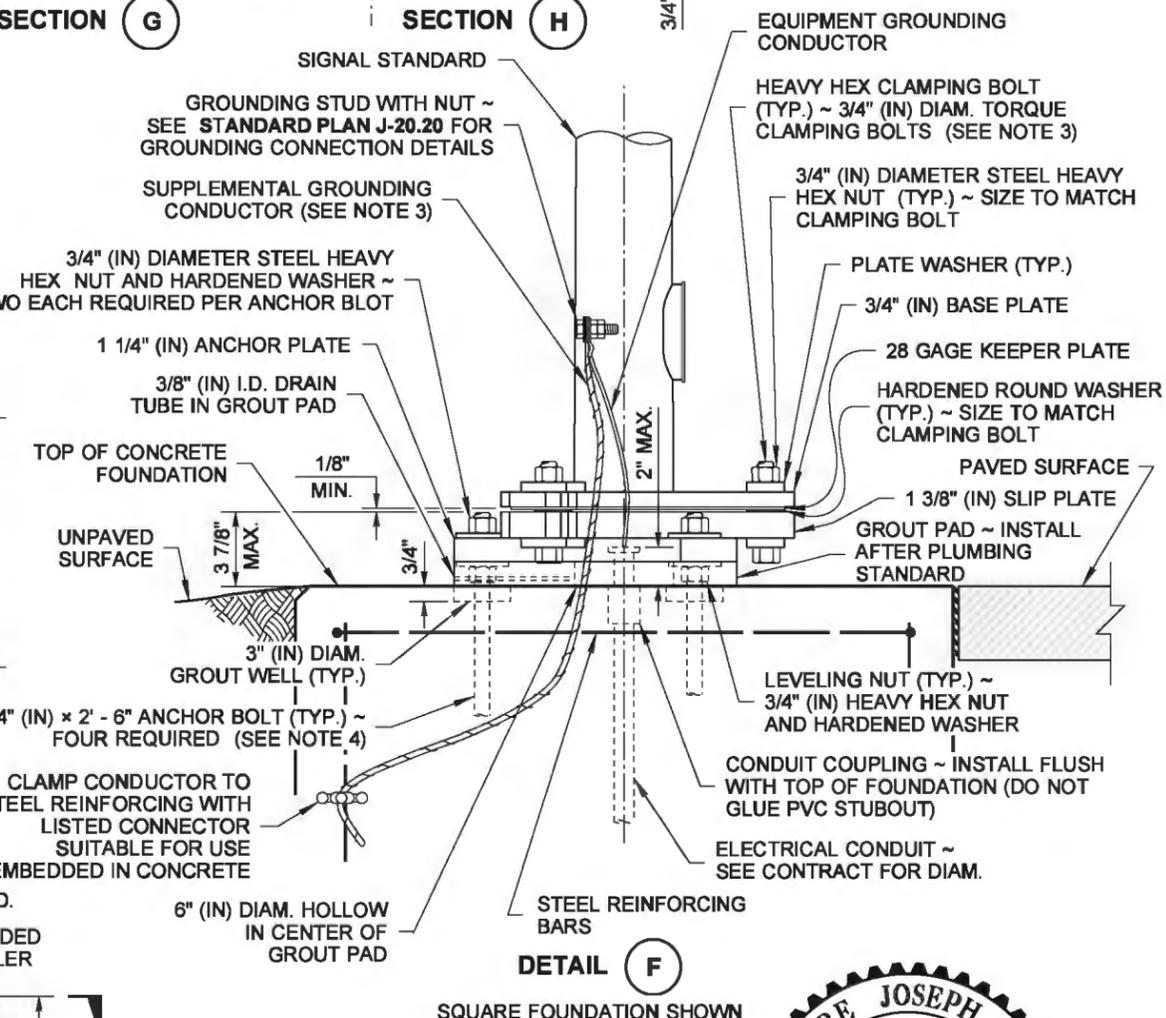


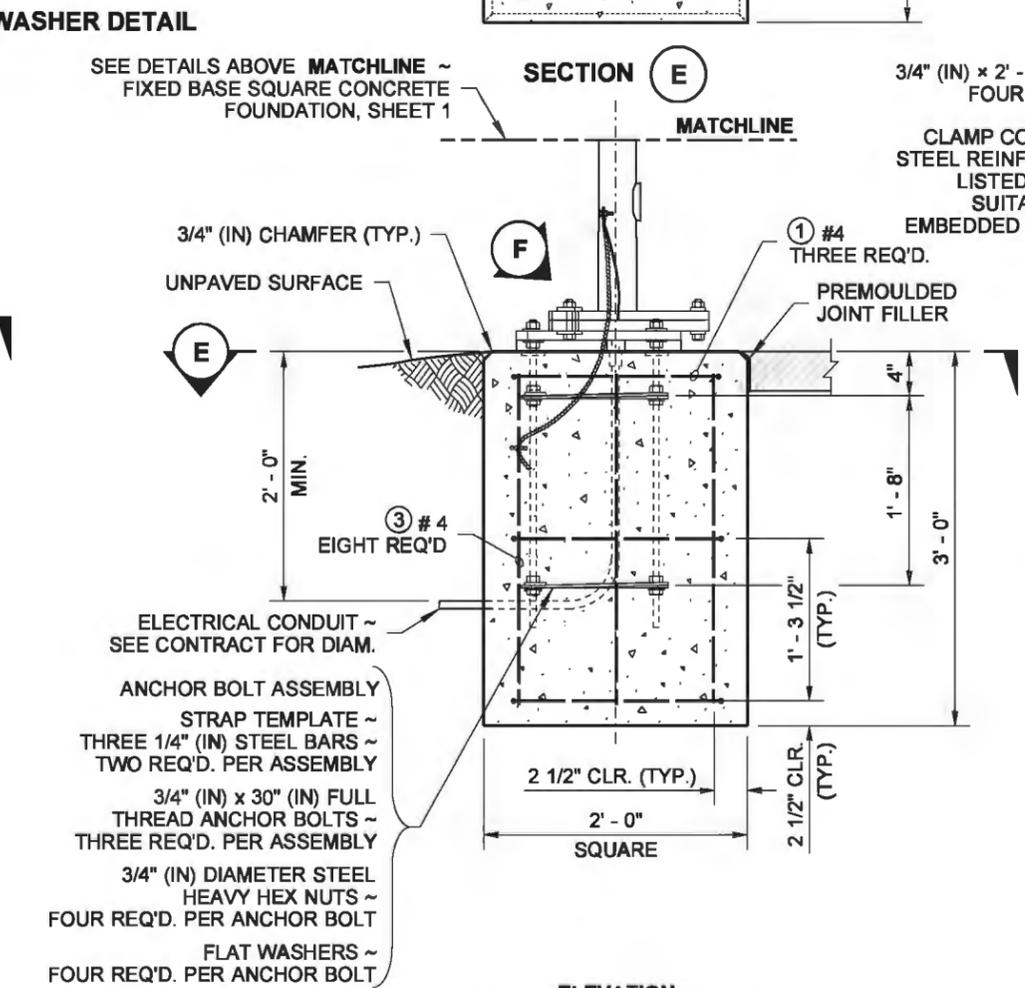
PLATE WASHER BAR DETAIL



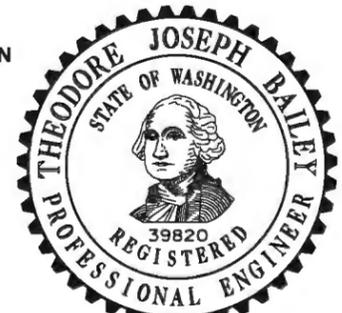
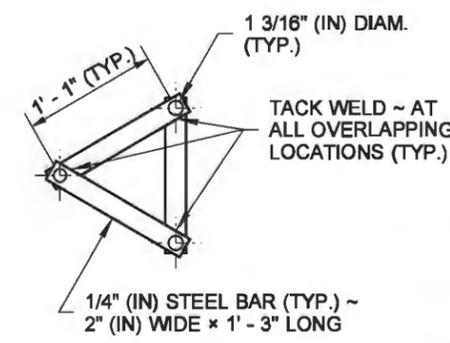
SECTION E



ROUND CONCRETE FOUNDATION DETAIL



SLIP BASE SQUARE CONCRETE FOUNDATION DETAIL

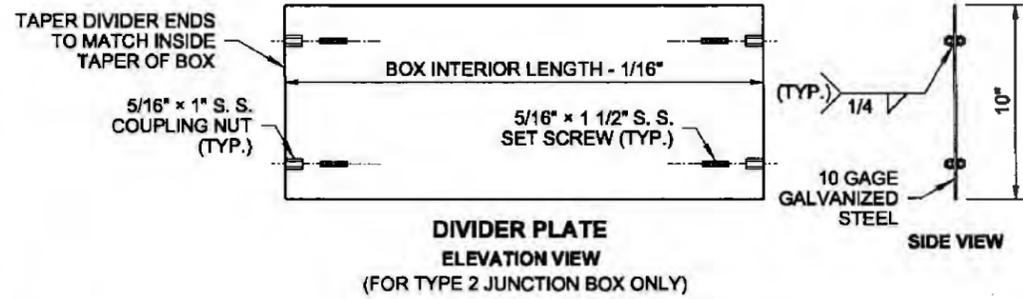


THEODORE JOSEPH BAILEY  
STATE OF WASHINGTON  
REGISTERED PROFESSIONAL ENGINEER  
39820

Bailey, Ted  
Jun 26 2014 4:29 PM

**TYPE PS, TYPE 1, RM & FB SIGNAL STANDARD FOUNDATION DETAILS**  
**STANDARD PLAN J-21.10-04**

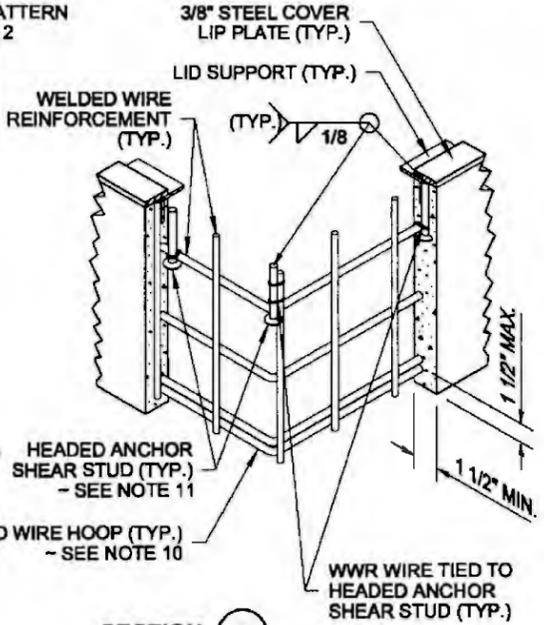
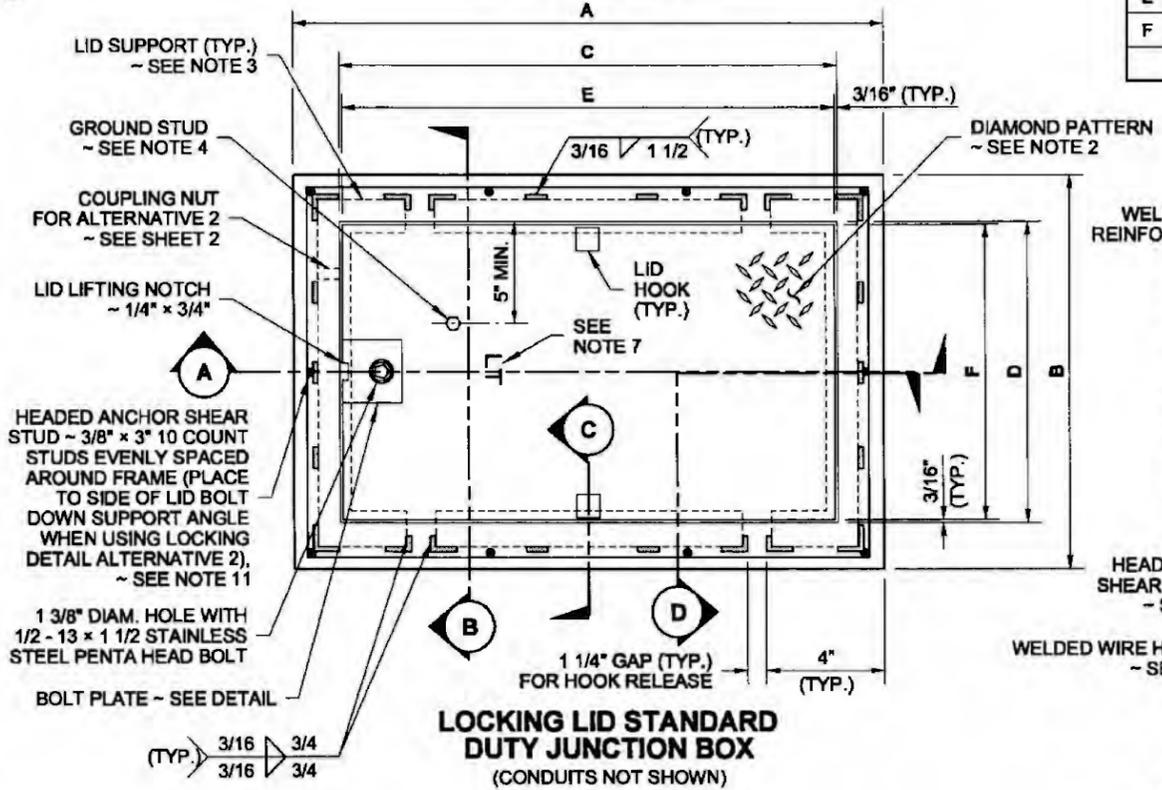
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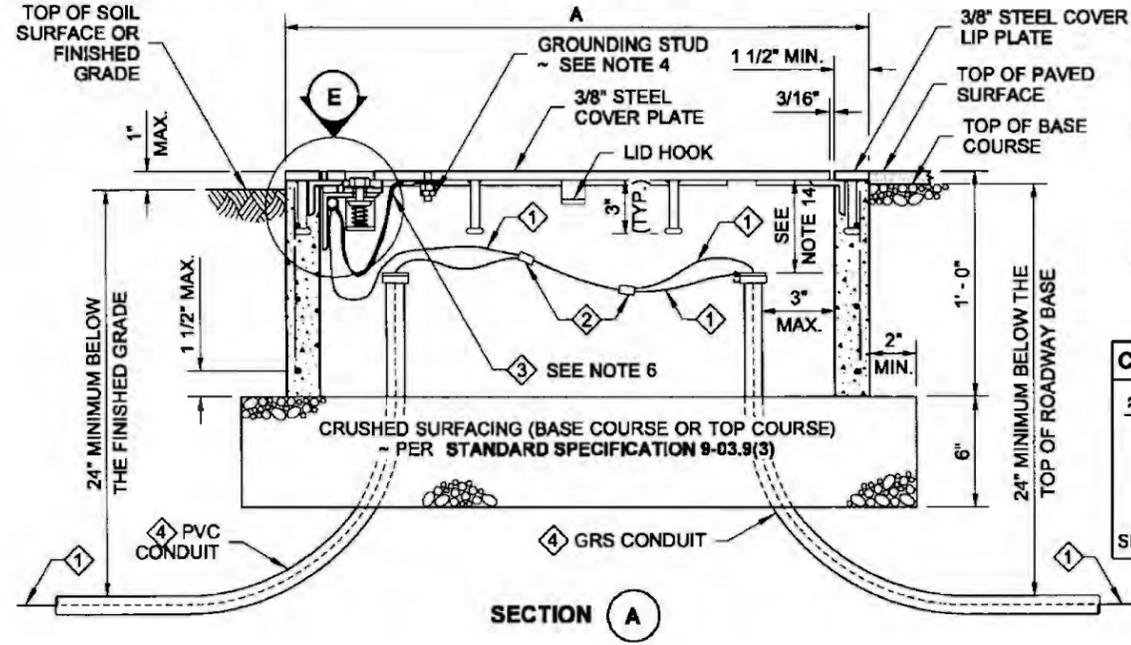
| JUNCTION BOX DIMENSION TABLE |                                |           |           |
|------------------------------|--------------------------------|-----------|-----------|
| MARK                         | ITEM                           | BOX TYPE  |           |
|                              |                                | TYPE 1    | TYPE 2    |
| A                            | OUTSIDE LENGTH OF JUNCTION BOX | 22"       | 33"       |
| B                            | OUTSIDE WIDTH OF JUNCTION BOX  | 17"       | 22 1/2"   |
| C                            | INSIDE LENGTH OF JUNCTION BOX  | 18" ~ 19" | 28" ~ 29" |
| D                            | INSIDE WIDTH OF JUNCTION BOX   | 13" ~ 14" | 17" ~ 18" |
| E                            | LID LENGTH                     | 17 5/8"   | 28 5/8"   |
| F                            | LID WIDTH                      | 12 5/8"   | 18 1/8"   |
| CAPACITY ~ CONDUIT DIAMETER  |                                | 6"        | 12"       |

**NOTES**

- All box dimensions are approximate. Exact configurations vary among manufacturers.
- Minimum lid thickness shown. The diamond pattern shall be a minimum of 28% of the overall thickness. Junction Boxes installed in sidewalks, walkways, and shared-use paths shall have a slip-resistant coating on the lid and lip cover plate, and shall be installed with the surface flush with and matched to the grade of the sidewalk, walkway, or shared-use path. The non-slip lid shall be identified with permanent markings on the underside, indicating the type of surface treatment (see Contract Documents for details) and the year of manufacture. The permanent marking shall be 1/8" inch line thickness formed with a stainless steel weld bead and shall be placed prior to hot-dip galvanizing.
- Lid support members shall be 3/16" minimum thick steel C, L, or T shape, welded to the frame.
- A 1/4-20 NC x 3/4" stainless steel ground stud shall be welded to the bottom of the lid; include (2) stainless steel nuts and (2) stainless steel flat washers.
- Bolts and nuts shall be liberally coated with anti-seize compound.
- Equipment Bonding Jumper shall be # 8 AWG min. x 4' of tinned braided copper.
- The System Identification letters shall be 1/8" line thickness formed by engraving, stamping, or with a S. S. weld bead. See Cover Marking detail. Grind off diamond pattern before forming letters. For System Identification details, see **Standard Specification 9-29.2(4)**.
- When required in the Contract, provide a 10" x 27 1/2", 10 gage divider plate, complete, with fasteners, in each Type 2 Junction Box where specified.
- When required in Contract, provide a 12" deep extension for each Type 2 Junction Box where specified.
- See the Standard Specifications for alternative reinforcement and class of concrete.
- Headed Anchor Shear Studs must be welded to the Steel Cover Lip Plate and wire tied in two places to the vertical Welded Reinforcement Wire when in contact with each other. Wire tie all other Headed Anchor Shear Studs to the horizontal Welded Reinforcement Wire.
- Lid Bolt Down Attachment Tab provides a method of retrofitting by using a mechanical process in lieu of welding. Attachment Tab shown depicts a typical component arrangement; actual configurations of assembly will vary among manufacturers. See approved manufacturers' shop drawings for specifics.
- Unless otherwise noted in the plans or approved by the Engineer, Junction Boxes, Cable Vaults, and Pull Boxes shall not be placed within the sidewalks, walkways, shared use paths, traveled ways or paved shoulders. All Junction Boxes, Cable Vaults, and Pull Boxes placed within the traveled way or paved shoulders shall be Heavy-Duty.
- Distance between the top of the conduit and the bottom of the Junction Box lid shall be 6" min. to 8" max. for final grade of new construction only. See **Standard Specification 8-20.3(5)**. Where adjustments are to be made to existing Junction Boxes, or for interim construction stages during the contract, the limits shall be from 6" min. to 10" max. See **Standard Specification 8-20.3(6)**.

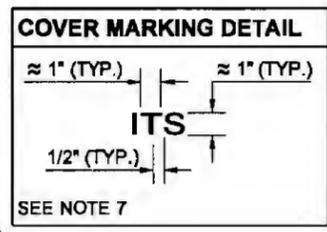


**SECTION D**  
PERSPECTIVE VIEW

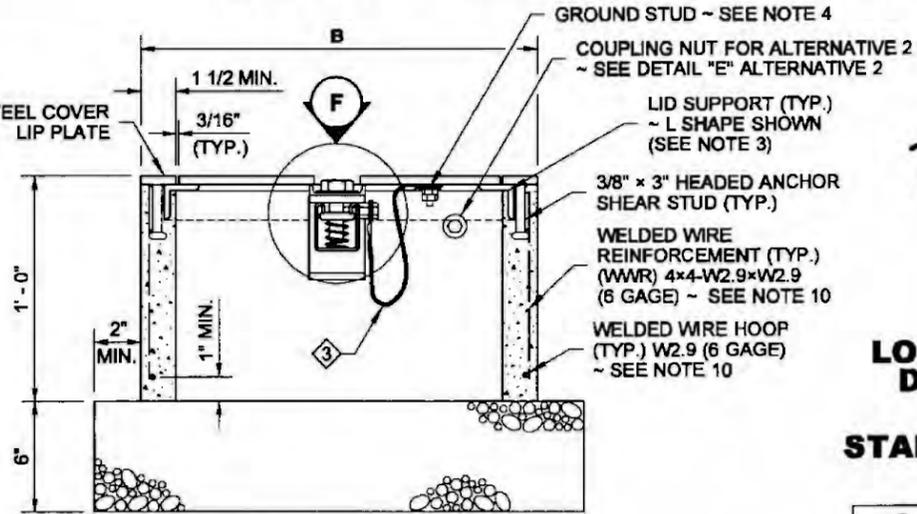


**SECTION A**

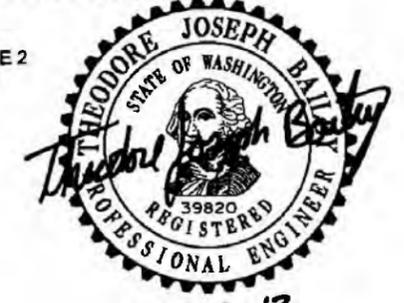
- ① Equipment Grounding Conductor
- ② Copper Solderless Crimp Connector
- ③ Equipment Bonding Jumper ~ see note 6
- ④ See Contract for conduit size and number



SEE NOTE 7



**SECTION B**  
(CONDUITS NOT SHOWN)

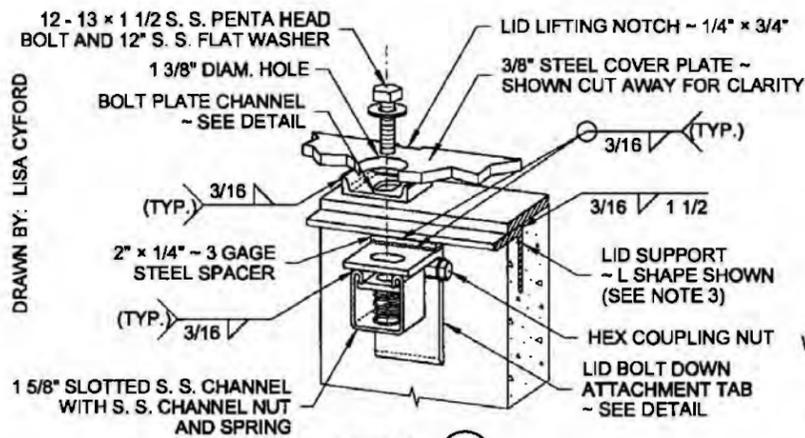


**LOCKING LID STANDARD DUTY JUNCTION BOX TYPES 1 & 2**  
**STANDARD PLAN J-40.10-03**

SHEET 1 OF 2 SHEETS

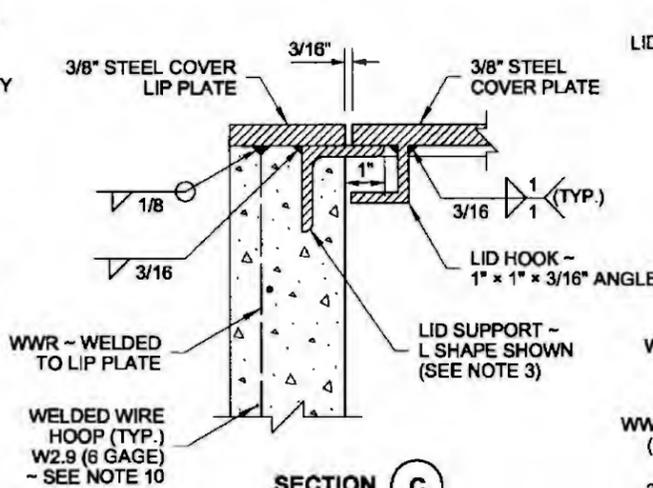
APPROVED FOR PUBLICATION  
*Pono Bofill* 5/20/13  
 STATE DESIGN ENGINEER DATE  
 Washington State Department of Transportation

DRAWN BY: LISA CYFORD

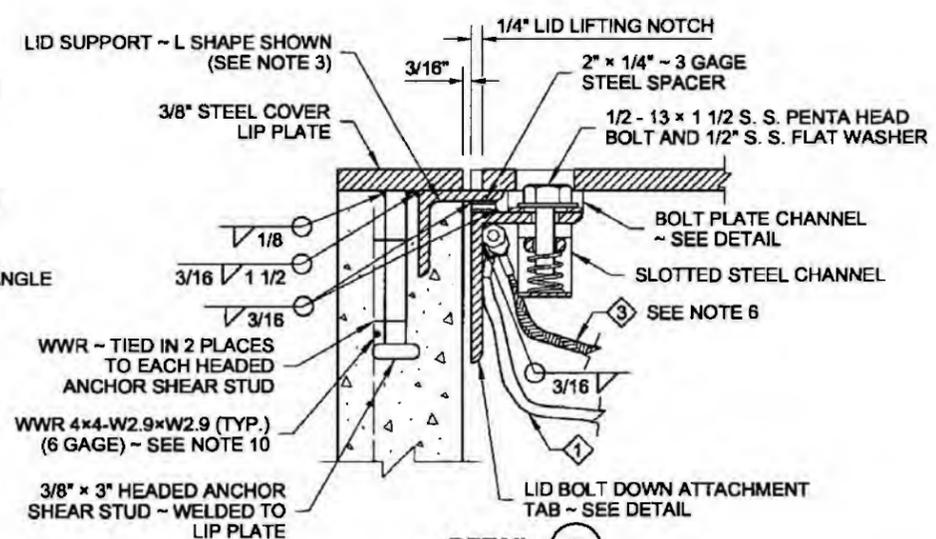


DETAIL F

ALTERNATIVE 1 SHOWN PERSPECTIVE VIEW

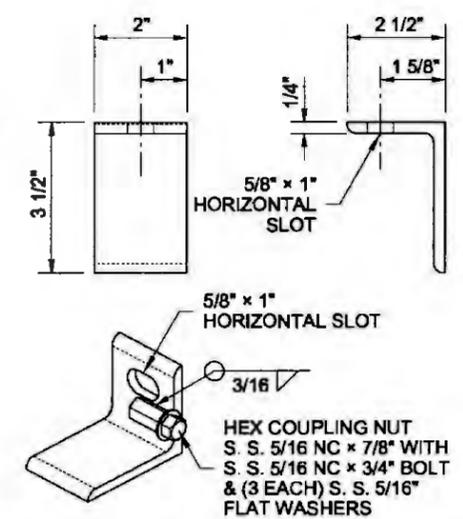


SECTION C

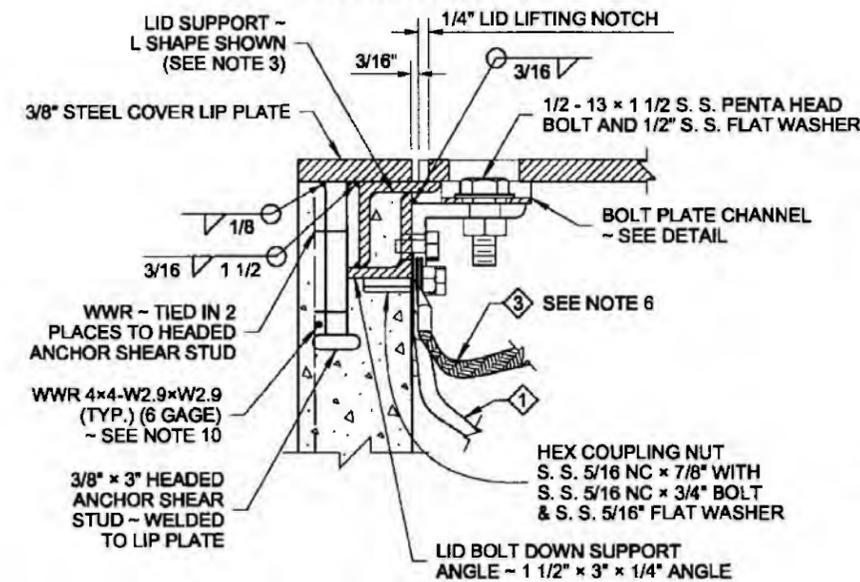


DETAIL E

ALTERNATIVE 1 SHOWN

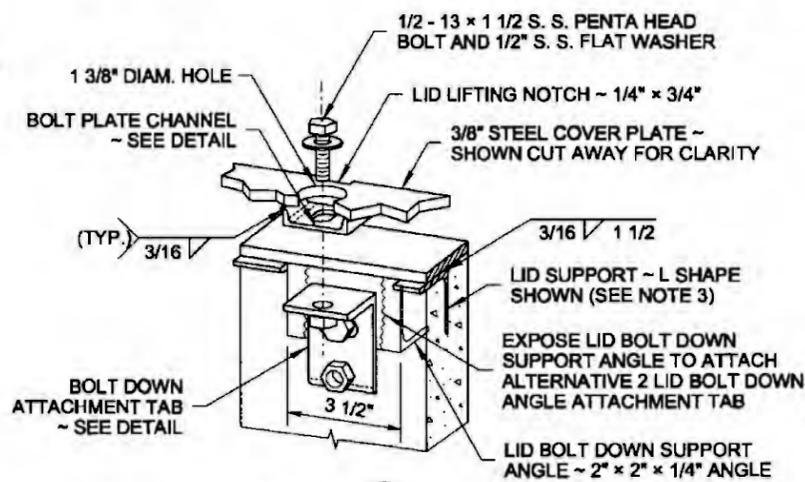


ALTERNATIVE 1 LID BOLT DOWN ATTACHMENT TAB (SEE NOTE 12)



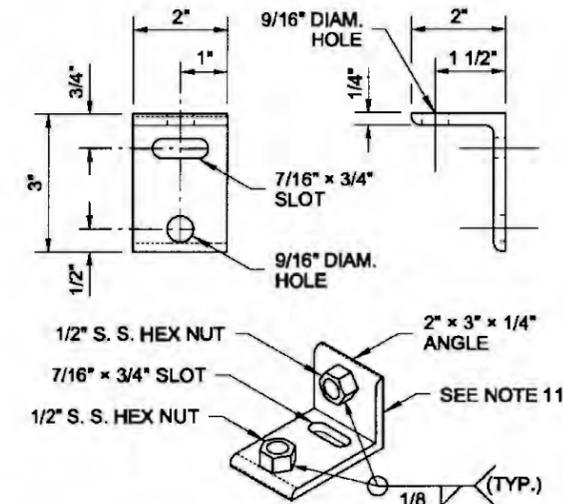
DETAIL E

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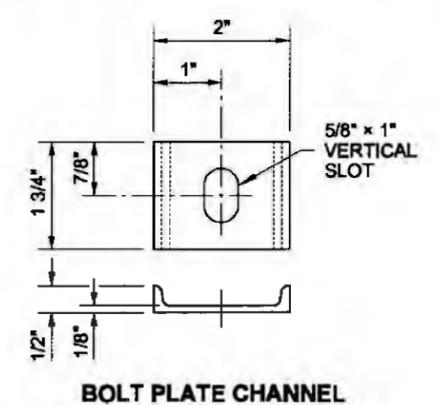


DETAIL F

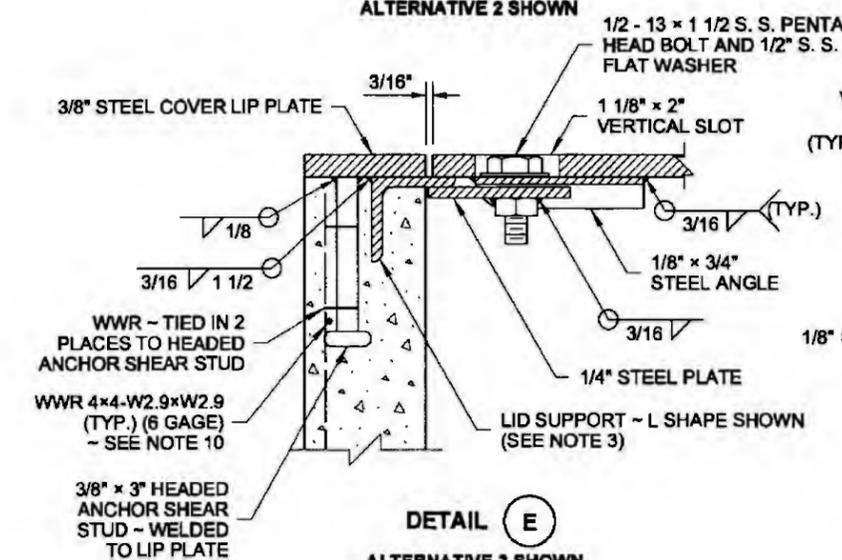
ALTERNATIVE 2 SHOWN PERSPECTIVE VIEW



ALTERNATIVE 2 LID BOLT DOWN ATTACHMENT TAB (SEE NOTE 12)

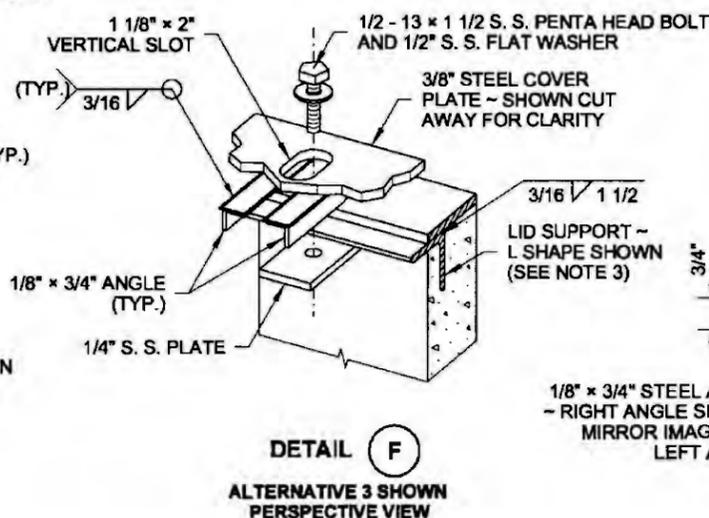


BOLT PLATE CHANNEL



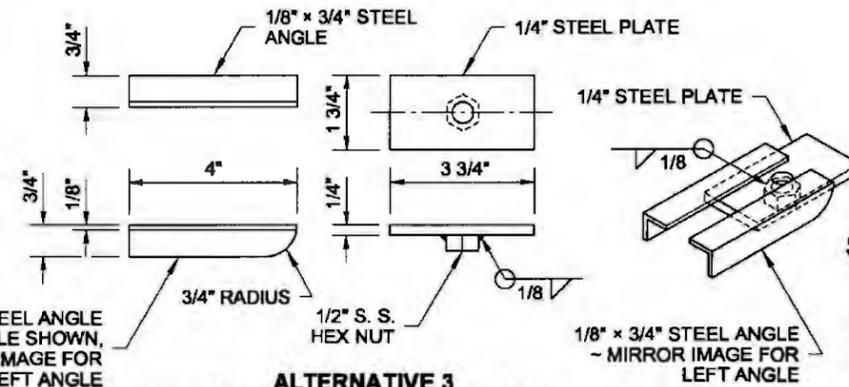
DETAIL E

ALTERNATIVE 3 SHOWN



DETAIL F

ALTERNATIVE 3 SHOWN PERSPECTIVE VIEW



ALTERNATIVE 3 LID BOLT DOWN ATTACHMENT TAB (SEE NOTE 12)



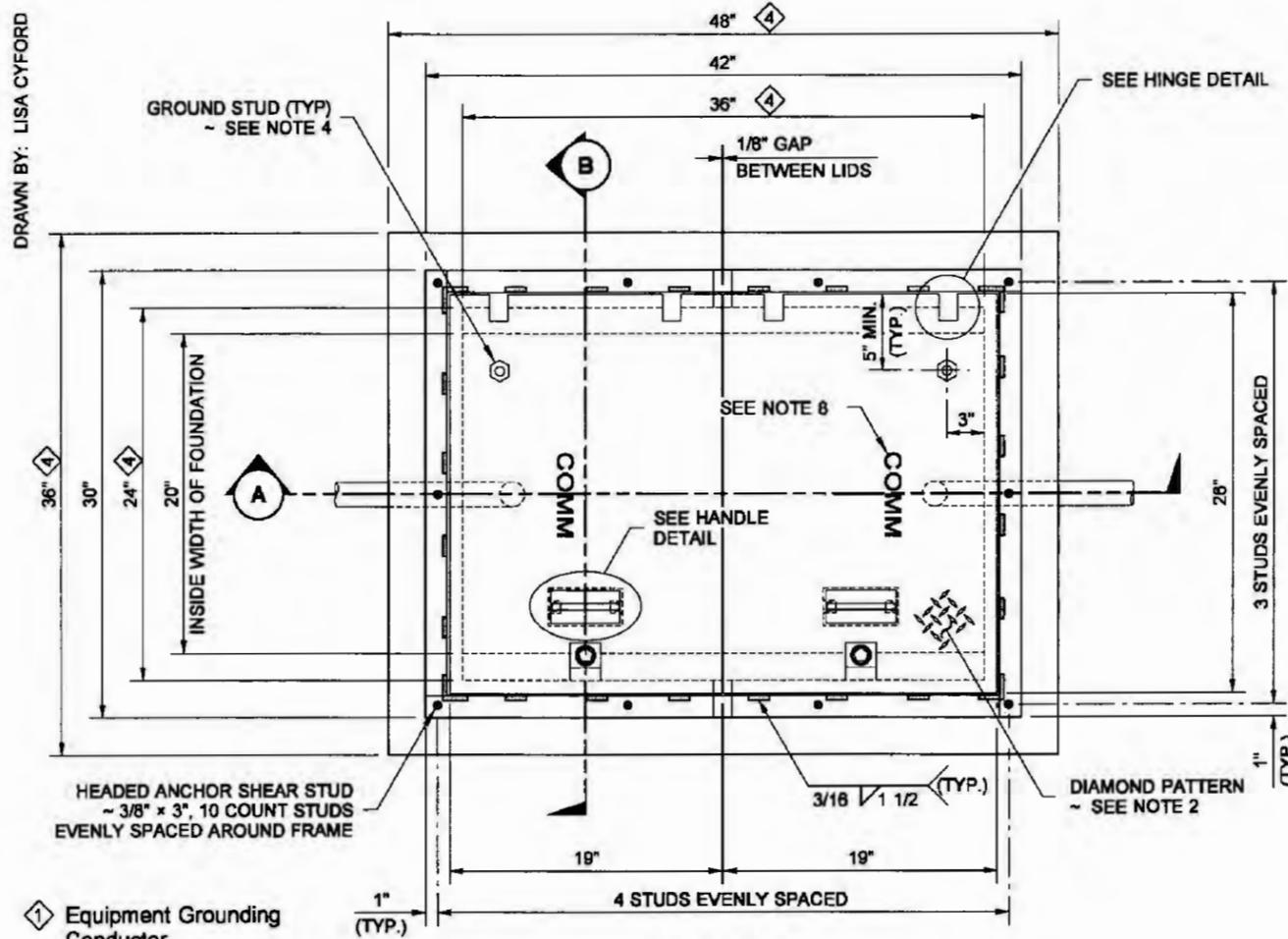
5-15-13

LOCKING LID STANDARD DUTY JUNCTION BOX TYPES 1 & 2 STANDARD PLAN J-40.10-03

SHEET 2 OF 2 SHEETS

APPROVED FOR PUBLICATION  
*Theodore Joseph Bailey* 5/20/13  
 STATE DESIGN ENGINEER DATE  
 Washington State Department of Transportation

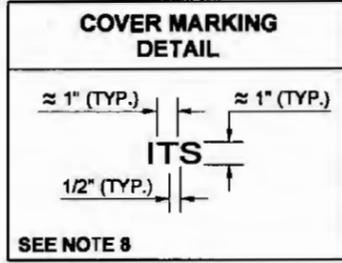
DRAWN BY: LISA CYFORD



PLAN VIEW

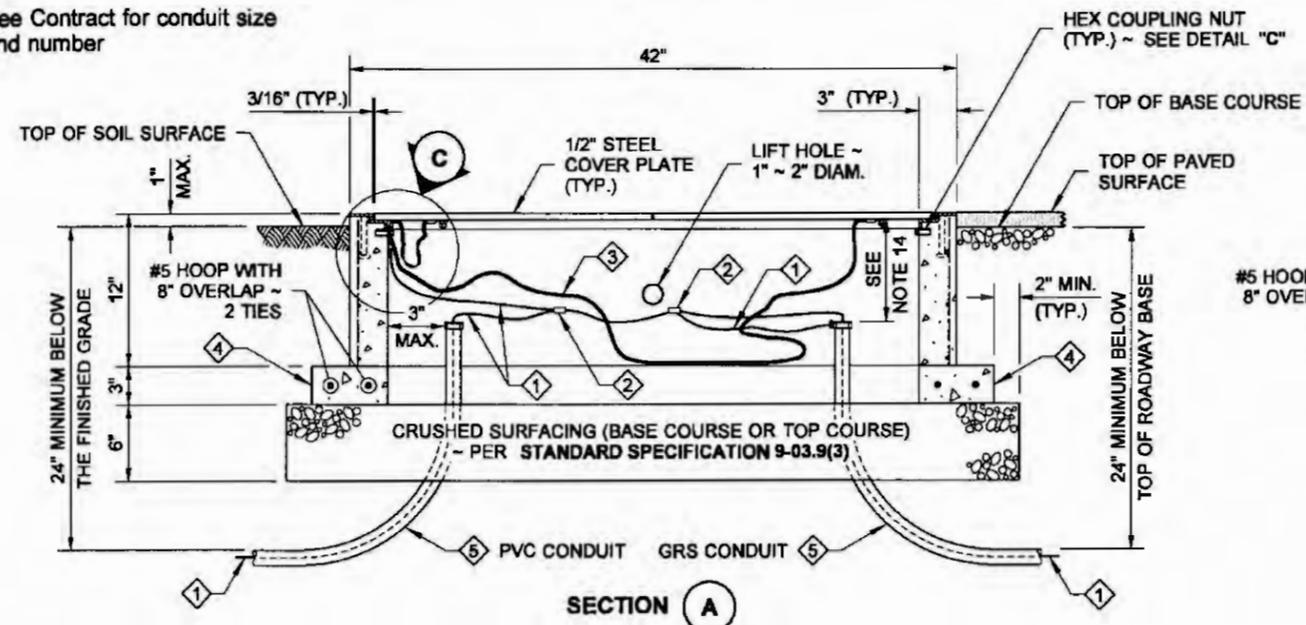
**LOCKING LID STANDARD DUTY JUNCTION BOX**

- ① Equipment Grounding Conductor
- ② Copper Solderless Crimp Connector
- ③ Equipment Bonding Jumper
- ④ Foundation
- ⑤ See Contract for conduit size and number

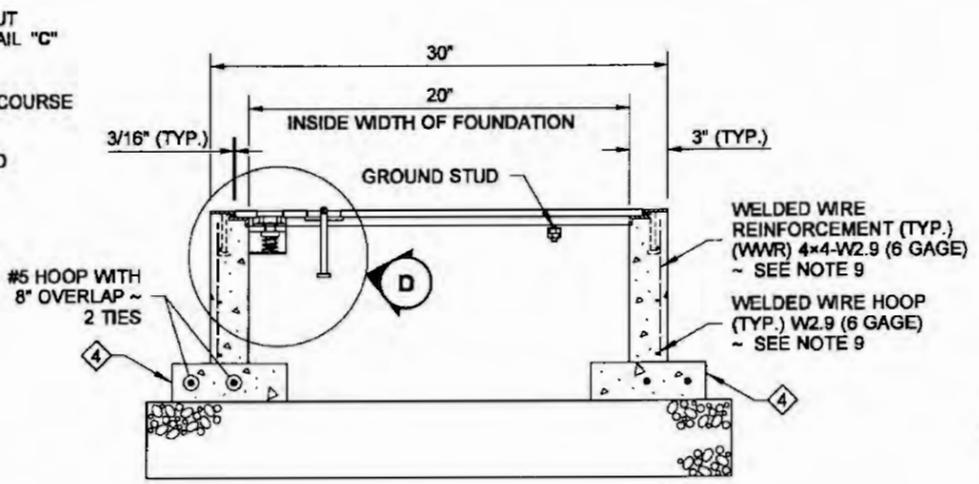


**NOTES**

1. All box dimensions are approximate. Exact configurations vary among manufacturers.
2. Minimum lid thicknesses are shown. The diamond pattern shall be a minimum of 28% of the overall thickness. Junction Boxes installed in sidewalks, walkways, and shared-use paths shall have a slip-resistant coating on the lid and lip cover plate and shall be installed with the surface flush with and matched to the grade of the sidewalk, walkway, or shared-use path. The non-slip lid shall be identified with permanent markings on the underside, indicating the type of surface treatment (see Contract Documents for details) and the year of manufacture. The permanent marking shall be 1/8" inch line thickness formed with a stainless steel weld bead and shall be placed prior to hot-dip galvanizing.
3. Lid support members shall be 3/16" min. thick steel C, L, or T shape, welded to the frame. Exact configurations vary among manufacturers.
4. A 1/4-20 NC x 3/4" S. S. ground stud shall be welded to the bottom of each lid; include (2) S. S. nuts and (2) S. S. flat washers.
5. The hinges shall allow the lids to open 180°.
6. Bolts and nuts shall be liberally coated with anti-seize compound.
7. Connect Equipment Bonding Jumper to ground stud on lid. As an alternative to the ground stud connection, the Equipment Bonding Jumper shall be attached to the front face of the hinge pocket with a 5/16-20 NC x 3/4" S. S. bolt, (2) each S. S. nuts, and (2) each S. S. flat washers. Equipment Bonding Jumper shall be #8 AWG min. x 4' of tinned braided copper.
8. The System Identification letters shall be 1/8" line thickness formed by engraving, stamping, or with a S. S. weld bead. See Cover Marking detail. Grind off diamond pattern before forming letters. See **Standard Specification 9-29.2(4)** for details.
9. See the Standard Specifications for alternative reinforcement and class of concrete.
10. See **Standard Plan J-40.10** for Welded Wire Reinforcement and Headed Anchor Shear Stud attachment details.
11. Capacity ~ conduit diameter = 24"
12. Lid Bolt Down Attachment Tab provides a method of retrofitting by using a mechanical process in lieu of welding. Attachment Tab shown depicts a typical component arrangement; actual configurations of assembly will vary among manufacturers. See approved manufacturers' shop drawing for specifics.
13. Unless otherwise noted in the plans or approved by the Engineer, Junction Boxes, Cable Vaults and Pull Boxes shall not be placed within the sidewalk, walkway, shared use path, traveled way or paved shoulders. All Junction Boxes, Cable Vaults, and Pull Boxes placed within the traveled way or paved shoulders shall be Heavy-Duty.
14. Distance between the top of the conduit and the bottom of the Junction Box lid shall be 6" min. to 8" max. for final grade of new construction only. See **Standard Specification 8-20.3(5)**. Where adjustments are to be made to existing Junction Boxes, or for interim construction stages during the contract, the limits shall be from 6" min. to 10" max. See **Standard Specification 8-20.3(6)**.



SECTION A



SECTION B

CONDUITS NOT SHOWN



5-15-13

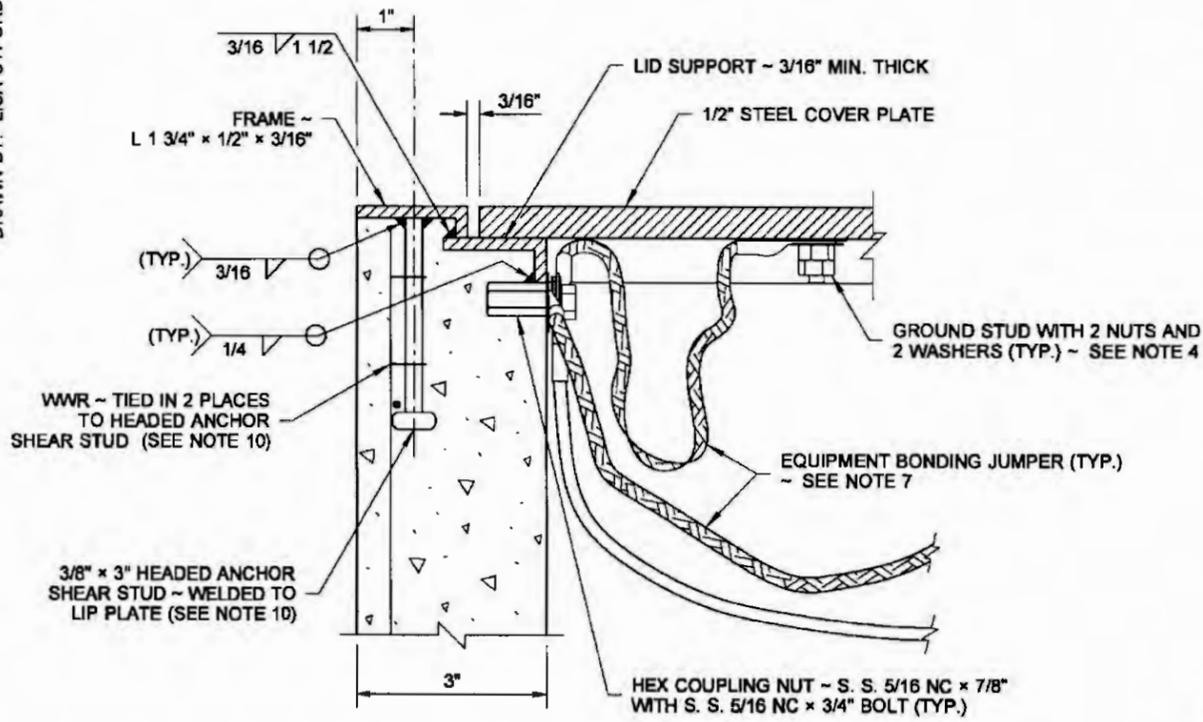
**LOCKING LID STANDARD DUTY JUNCTION BOX TYPE 8**

**STANDARD PLAN J-40.30-03**

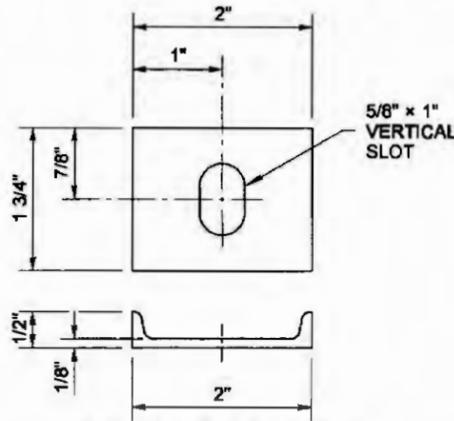
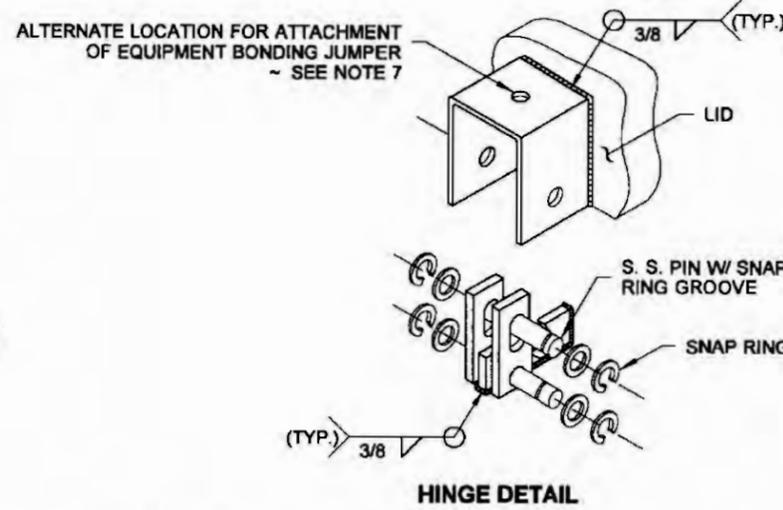
SHEET 1 OF 2 SHEETS

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*Pino Bralich* 5/20/13  
 STATE DESIGN ENGINEER DATE  
 Washington State Department of Transportation

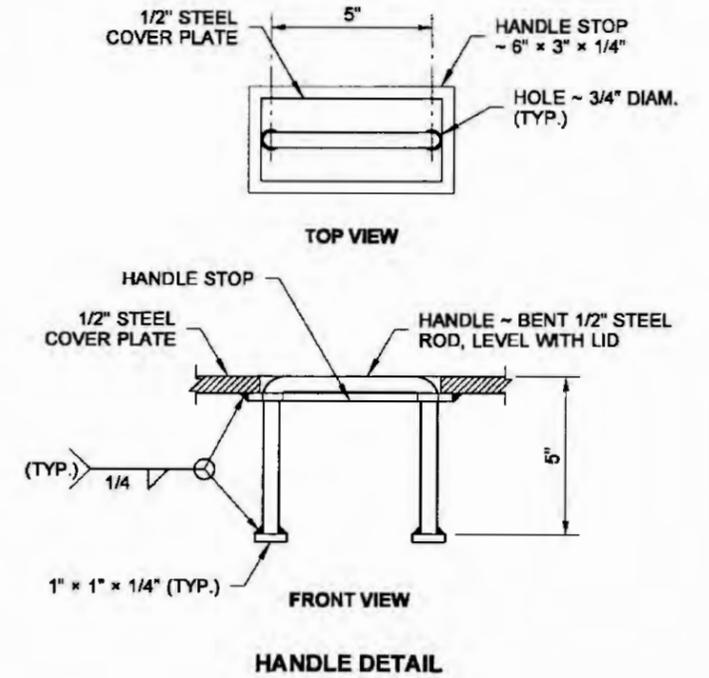
DRAWN BY: LISA CYFORD



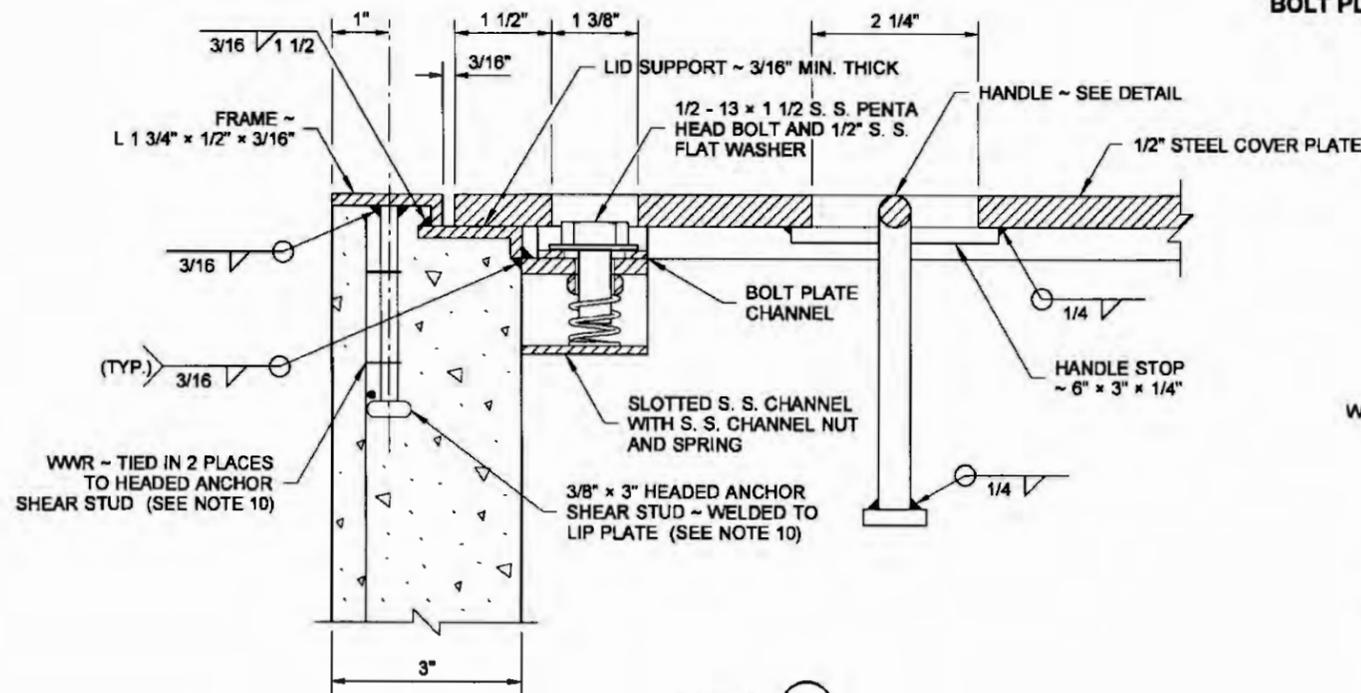
DETAIL C



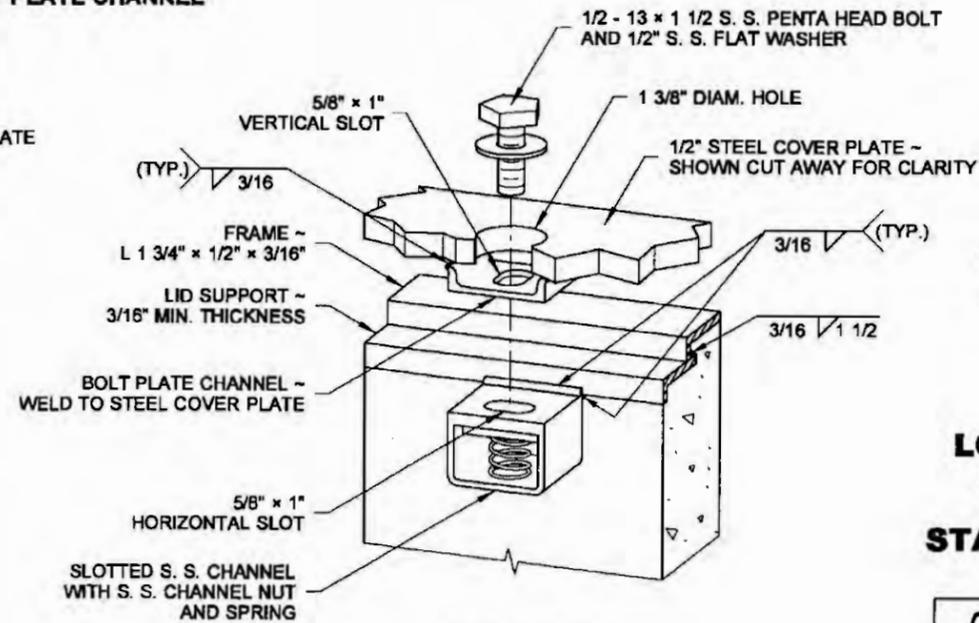
BOLT PLATE CHANNEL



HANDLE DETAIL



DETAIL D



DETAIL D  
ISOMETRIC VIEW



5-15-13

**LOCKING LID STANDARD DUTY JUNCTION BOX TYPE 8**  
**STANDARD PLAN J-40.30-03**

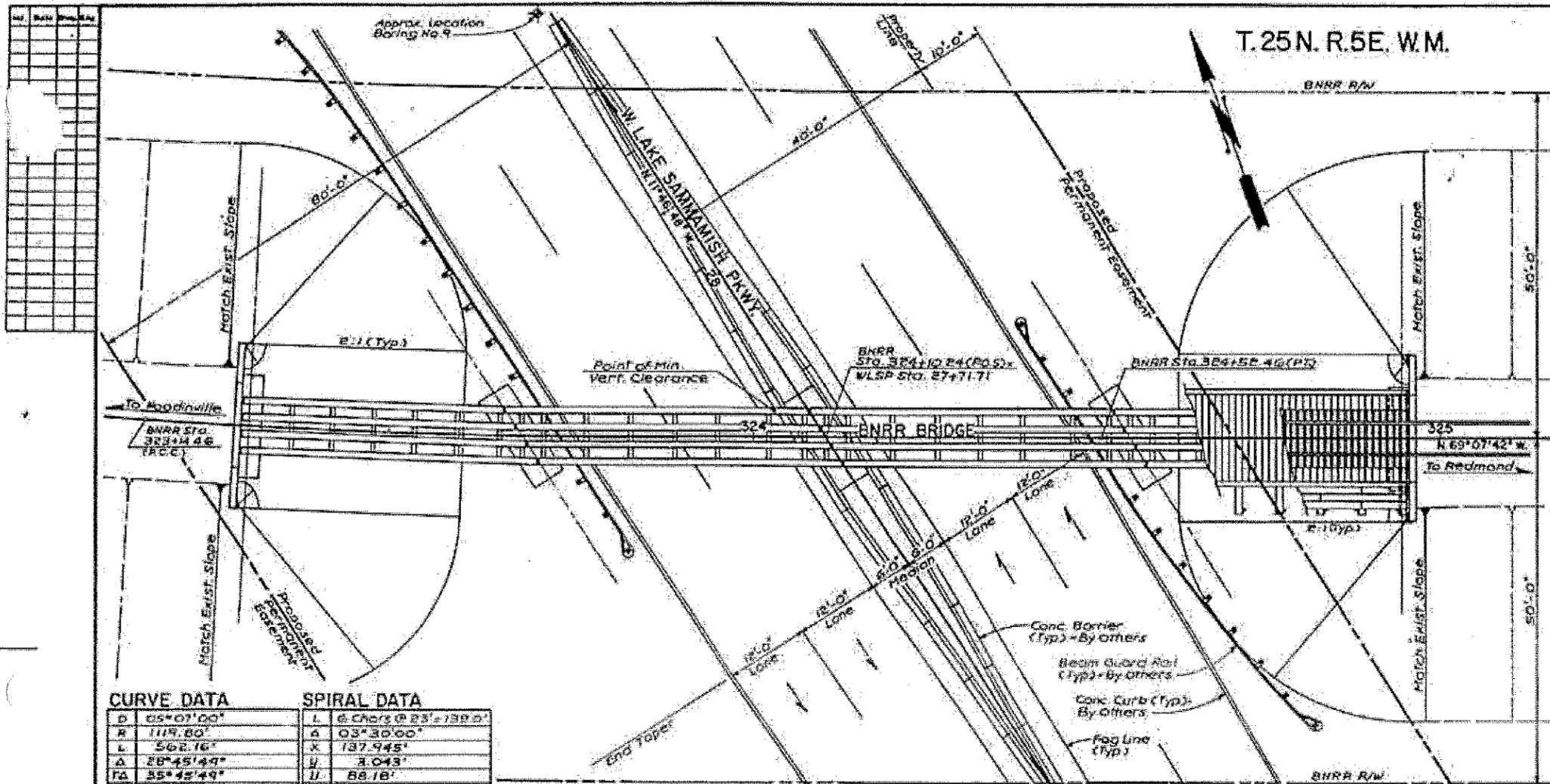
SHEET 2 OF 2 SHEETS

APPROVED FOR PUBLICATION  
*Peter B. ...* 5/13/13  
STATE DESIGN ENGINEER DATE  
Washington State Department of Transportation

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## Appendix B — Plans of Existing Bridges

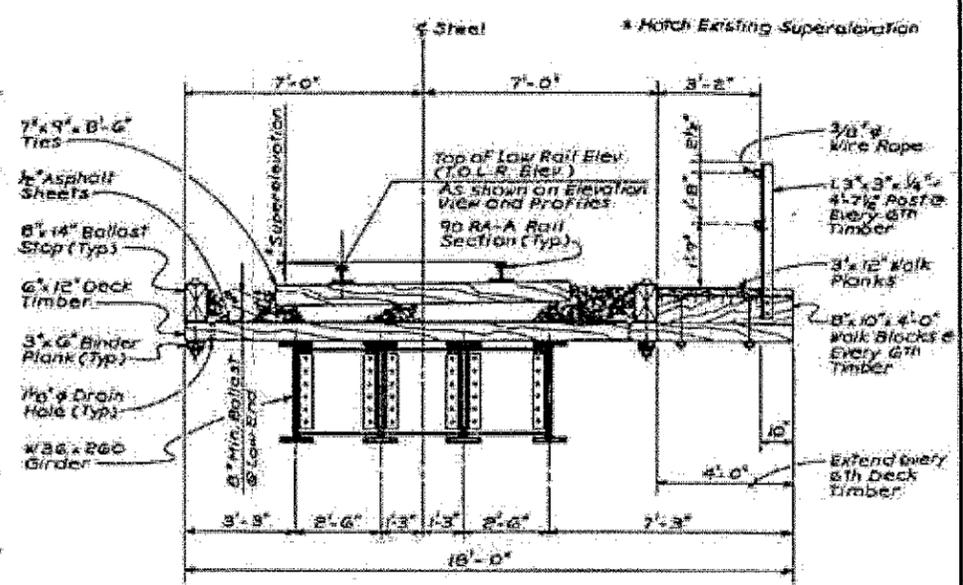
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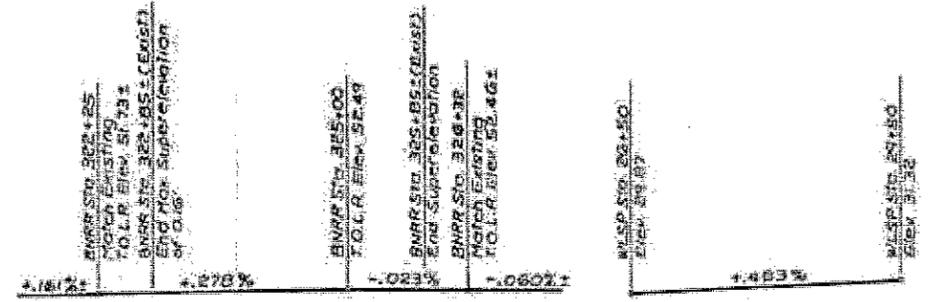
T.25N. R.5E. W.M.

| CURVE DATA |           | SPIRAL DATA |                       |
|------------|-----------|-------------|-----------------------|
| D          | 05*07'00" | L           | 6 Chgs @ 23' = 138'0" |
| R          | 1119.80'  | Δ           | 03°30'00"             |
| L          | 562.16'   | X           | 137.945'              |
| Δ          | 28°45'49" | Y           | 3.043'                |
| PΔ         | 35°45'49" | U           | 88.18'                |
| T          | 287.14'   | V           | 49.85'                |
| TR         | 431.20'   | C           | 137.98'               |

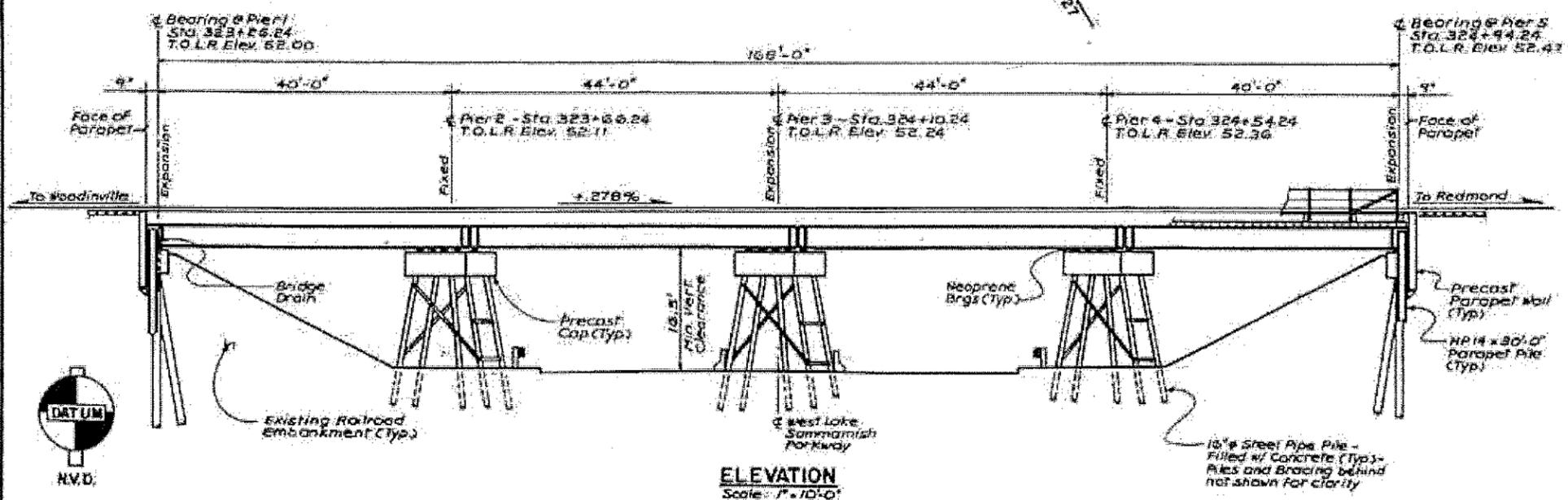
PLAN  
Scale: 1" = 10'



TYPICAL CROSS SECTION  
Scale: 3/8" = 1'-0"



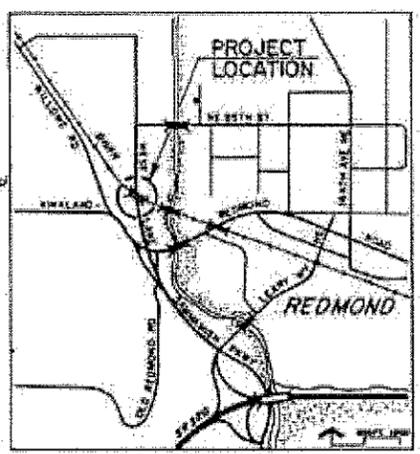
BNRR  
WLSP  
PROFILES  
Scale: 1" = 100' (HORIZ.)  
1" = 10' (VERT.)



ELEVATION  
Scale: 1" = 10'-0"

GENERAL NOTES:

- DESIGN SPECIFICATIONS: 1988 A.R.E.A.
- LOADING: Coopers E60 + Diesel Impact
- MATERIAL AND WORKMANSHIP: Shall conform to 1988 A.R.E.A. and the 1988 Standard Specifications for Road, Bridge and Municipal Construction by the Washington State Department of Transportation.
- REINFORCING STEEL: ASTM A-615, Grade 60,  $f_y = 24,000$  psi
- STRUCTURAL STEEL: ASTM A-36
- CONCRETE: Pier Caps & Parapet Walls: Class 40,  $f'_c = 4000$  psi; Piles: Class 30,  $f'_c = 3000$  psi
- PIPE PILING: 10" dia. 3/8" wall, ASTM A-252, Grade 2, Concrete Filled, Seamless or Straight welded. Piles are friction piles and shall be driven to 110 ton capacity.



LOCATION MAP

|                                 |  |  |   |  |   |                             |
|---------------------------------|--|--|---|--|---|-----------------------------|
|                                 |  | Approved By: _____<br>Date: 4-89   | ENTRANCO ENGINEERS, INC.<br>Kirkland and Everett, WA      Phoenix, AZ | Scale: _____<br>North: AS<br>South: SD<br>East: E<br>West: W | BURLINGTON NORTHERN RAILROAD COMPANY<br>SEATTLE REGION      PACIFIC DIVISION<br>WOODHVILLE TO ISBAQUAH SPUR<br>BRIDGE NO. 6-14 OVER W. LAKE SAMMAMISH PKWY. | Sheet 1<br>Of 6<br>PLAN NO. |
| No. Date By Chd. Appr. Revision |  | Drawn By: GTV<br>Designed By: KTD<br>Checked By: M/Bea<br>Approved By: _____ |   | Job No.: 88126-21  | BRIDGE LAYOUT   |                             |





**CONCRETE NOTES:**

Design loading: Coopers E-80 with Diesel Impact per Current A.R.E.A. Manual.

Precast concrete shall have a minimum compressive strength of 4000 psi at 28 days (Class 40) per Burlington Northern Specification for Precast Concrete Products.

All exposed edges of concrete shall be chamfered 3/4" unless otherwise shown or noted.

Concrete shall be vibrated internally during placement to provide thorough consolidation and compaction. Care shall be taken to avoid displacement of embedded items.

Shear stud connectors conforming to A.S.T.M. A-108, Grade 1010 thru 1020 shall be fastened to A-36 steel plate per current AWS Welding Code D1.1, Section 7. Surface of plate shall be cleaned per steel structures Painting Council Specification SPC-SP6 prior to welding studs. Do not shop paint.

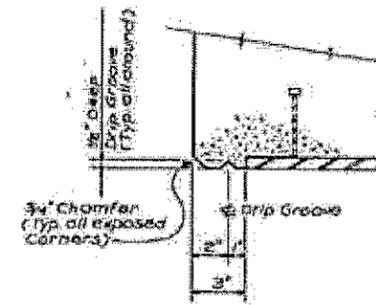
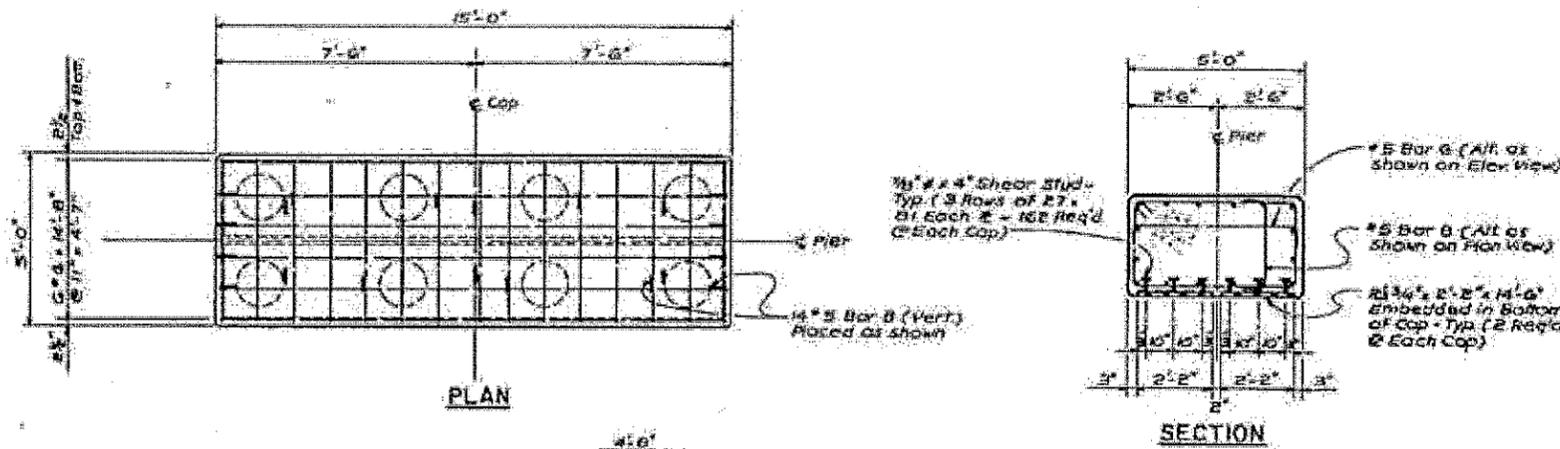
All reinforcing steel shall conform to A.S.T.M. A-615 Grade 60.

All reinforcing steel shall have a minimum 2" cover unless otherwise shown or noted.

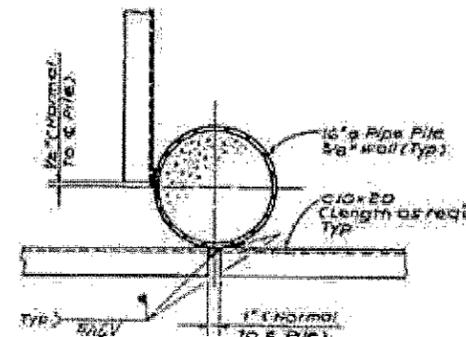
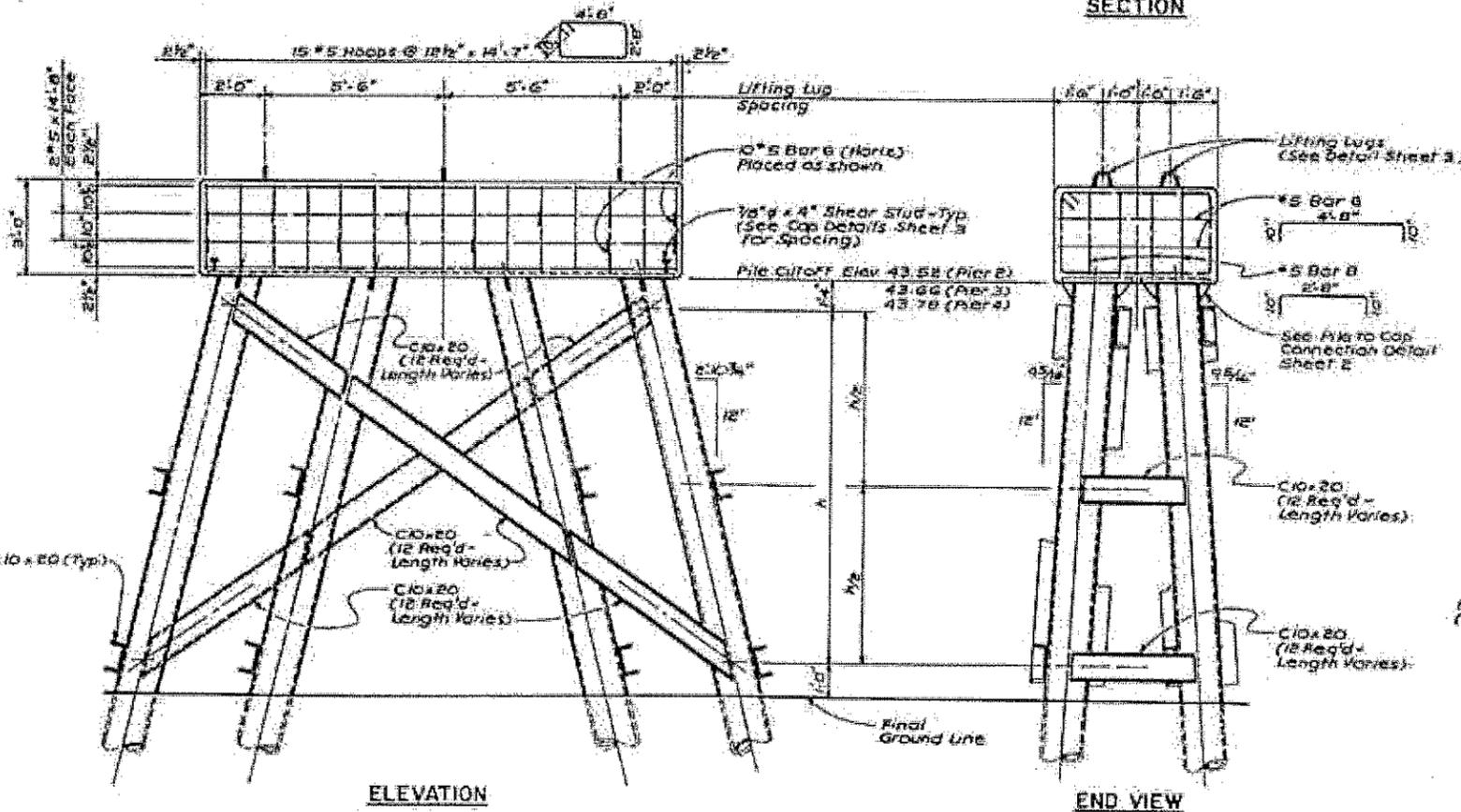
All bar bending and standard hook dimensions shall be in accordance with "Manual of Standard Practice" as published by The Concrete Reinforcing Steel Institute unless otherwise shown or noted.

**GENERAL NOTES:**

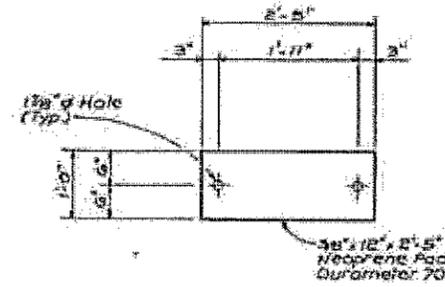
See Sheet 3 For Field Notes and Sheet 2 For Pier Layout and Details.



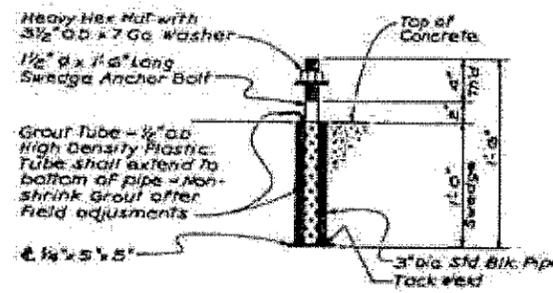
**DRIP GROOVE DETAIL**  
NOT TO SCALE



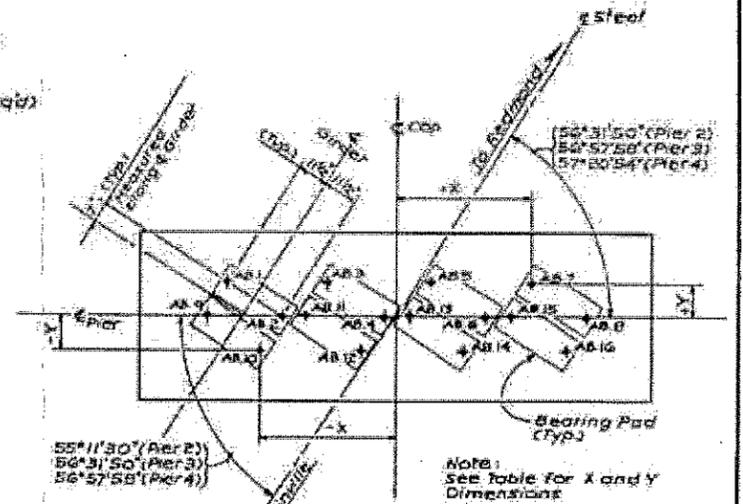
**BRACING DETAIL**  
NOT TO SCALE



**NEOPRENE BEARING PAD NP-2**  
NOT TO SCALE (E4 Required)



**ANCHOR BOLT DETAIL**  
NOT TO SCALE (ASTM A-36 - 52 Req'd)



| ANCHOR BOLT NUMBER | OFFSET FROM E CAP (X OR -X) |             |             | OFFSET FROM E PIER (Y OR -Y) |            |            |
|--------------------|-----------------------------|-------------|-------------|------------------------------|------------|------------|
|                    | Pier 2                      | Pier 3      | Pier 4      | Pier 2                       | Pier 3     | Pier 4     |
| AB.1               | -4'-11 1/2"                 | -4'-11 1/2" | -4'-11 3/8" | +1'-0 1/8"                   | +1'-0 1/8" | +1'-0 1/8" |
| AB.2               | -3'-4 1/2"                  | -3'-4 1/4"  | -3'-4"      | -0'-0 1/2"                   | -0'-0 3/8" | -0'-0 1/4" |
| AB.3               | -1'-11 3/4"                 | -1'-11 1/4" | -1'-11 3/8" | +1'-0 1/8"                   | +1'-0 1/8" | +1'-0 1/8" |
| AB.4               | -0'-4 1/2"                  | -0'-4 1/2"  | -0'-4 3/8"  | -0'-0 1/8"                   | -0'-0 1/8" | -0'-0 1/8" |
| AB.5               | +1'-0 1/2"                  | +1'-0 1/8"  | +0'-11 3/8" | +1'-0 1/8"                   | +1'-0 1/8" | +1'-0 1/8" |
| AB.6               | +2'-7 3/8"                  | +2'-7 3/8"  | +2'-7 1/4"  | -0'-0 1/2"                   | -0'-0 3/8" | -0'-0 1/4" |
| AB.7               | +4'-0 1/2"                  | +2'-11 3/8" | +3'-11 3/8" | +1'-0 1/8"                   | +1'-0 1/8" | +1'-0 1/8" |
| AB.8               | +5'-7 3/8"                  | +5'-7 3/8"  | +5'-6 3/8"  | -0'-0 1/2"                   | -0'-0 3/8" | -0'-0 1/4" |
| AB.9               | -5'-8 1/4"                  | -5'-7 3/8"  | -5'-7 3/8"  | +0'-0 1/8"                   | +0'-0 1/8" | +0'-0 1/8" |
| AB.10              | -4'-1 3/8"                  | -4'-0 1/4"  | -3'-11 3/8" | -1'-0 1/4"                   | -1'-0 1/8" | -1'-0 1/8" |
| AB.11              | -2'-7 3/8"                  | -2'-7 3/8"  | -2'-7 3/8"  | +0'-0 1/8"                   | +0'-0 1/8" | +0'-0 1/8" |
| AB.12              | -1'-0 1/8"                  | -1'-0 1/4"  | -1'-0 1/8"  | -1'-0 1/4"                   | -1'-0 1/8" | -1'-0 1/8" |
| AB.13              | +0'-4 1/8"                  | +0'-4 1/2"  | +0'-4 3/8"  | +0'-0 1/8"                   | +0'-0 1/8" | +0'-0 1/8" |
| AB.14              | +1'-11 3/4"                 | +1'-11 3/4" | +1'-11 3/8" | -1'-0 1/4"                   | -1'-0 1/8" | -1'-0 1/8" |
| AB.15              | +3'-5 3/4"                  | +3'-4 1/2"  | +3'-4 1/2"  | +0'-0 1/8"                   | +0'-0 1/2" | +0'-0 1/8" |
| AB.16              | +5'-0 1/4"                  | +4'-11 3/8" | +4'-11 1/2" | -1'-0 1/4"                   | -1'-0 1/8" | -1'-0 1/8" |

**ANCHOR BOLT LOCATION DETAILS**

**PIER CAP DETAILS**

Scale: 3/8" = 1'-0" (3 Req'd)  
18 Tons Lifting Weight Each  
8.4 CY Concrete Each  
692 # Steel Reinf. Each

| No. | Date    | By  | Chg. | Appr. | Revision  |
|-----|---------|-----|------|-------|---|
| 1   | 4-24-84 | STV | WTD  |       | Changed Square Head Anchor Bolts to Sledge Anchor Bolts |

Professional Engineer seals for Entranco Engineers, Inc. and Burlington Northern Railroad Company.

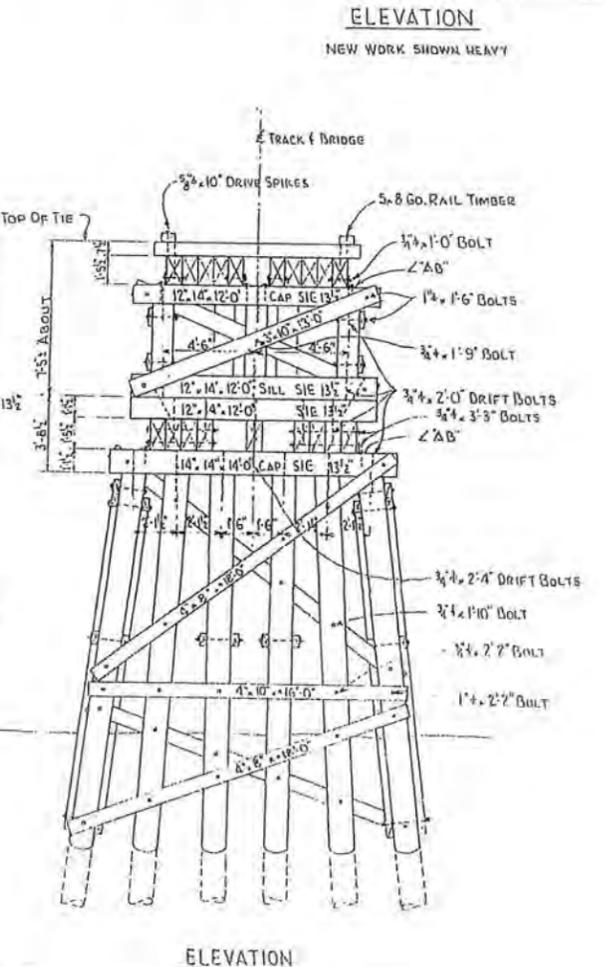
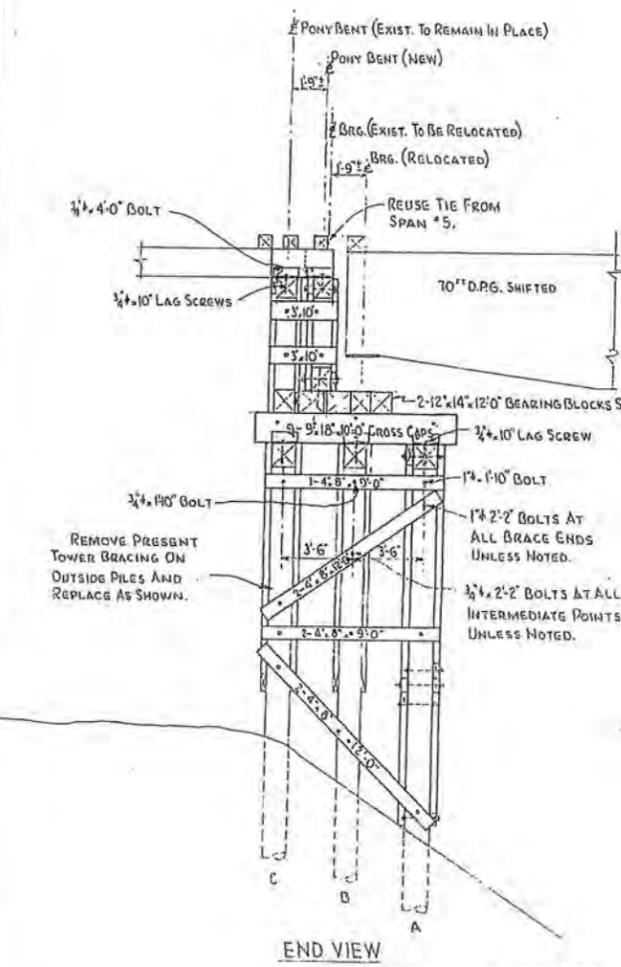
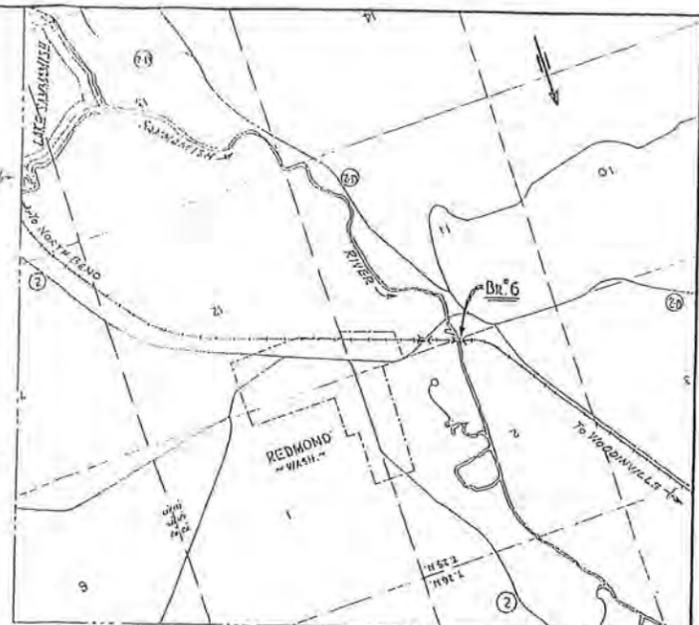
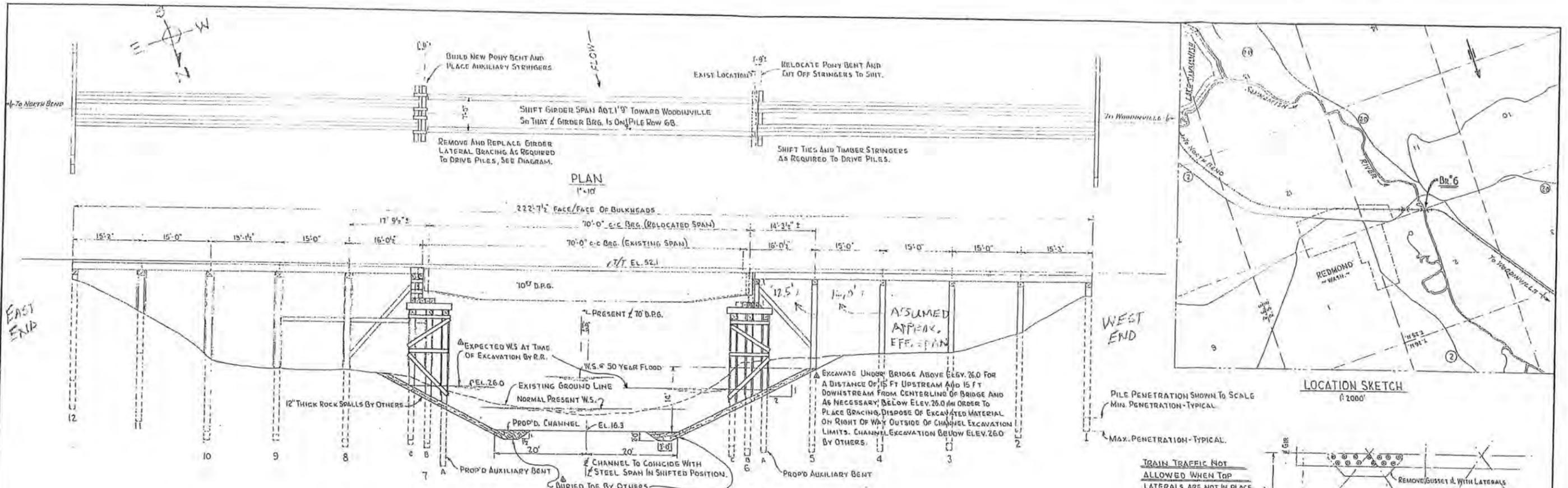
Approved By: [Signature]  
Drawn By: STV  
Designed By: STV  
Checked By: M/Ace  
Date: 4-24-84

**ENTRANCO ENGINEERS, INC.**  
Kirkland and Everett, WA Phoenix, AZ

**BURLINGTON NORTHERN RAILROAD COMPANY**  
BEATTLE REGION PACIFIC DIVISION  
WOODHURVE TO IRAGUAH SPUR  
BRIDGE NO. 6.14 OVER W. LAKE SAMMAMISH PKWAY.  
Scale: 1/8" = 1'-0" (3 Req'd)  
Job No.: 88128-21  
Sheet: 4 of 6  
PLAN NO.





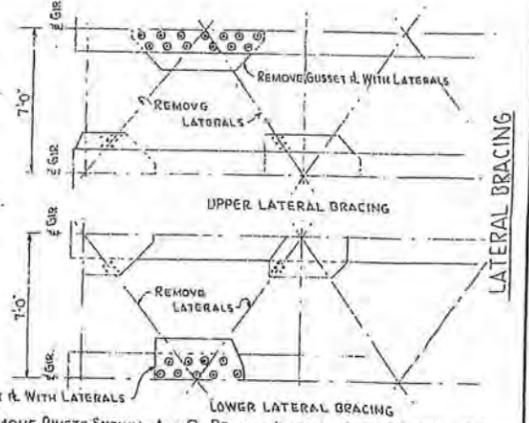


**PIER 6**  
 ADD LOWER TIER OF LATERAL BRACING TO PILES IN ROWS B & C.  
 REPLACE LONGIT. TOWER BRACING WITH NEW BRACING TO INCLUDE ALL 3 ROWS OF PILING.

| BILL OF MATERIAL |                 |  |                  | W.C.L.A. GRADING RULES PARA. 110 |
|------------------|-----------------|--|------------------|----------------------------------|
| QUAN             | SIZE            | DESCRIPTION                            |                  |                                  |
| 6                | 45'-0"          | PILES                                  | TREATED - Row 7A |                                  |
| 6                | 40'-0"          |  | 6A               |                                  |
| 2                | 14'-14", 14'-0" | CAPS                                   | SIE 13 1/2       | 124 a                            |
| 4                | 10'-14", 12'-0" | BRG. BLOCKS (PIER 6)                   |                  |                                  |
| 4                | 12'-14"         | PONY BENT CAPS, SILLS & BLOCKS         |                  |                                  |
| 3                | 12'-12", 4'-0"  | POSTS                                  |                  | 12.5 a                           |
| 2                | 3'-10", 13'-0"  | BRACING                                |                  | 816/504 b                        |
| 4                | 9'-10", 3'-0"   | AUXILIARY STRINGERS                    | SIE 17 1/2       | 124 a                            |
| 18               | 10'-0"          | CROSS CAPS                             |                  |                                  |
| 12               | 4'-8", 18'-0"   | BRACING                                |                  | 816/504 b                        |
| 16               | 12'-0"          |  |                  |                                  |
| 8                | 4'-10", 16'-0"  |  |                  |                                  |
| 4                | 3'-10", 3'-0"   | PONY BENT BRACING                      |                  |                                  |
| 2                | 5'-8", 4'-0"    | GO. RAIL TIMBER                        |                  |                                  |
| 20               | 4'-8", 9'-0"    | BRACING                                |                  |                                  |
| 24               | 3/4" x 3'-3"    | BOLTS                                  |                  |                                  |
| 2                | 4'-0"           |  |                  |                                  |
| 28               | 2'-2"           |  |                  |                                  |
| 32               | 1'-9"           |  |                  |                                  |
| 32               | 1'-10"          |  |                  |                                  |
| 12               | 1'-6"           |  |                  |                                  |
| 4                | 3/4" x 1'-0"    |  |                  |                                  |
| 22               | 3/4" x 10"      | LAG SCREWS                             |                  |                                  |
| 6                | 2'-4"           | DRIFT BOLTS                            |                  |                                  |
| 46               | 2'-0"           |  |                  |                                  |
| 10               | 6-3/4" x 1'-3"  | ANGLES "AB" AS DETAILED ON INDEX M1069 |                  |                                  |
| 8                | 5/8" x 10"      | WASHER HEAD DRIVE SPIKES               |                  |                                  |
| 111              |                 | GRIPCO NUTS FOR 3/4" BOLTS             |                  |                                  |
| 76               |                 | OG WASHERS - 3/4"                      |                  |                                  |
| 202              |                 |  |                  |                                  |
| 152              |                 |  |                  |                                  |

TRAIN TRAFFIC NOT ALLOWED WHEN TOP LATERALS ARE NOT IN PLACE.

H.S. BODY BOLTS - 1 WASH. EA.  
 21 REQ. 3/4" x 3" LG.  
 1 " " x 3 1/2"  
 H.S. STRUCTURAL BOLTS - 2 WASH. EA.  
 4 REQ. 7/8" x 2 1/2" LG.  
 14 " " x 2 1/2"  
 2 " " x 2 3/4"  
 2 " " x 3 3/4"  
 1 " " x 4 1/4"



**NOTES:**  
 TIMBER WORKMANSHIP TO CONFORM TO CONFORM TO N.P.R.Y. INDEX M1069.  
 USE CARE TO PLUG ALL OLD HOLES IN TREATED MATERIAL.  
 TIMBER MATERIAL TO CONFORM TO W.C.L.A. GRADING RULES 11015, PARAGRAPHS AS NOTED IN BILL OF MATERIAL.  
 TIMBER TREATMENT TO CONFORM TO A.W.R.A. SPEC. P31-C7.  
 PILES TO CONFORM TO N.P.R.Y. SPEC. E-110, TREATMENT TO CONFORM TO A.W.R.A. SPEC. P31-C3. BE PREPARED TO USE FOLLOWER TO DRIVE TOP OF PILING BELOW T.O.T. IF REQUIRED TO OBTAIN 25 TON BEARING. BORE HOLES FOR PILES IF NECESSARY TO OBTAIN 10" PENETRATION BELOW BOTTOM OF PROPOSED ROCK SPALLS.

**REFERENCE**  
 V 1072-15 GEN. PLAN  
 A.E.E. 424-1-45  
 FORM 124-A + OCT. 46  
 B.E. 1815  
 C.E. 9497  
 TRK. PROP. 54-4

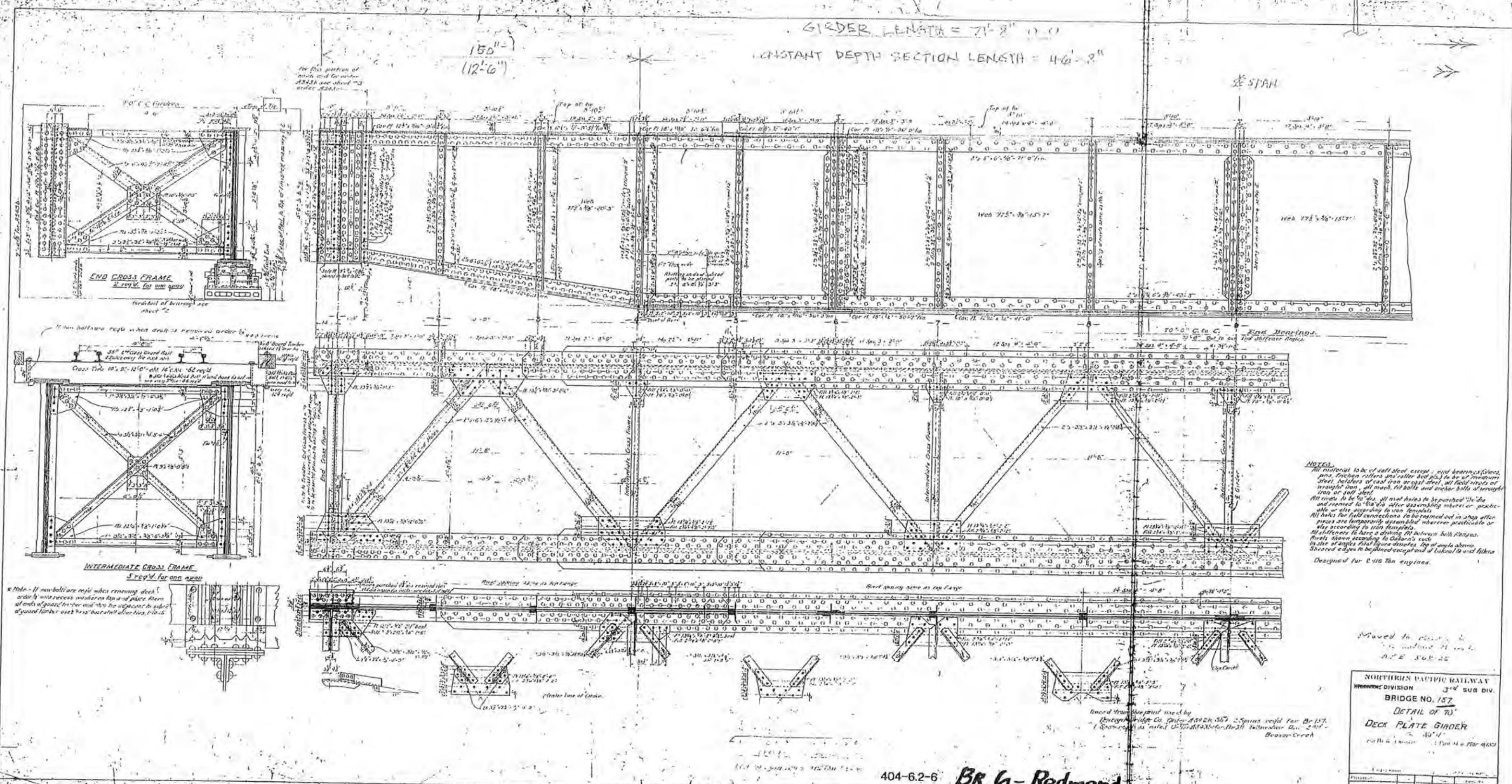
**LIST OF DRAWINGS**  
 1952-53 New Work INDEX M329A  
 EXISTING DOUBLE BENTS " 113006  
 STEEL SHIP PLANS, REFERRED ORDER A 3076 SHEETS 1 & 2  
 STD. PLAN TRESTLE PILE TRESTLE  
 E55 LOGGING, INDEX M1069

404-6.2-6

**NORTHERN PACIFIC RAILWAY ENGINEERING DEPARTMENT**  
 TACOMA DIVISION 12<sup>th</sup> SUB-DIV. SNOQUALMIE BR.  
 BR "6  
 OVER SAMMAMISH RIVER PREDMOND, WASH.  
**REMODEL BRIDGE**  
 ACCT OF U.S.C.E. FLOOD CONTROL PROJECT  
 RECOMMENDED BY: [Signature] APPROVED BY: [Signature]  
 OFFICE OF Bridge Engr. ST. PAUL, MINN. DATE: Nov 29, 1952  
 SCALE: [Blank]  
 DATE: [Blank] REVISIONS: [Blank] DR. [Blank] CIL. [Blank] L.F.C. [Blank] TH. [Blank] L.F.C. [Blank] CIL. [Blank]  
 SHEET 1 OF 1 INDEX 113290-2  
 FILE NO. 1672-15



R12 1/2 x 1/2  
 R13 x 1/2  
 R18 x 1/2  
 (2) L5 x 3 x 5/8



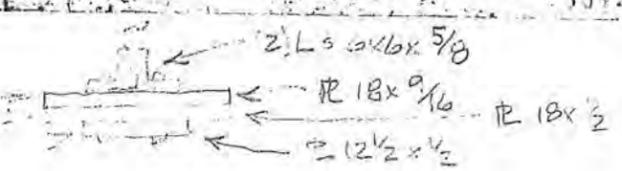
**NOTES**  
 All material to be of soft steel except end bearings, girders, pins, friction rollers and roller bed plate to be of medium steel. Bolters of cast iron or cast steel, all fastenings of wrought iron, all nuts, all bolts and anchor bolts of wrought iron or soft steel.  
 All nuts to be 4/8 dia. all end bolts to be punched in the end and reamed to 4/8 dia. after assembling rollers or anchor bolts or else according to iron templates.  
 All holes for field connections to be reamed out in shops after pieces are temporarily assembled wherever practicable or else according to iron templates.  
 All differences in line a drawing of between both drawings to be shown according to Callender's code.  
 In size of angles first figure denotes leg of angle shown. Shaded edges to be placed except end of L's and end fillets.  
 Designed for 2100 Ton engines.

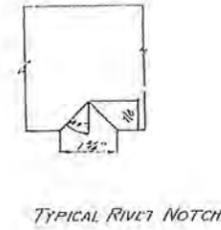
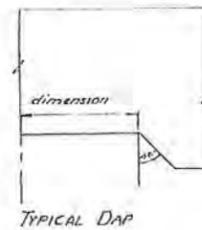
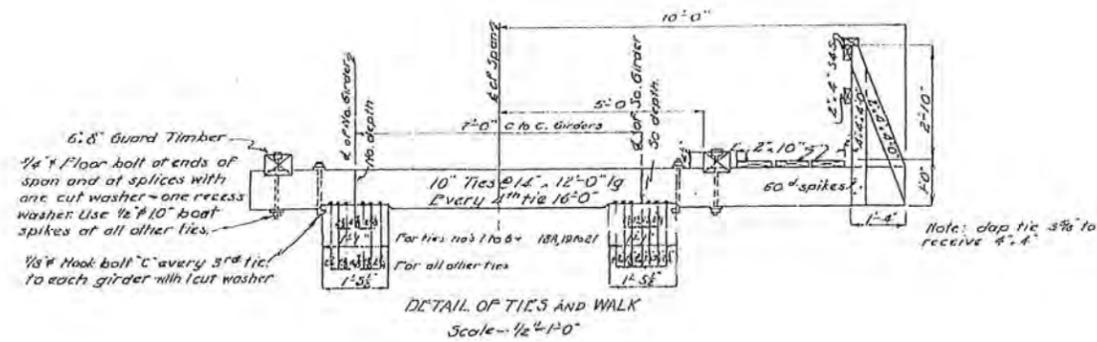
MOVED TO SECTION 2  
 BRIDGE NO. 157  
 DETAIL OF 70'  
 DECK PLATE GIRDER  
 40'-4"  
 1912-13

404-6-2-6 BR. 6-Redmond

STOI

|                                 | LAYER NO. | PLATE No. | DESCRIPTION   |
|---------------------------------|-----------|-----------|---|
| COVER PLATES<br>@ bottom flange | 1         | 1         | 18 x 9/16 x 12'-6 1/4 @ ENDS OF SPAN, BUTTS TO END OF PLATE # 2 |
|                                 | 1         | 2         | 18 x 9/16 x 36'-9" @ CONST. DEPTH MID-SECTION OF SPAN           |
|                                 | 2         | 3         | 18 x 1/2 x 50'-7" @ " " " " " "                                 |
|                                 | 3         | 4         | 12 1/2 x 1/2 x 4'-0" @ " " " " " "                              |

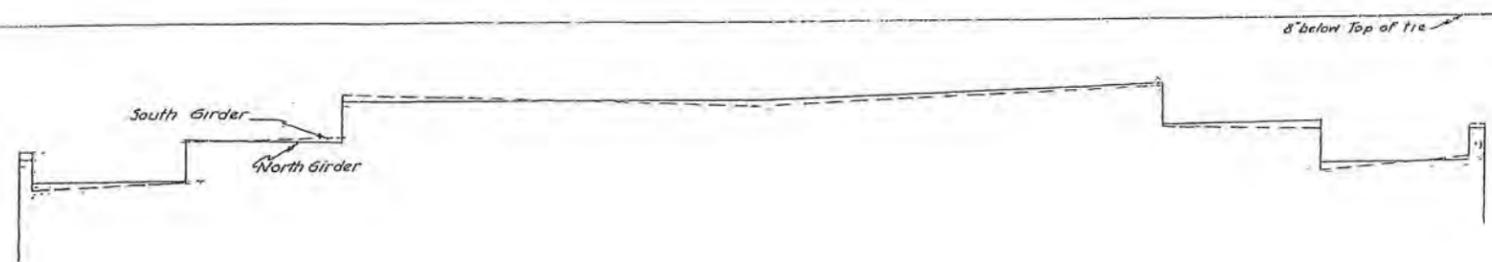




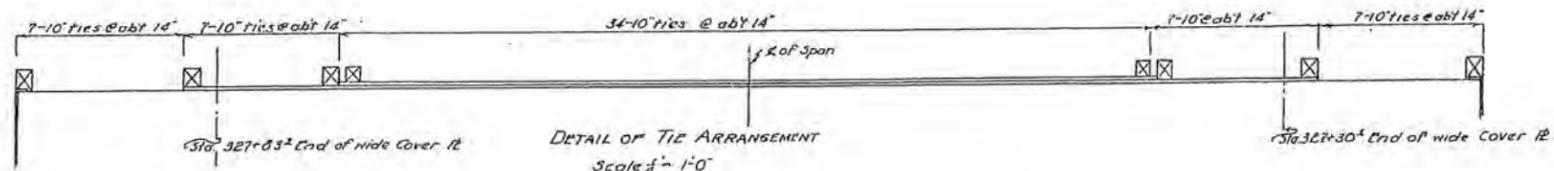
**TIE DAPPING SCHEDULE**

| MARK | NO | No. Dap | So. Dap | SIZE  | LENGTH |
|------|----|---------|---------|-------|--------|
| 1    | 1  | 9 3/8   | 9 3/8   | 10.10 | 16.0   |
| 2    | 3  | 9 3/4   | 9 3/4   | 10.11 | 12.0   |
| 3    | 7  | 9 3/4   | 9 3/4   | 10.11 | 16.0   |
| 4    | 1  | 9 3/4   | 9 3/4   | 10.11 | 12.0   |
| 5    | 1  | 9 3/4   | 9 3/4   | 10.11 | 16.0   |
| 6    | 2  | 9 3/4   | 9 3/4   | 10.10 | 12.0   |
| 7    | 2  | 9 3/4   | 9 3/4   | 16.0  |        |
| 8    | 3  | 8 3/4   | 8 3/4   | 12.0  |        |
| 9    | 3  | 8 3/4   | 8 3/4   | 16.0  |        |
| 10   | 1  | 8 3/4   | 9       | 16.0  |        |
| 11   | 5  | 8 3/4   | 8 3/4   | 12.0  |        |
| 12   | 3  | 9       | 9       | 16.0  |        |
| 13   | 4  | 8 3/8   | 9       | 12.0  |        |
| 14   | 8  | 9       | 9       | 16.0  |        |
| 15   | 2  | 9       | 8 3/8   | 16.0  |        |
| 16   | 6  | 9       | 8 3/8   | 12.0  |        |
| 17   | 2  | 9 3/8   | 9 3/8   | 16.0  |        |
| 18   | 4  | 9 3/8   | 9 3/8   | 12.0  |        |
| 19   | 5  | 9 3/8   | 10      | 10.11 | 12.0   |
| 20   | 1  | 9 3/8   | 10      | 10.11 | 16.0   |
| 21   | 1  | 9 3/8   | 8 3/8   | 10.10 | 12.0   |
| 18A  | 1  | 9 3/8   | 9 3/8   | 10.10 | 12.0   |
| 6A   | 3  | 9 3/4   | 9 3/4   | 10.10 | 12.0   |

Note: Mark north end of ties



ELEVATION OF COVER PLATES  
horizontal - 1/4" = 1'-0"  
Scale - vertical - natural

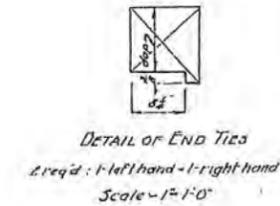


DETAIL OF TIE ARRANGEMENT  
Scale - 1/4" = 1'-0"

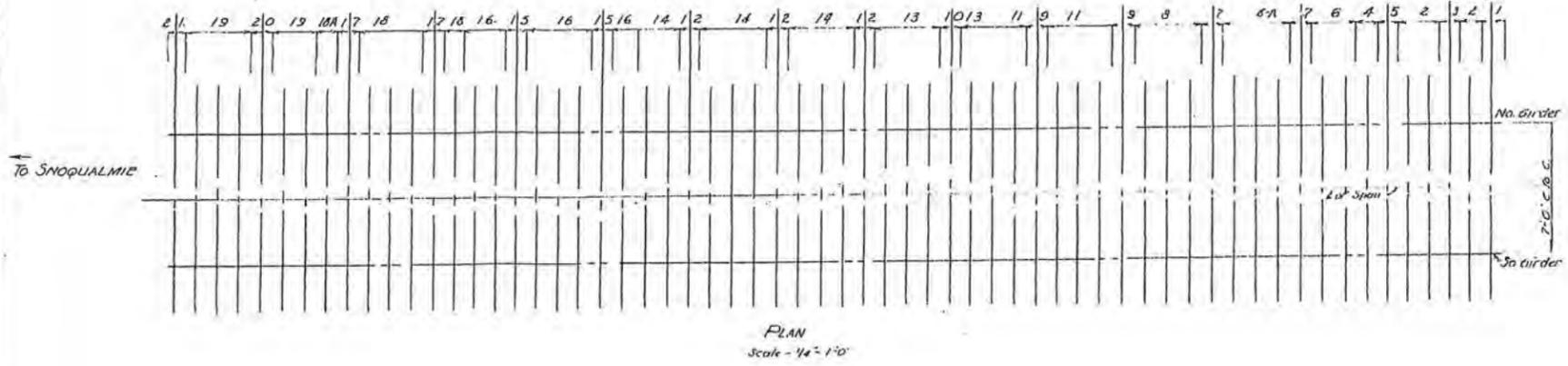
**BILL OF MATERIAL FOR DECK**

| ITEM | NO     | SIZE      | LENGTH | REMARKS                           |
|------|--------|-----------|--------|-----------------------------------|
| 1    | 9      | 10.11     | 12.0   | Ties size W.C.L.A. #2 per 501-222 |
| 2    | 3      | 10.11     | 16.0   | " " " " " " " "                   |
| 3    | 14     | 10.10     | 16.0   | " " " " " " " "                   |
| 4    | 36     | 10.10     | 12.0   | " " " " " " " "                   |
| 5    | 10     | 6.8       | 16.0   | Guard lbc " " " "                 |
| 6    |        |           |        |                                   |
| 7    | 4      | 4.4       | 16.0   | Posts " " " " 224                 |
| 8    | 1      | 4.4       | 4.0    | " " " " " " " "                   |
| 9    | 4      | 2.4       | 16.0   | Bracing " " " " " "               |
| 10   | 1      | 2.4       | 4.0    | " " " " " " " "                   |
| 11   | 15     | 2.4       | 16.0   | Railing 343 " " " "               |
| 12   |        |           |        |                                   |
| 13   | 15     | 2.10      | 16.0   | Plank " " " " " "                 |
| 14   |        |           |        |                                   |
| 15   | 8 1/2  | 60"       |        | Spikes " " " " " "                |
| 16   | 12 3/4 | 20"       |        | Nails " " " " " "                 |
| 17   | 20     | Blues     |        | Recess washers for #1 see 40-224  |
| 18   | 20     | 3/4"      | 1 1/2" | Floor bolts B                     |
| 19   | 44     | 1/8"      | 1'-0"  | Hook bolts C see 40-224           |
| 20   |        |           |        |                                   |
| 21   | 64     | 1/4" thru | 3"     | Cut washers - 1" hole             |
| 22   | 104    | 1/2"      | 10"    | Boat spikes                       |

NOTE:  
If walk + railing is not required, omit items 7 to 16 + make all 16.0 ties 12.0"



DETAIL OF END TIES  
2 req'd - 1 left hand - 1 right hand  
Scale - 1" = 1'-0"



PLAN  
Scale - 1/4" = 1'-0"

NORTHERN PACIFIC RAILWAY  
SEATTLE DIVISION 5<sup>TH</sup> SUB DIV. SNOQUALMIE BRANCH  
BRIDGE NO. 6<sup>L</sup>

Tie PLAN  
Near Redmond Wash

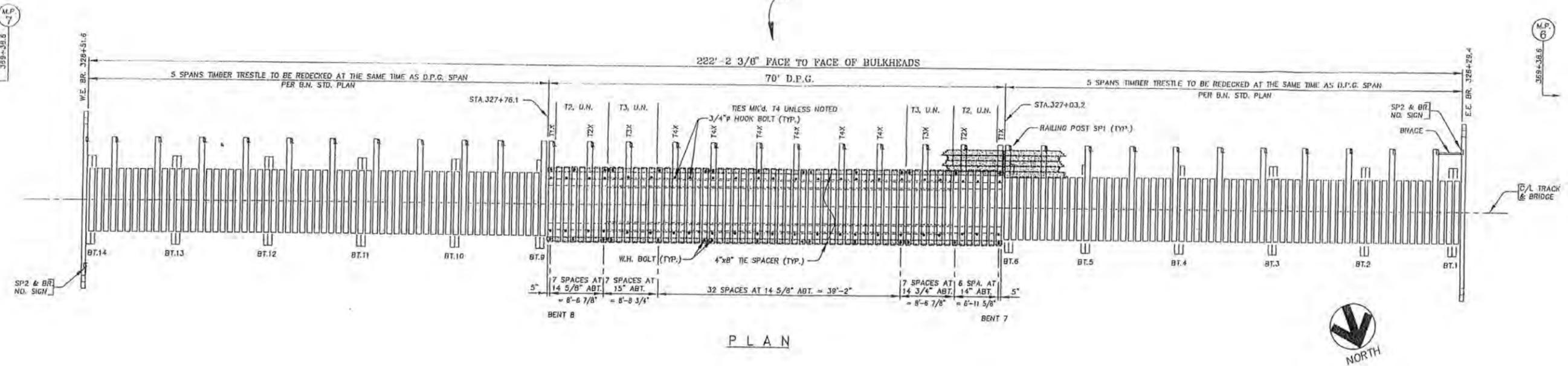
Scale, as noted  
Office of Bridge Engineer St. Paul, Minn. Feb. 1, 1930.  
Approved

|                   |                |
|-------------------|----------------|
| Bridge Engineer   | Chief Engineer |
| Revision of       | Date           |
| D                 | C              |
| Sheet No. 11.0359 | Index No.      |
| Drawn by          | Checked by     |

R.R. WEST TO ISSAQUAH, WA.  
0.3 MILES TO REDMOND, WA.

R.R. EAST TO WOODINVILLE, WA.  
0.9 MILES TO DOUGLAS PALMER, WA.

FLOW



PLAN

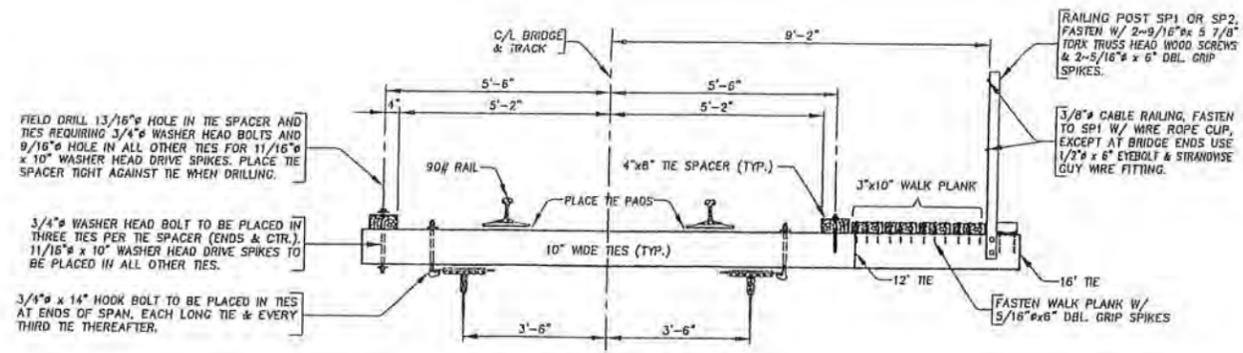
**TIE SCHEDULE**

| QTY. | MARK | SIZE               | REMARKS        |
|------|------|--------------------|----------------|
| 2    | T1X  | 10" x 11" x 16'-0" | S2E TO 10 3/8" |
| 11   | T2   | 10" x 11" x 12'-0" | S2E TO 10 3/4" |
| 2    | T2X  | 10" x 11" x 16'-0" | S2E TO 10 3/4" |
| 10   | T3   | 10" x 11" x 12'-0" | S2E TO 10 1/4" |
| 2    | T3X  | 10" x 11" x 16'-0" | S2E TO 10 1/4" |
| 27   | T4   | 10" x 10" x 12'-0" | S2E TO 9 3/4"  |
| 6    | T4X  | 10" x 10" x 16'-0" | S2E TO 9 3/4"  |

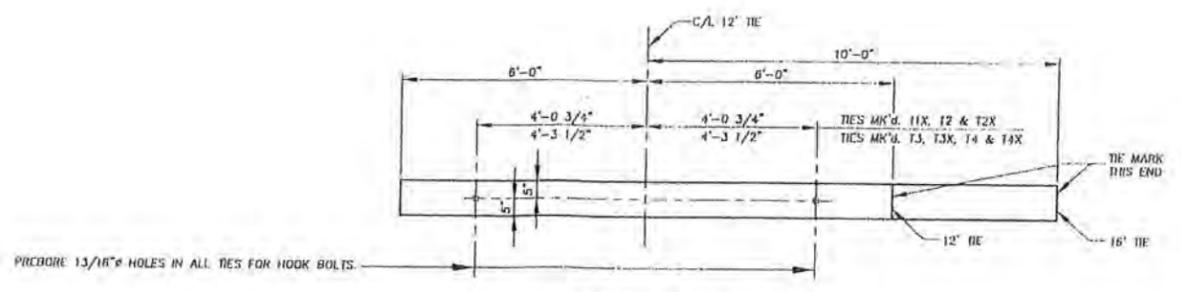
**BILL OF MATERIAL**

| QTY. | CODE         | DESCRIPTION  | MARK | SIZE                           | LENGTH  | REMARKS  |
|------|--------------|--|------|--------------------------------|---------|--|
| 27   | 04-230-19109 | BRIDGE TIES, TREATED, S2E TO 9 3/4"  | T4   | 10"x10"                        | 12'-0"  |  |
| 6    | 04-230-19504 | BRIDGE TIES, TREATED, S2E TO 9 3/4"  | T4X  | 10"x10"                        | 16'-0"  |  |
| 10   | 04-230-20205 | BRIDGE TIES, TREATED, S2E TO 10 1/4"   | T3   | 10"x11"                        | 12'-0"  | BORE, SIZE AND/OR DAP PER TIE SCHEDULE & DETAILS, THIS SHEET, BEFORE TREATING. |
| 2    | 04-230-20601 | BRIDGE TIES, TREATED, S2E TO 10 1/4"   | T3X  | 10"x11"                        | 16'-0"  |  |
| 11   | 04-230-20208 | BRIDGE TIES, TREATED, S2E TO 10 3/4"   | T2   | 10"x11"                        | 12'-0"  |  |
| 2    | 04-230-20601 | BRIDGE TIES, TREATED, S2E TO 10 3/4"   | T2X  | 10"x11"                        | 16'-0"  |  |
| 2    | 04-230-20601 | BRIDGE TIES, TREATED, S2E TO 10 3/8"   | T1X  | 10"x11"                        | 16'-0"  |  |
| 10   | 04-223-60241 | TIE SPACERS, TREATED, ROUGH  |      | 4"x8"                          | 16'-0"  |  |
| 22   | 04-223-40267 | WALK PLANK, TREATED, ROUGH   |      | 3"x10"                         | 14'-0"  | D.P.G. SPAN  |
| 33   |              | RAILING POSTS, STRUCT. STEEL, A.S.T.M. A-36  | SP1  | 1 1/4"x3 1/2"x3"               | 4'-6"   | PER SEATTLE REGION DWG. SEA-SP1  |
| 2    |              | RAILING POSTS, STRUCT. STEEL, A.S.T.M. A-36  | SP2  | 1 1/4"x3 1/2"x3"               | 4'-6"   |  |
| 2    | 15-032-0304X | ANGLE BRACE, PLAIN MATERIAL, A.S.T.M. A-36   |      | 1 1/4"x3 1/2"x3"               | 10'-0"  |  |
| 46   | 11-019-42253 | HOOK BOLTS, 3/4" HOOK, "SEALTITE"  |      | 3/4"                           | 14"     |  |
| 22   | 11-028-88522 | WASHER HEAD MACHINE BOLTS  |      | 3/4"                           | 16"     | FASTEN TIE SPACERS   |
| 8    | 11-028-88549 | WASHER HEAD MACHINE BOLTS  |      | 3/4"                           | 17"     |  |
| 4    | 11-019-13109 | EYEBOLTS, GALV. W/ 2 GALV. HEX NUTS EACH.  |      | 1 1/2"                         | 6"      | USE W/ STRANDWISE  |
| 90   | 01-319-86770 | SPIKES, TIMBER DRIVE, WASHER HEAD  |      | 1 1/16"                        | 10"     | FASTEN TIE SPACERS   |
| 272  | 01-319-19375 | SPIKES, FLAT HEAD  |      | 5/16"                          | 6"      | FASTEN WALK PLANK  |
| 46   | 11-073-80001 | WASHERS, SPRING LOCK, DBL. COIL  |      | 3/4"                           |         | FOR HOOK BOLTS   |
| 78   | 11-071-05255 | WASHERS, FLAT, ROUND, CUT-PLATE  |      | 13/16" ID x 3/32" x 5/16" THK. |         | ALL 3/4" BOLTS   |
| 76   | 11-050-32003 | LOCKNUTS, HWY. HEX, SELF-LOCKING   |      | 3/4"                           |         |  |
| 86   | 45-220-26708 | WIRE ROPE CLIPS, GALV. U-BOLT DESIGN W/ NUTS   |      | 3/8"                           |         | FASTEN CABLE AT ENDS   |
| 4    |              | UNIVERSAL STRANDWISE #5202, GUY WIRE FITTING   |      |                                |         | 2 LINES AT 223'-0"   |
| 446  | 15-076-10208 | I.F., WIRE ROPE, 7-WIRE SINGLE STRAND  |      | 3/8"                           |         |  |
| 54   |              | TORX TRUSS HEAD WOOD SCREWS W/ 1 5/8" SHOULDER   |      | 9/16"                          | 5 7/8"  | FASTEN SP1 & SP2   |
| 2    | 03-800-41006 | BRIDGE NO. SIGN - COMPLETE WITH BACKUP PLATE AND 4-1/2" x 1 1/4" CAD. PL. BOLTS, NUTS, & LOCKWASHERS, AND 1 3/8" O.D. x 12 GA. FLAT GALV. WASHER EACH. | 6.0  |                                |         | 2 DIGIT PER B.N. DWG. #C-8051 LATEST REVISION                                  |
| 10   | 01-315-04208 | TIE PADS, COATED BOTH SIDES  |      | 1/4" x 3/4"                    | 10 3/4" |  |
| 240  | 01-236-24008 | TRACK SPIKES   |      | 5/8" SQ.                       | 6"      |  |

D & B SUPERVISOR TO FURNISH CREOSOTE OIL OR "OSMOPLASTIC", BRIDGE CEMENT, BRIDGE PAINT AND ALL NECESSARY TIMBER AND HARDWARE, INCLUDING WALK PLANK, BUT NOT INCLUDING RAILING CABLE & POSTS AND ASSOCIATED FASTENING HARDWARE REQUIRED TO REDECK TIMBER SPANS PER CURRENT B.N. STANDARD PLANS.



SECTION THRU D.P.G. LOOKING WEST



TIE BORING DETAIL

**GENERAL NOTES**

- ALL TIES TO BE CUT TO EXACT LENGTH, SIZED AND/OR DAPPED AND BOLT HOLES FOR HOOK BOLTS TO BE PREBORED BEFORE DAPPING. ALL OTHER HOLES TO BE DRILLED IN FIELD AND FIELD TREATMENT APPLIED.
- ALL ABRASIONS AND/OR CUTS MADE IN TREATED TIMBER SHALL BE SWABBED WITH CREOSOTE OIL OR "OSMOPLASTIC" AND BOLTS DIPPED IN BRIDGE CEMENT PRIOR TO PLACING.
- HOLES FOR BOLTS TO BE 1/16" LARGER THAN BOLT DIAMETER AND FOR DRIVE SPIKES TO BE 1/8" SMALLER THAN SPIKE DIAMETER.
- CLEAN TOP SURFACES OF STEEL, INACCESSIBLE AFTER DECK IS IN PLACE, PER STEEL STRUCTURES PAINTING COUNCIL, SPEC. SP-10 AND PAINT ONE COAT (5 MIL DRY THICKNESS) WITH STEELCOYTE'S STEELMASTIC #168, BEFORE PLACING TIES.
- PLACE 5/16" THICK x 3" O.D. CUT WASHER UNDER BOLT HEAD OR NUT BEARING ON TIMBER.
- PLACE ALL TIES, WITH TIE MARK ON NORTH SIDE OF BRIDGE.
- RENEW DECK ON TIMBER TRESTLE SPANS AT SAME TIME AS D.P.G. SPAN, PER B.N. STANDARD PLAN.

**BRIDGE DATA**

- 90# RUNNING RAIL
- 7 1/2"x10 1/2" TIE PLATES, PLACE NEW PADS
- PLACE WALK ON RIGHT SIDE
- 27-10" x 10" x 12'-0" BR. TIES
- 6-10" x 10" x 16'-0" BR. TIES
- 21-10" x 11" x 12'-0" BR. TIES
- 6-10" x 11" x 16'-0" BR. TIES
- TIES ON TIMBER SPANS PER B.N. STD. PLAN

**REFERENCE**

- GENERAL PLAN - VF 1072-15 INDEX #113294
- 'OLD' TIE PLAN - VF 1072-15 INDEX #113059
- STEEL SHOP PLANS - FENCOYD ORDER A342b SHT. 1 & 2
- BRIDGE DECK SURVEY - DATED 8/7/07 F.B. pgs. 25-32

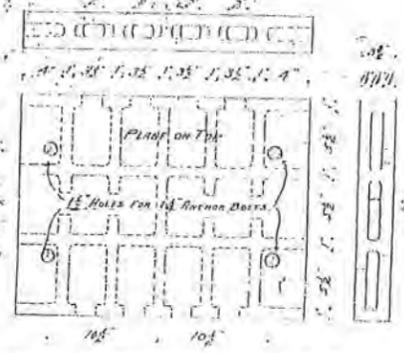
**BURLINGTON NORTHERN RAILROAD COMPANY**  
SEATTLE REGION PACIFIC DIVISION 7th. SUBDIVISION L.S.404

**BRIDGE #6.0**  
1~70' D.P.G. & 10 TIMBER TRESTLE SPANS  
**DECK PLAN**  
OVER SAMMAMISH RIVER AT REDMOND, WA.

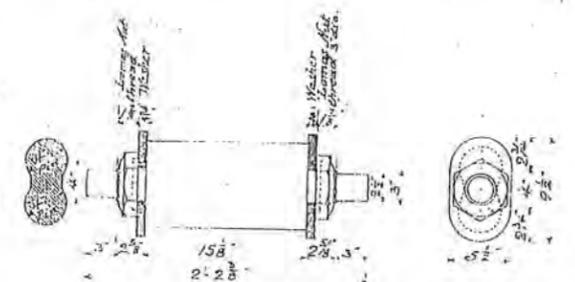
BRIDGE ENGINEER: \_\_\_\_\_ CHIEF ENGINEER: \_\_\_\_\_  
OFFICE OF CHIEF ENGINEER SEATTLE, WASHINGTON DATE: JULY 1988

Date: \_\_\_\_\_ DESIGNS: \_\_\_\_\_ Des. R.E.R. Ck. \_\_\_\_\_  
AUTHORITY: A.P.E. 88-0108  
D.P.G. # 404-6.0-1 Sheet 1 of 1  
PLAN FILE: VF 1072 15

LOCATION CODE: 404-6.2-6

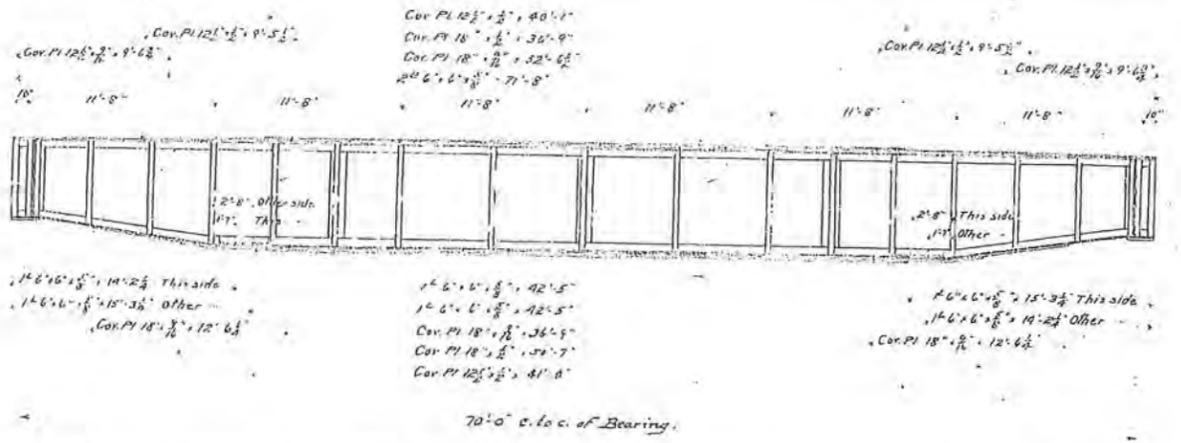
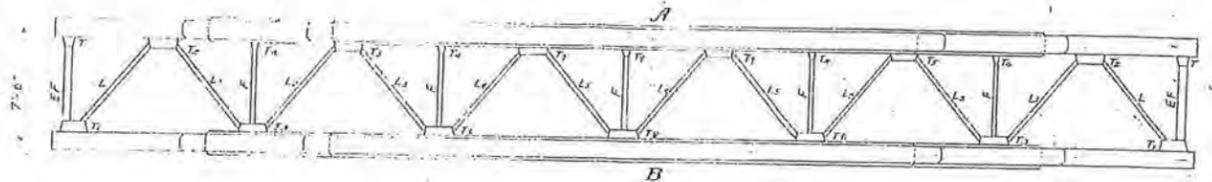


REQ. 4 CAST IRON BOLSTERS, PAT. NO. B. 791.



REQ. 9 GIRDERS, (2 END, 2) Soft Steel.

|    |                 |                    |    |        |                     |
|----|-----------------|--------------------|----|--------|---------------------|
| 8  | End Webs        | 17 1/2" x 8"       | 20 | 3      | fin.                |
| 8  | Webs            | 15"                | 7  |        |                     |
| 8  | Top Flg. Angles | 6" x 6" x 5/8"     | 71 | 8      |                     |
| 8  | Bolt            |                    | 42 | 5      | bent twice          |
| 8  |                 |                    | 15 | 3 1/2  |                     |
| 8  |                 |                    | 19 | 2 1/2  |                     |
| 8  | Top Covers      | 12 1/2" x 3/4"     | 9  | 6 1/2  |                     |
| 8  |                 | 12 1/2" x 3/4"     | 9  | 3 1/2  |                     |
| 4  |                 |                    | 10 | 1      |                     |
| 4  |                 | 18" x 3/4"         | 52 | 6 1/2  |                     |
| 4  |                 | 18" x 3/4"         | 36 | 9      |                     |
| 4  | Bolt            |                    | 50 | 7      | bent twice          |
| 4  |                 | 18" x 3/4"         | 36 | 7      |                     |
| 8  |                 |                    | 12 | 6 1/2  | bent once           |
| 4  |                 | 12 1/2" x 3/4"     | 41 | 0      |                     |
| 8  | End             | 12 1/2" x 3/4"     | 5  | 8 1/2  |                     |
| 16 | Fillers         | 2 1/2" x 3/4"      | 4  | 0      |                     |
| 24 | Splice Pls      | 13" x 3/8"         | 5  | 6      |                     |
| 8  | Base Pls        | 15" x 3/4"         | 1  | 8 1/2  | Plane to 2          |
| 16 | End Stillaners  | 5" x 3 1/2" x 3/8" | 4  | 11 1/2 | at Girders (paired) |
| 8  |                 | 5" x 3 1/2" x 3/8" | 4  | 10 1/2 | B                   |
| 8  |                 | 5" x 3 1/2" x 3/8" | 4  | 11 1/2 | C                   |
| 8  | Fillers         | 7 1/2" x 1/2"      | 4  | 0      |                     |
| 16 | Still Angles    | 5" x 3 1/2" x 3/8" | 5  | 1      |                     |
| 16 | Fillers         | 3 1/2" x 3/8"      | 4  | 5      |                     |
| 16 | Angles          | 5" x 3 1/2" x 3/8" | 5  | 11     |                     |
| 16 | Fillers         | 3 1/2" x 3/8"      | 5  | 0      |                     |
| 8  | Angles          | 5" x 3 1/2" x 3/8" | 6  | 5 1/2  | 1" Crimp            |
| 8  |                 | 3 1/2" x 3/8"      | 6  | 5 1/2  | 1" Crimp            |
| 16 | Fillers         | 3 1/2" x 3/8"      | 5  | 6      |                     |
| 16 | Angles          | 5" x 3 1/2" x 3/8" | 6  | 5 1/2  | 1" Crimp            |
| 56 |                 | 3 1/2" x 3/8"      | 6  | 5 1/2  | 1" Crimp            |
| 32 | Fillers         | 2 1/2" x 3/8"      | 5  | 6      |                     |
| 16 |                 |                    | 5  | 6      |                     |
| 16 | Splice Angles   | 6" x 6" x 3/8"     | 3  | 3      |                     |



REQ. 4 END FRAMES (IN PAIRS), MKD "EF"

|    |         |                        |   |       |             |
|----|---------|------------------------|---|-------|-------------|
| 8  | Angles  | 3 1/2" x 3 1/2" x 3/8" | 6 | 10"   | Soft Steel. |
| 8  |         |                        | 6 | 3     |             |
| 8  | Diag.   | 6" x 4" x 1/2"         | 7 | 6 1/2 |             |
| 16 | Plates  | 15 1/2" x 3/8"         | 1 | 2 1/2 |             |
| 4  |         | 10" x 3/8"             | 1 | 3     |             |
| 16 | Washers | 5" x 3/8"              |   |       |             |

REQ. 10 INTERM FRAMES, MKD "F"

|    |        |                        |   |       |             |
|----|--------|------------------------|---|-------|-------------|
| 20 | Angles | 5 1/2" x 3 1/2" x 3/8" | 6 | 3 1/2 | Soft Steel. |
| 20 | Diag.  |                        | 8 | 4     |             |
| 10 | Plates | 12 1/2" x 3/8"         | 1 | 0 1/2 |             |
| 10 |        | 9" x 3/8"              | 0 | 8 1/2 |             |

TOP LATERALS Soft Steel

|   |         |                        |   |        |            |
|---|---------|------------------------|---|--------|------------|
| 4 | Angles  | 6" x 4" x 3/8"         | 1 | 10 1/2 | L paired   |
| 8 |         | 6" x 4" x 3/8"         | 0 | 6      |            |
| 4 |         | 6" x 4" x 3/8"         | 7 | 10 1/2 | L paired   |
| 8 |         | 5" x 3 1/2" x 3/8"     | 7 | 10 1/2 | 4 L2, 4 L3 |
| 8 |         | 3 1/2" x 3 1/2" x 3/8" | 7 | 8 1/2  | 4 L4, 4 L5 |
| 4 | Plates  | 13 1/2" x 3/8"         | 1 | 5      | T          |
| 4 |         | 13 1/2" x 3/8"         | 2 | 6 1/2  | T1         |
| 4 |         |                        | 3 | 2 1/2  | T2         |
| 4 |         |                        | 2 | 7 1/2  | T3         |
| 4 | Fillers | 2 1/2" x 3/8"          | 2 | 7 1/2  | T4         |
| 8 | Pls     | 10" x 3/8"             | 0 | 8 1/2  | T5         |
| 8 | Fillers | 2 1/2" x 3/8"          | 0 | 10     | T6         |
| 4 | Pls     | 13 1/2" x 3/8"         | 2 | 3      | T7         |
| 4 | Fillers | 2 1/2" x 3/8"          | 2 | 2      | T8         |
| 4 | Pls     | 13 1/2" x 3/8"         | 2 | 2      | T9         |
| 4 | Fillers | 2 1/2" x 3/8"          | 2 | 1 1/2  | T10        |
| 4 | Pls     | 13 1/2" x 3/8"         | 2 | 1 1/2  | T11        |
| 4 | Fillers | 2 1/2" x 3/8"          | 2 | 1      | T12        |
| 4 | Pls     | 13 1/2" x 3/8"         | 2 | 1      | T13        |
| 4 | Fillers | 2 1/2" x 3/8"          | 2 | 1      | T14        |

BOTTOM LATERALS Soft Steel

|   |         |                        |   |       |                |
|---|---------|------------------------|---|-------|----------------|
| 4 | Angles  | 3 1/2" x 3 1/2" x 3/8" | 7 | 7     | L6 paired      |
| 8 |         | 3 1/2" x 3 1/2" x 3/8" | 0 | 9     |                |
| 4 |         | 3 1/2" x 3 1/2" x 3/8" | 7 | 7     | L7 paired      |
| 4 |         | 3 1/2" x 3 1/2" x 3/8" | 0 | 9     |                |
| 4 |         |                        | 0 | 8 1/2 | 8 L8, 8 L9     |
| 4 | Plates  | 12 1/2" x 3/8"         | 2 | 1     | bent B paired  |
| 4 | Angles  | 5" x 2 1/2" x 3/8"     | 1 | 6     | B1             |
| 4 | Pls     | 12 1/2" x 3/8"         | 2 | 4     | B2             |
| 4 | Fillers | 2 1/2" x 3/8"          | 2 | 2     | bent B2 paired |
| 4 | Pls     | 12 1/2" x 3/8"         | 2 | 2     | B3             |
| 4 | Fillers | 2 1/2" x 3/8"          | 2 | 1 1/2 | B4             |
| 4 | Pls     | 12 1/2" x 3/8"         | 2 | 3     | B5             |
| 4 | Fillers | 2 1/2" x 3/8"          | 2 | 2     | B6             |
| 4 | Pls     | 12 1/2" x 3/8"         | 2 | 0     | B7             |
| 4 | Fillers | 2 1/2" x 3/8"          | 2 | 0     | B8             |
| 4 | Pls     | 12 1/2" x 3/8"         | 2 | 0     | B9             |
| 4 | Fillers | 2 1/2" x 3/8"          | 2 | 0     | B10            |

REQ. 4 ROCKER SHOES, MKD "R"

|    |        |                 |   |       |            |
|----|--------|-----------------|---|-------|------------|
| 4  | Plates | 15" x 3/8"      | 1 | 8 1/2 | Plane to 2 |
| 8  | Webs   | 8" x 3/8"       | 1 | 8     |            |
| 8  |        | 8" x 3/8"       | 1 | 8     |            |
| 16 | Angles | 5" x 3" x 3/8"  | 1 | 8     |            |
| 8  | Pl.    | 7" x 3/8" bent  | 1 | 10    |            |
| 16 |        | 12" x 3" x 3/8" | 0 | 7 1/2 |            |

REQ. 4 ROCKER BED PLS, MKD "RB"

|    |         |                 |   |       |  |
|----|---------|-----------------|---|-------|--|
| 4  | Bed Pls | 20 1/2" x 3/8"  | 2 | 5     |  |
| 8  | Webs    | 8" x 3/8"       | 1 | 8     |  |
| 8  |         | 8" x 3/8"       | 1 | 8     |  |
| 16 | Angles  | 5" x 3" x 3/8"  | 1 | 8     |  |
| 8  | Pl.     | 7" x 3/8" bent  | 1 | 10    |  |
| 16 |         | 12" x 3" x 3/8" | 0 | 7 1/2 |  |

REQ. 4 FIXED SHOES, MKD "F"

|    |        |                 |   |       |            |
|----|--------|-----------------|---|-------|------------|
| 4  | Plates | 15" x 3/8"      | 1 | 8 1/2 | Plane to 2 |
| 8  | Webs   | 6" x 3/8"       | 1 | 8     |            |
| 8  | Webs   | 10" x 3/8"      | 1 | 8     |            |
| 16 | Angles | 5" x 3" x 3/8"  | 1 | 8     |            |
| 8  | Pl.    | 7" x 3/8" bent  | 1 | 10    |            |
| 16 |        | 12" x 3" x 3/8" | 0 | 7 1/2 |            |

REQ. 4 FIXED BED PLS, MKD "FB"

|    |            |                 |   |        |     |
|----|------------|-----------------|---|--------|-----|
| 4  | Bed Pls    | 20 1/2" x 3/8"  | 2 | 5      |     |
| 8  | Webs       | 6" x 3/8"       | 1 | 8      | fin |
| 8  | Webs       | 10 1/2" x 3/8"  | 1 | 8      |     |
| 16 | Angles     | 5" x 3" x 3/8"  | 1 | 8      |     |
| 8  | Fillers    | 2 1/2" x 3/8"   | 0 | 10     |     |
| 8  | Pls        | 7" x 3/8" bent  | 1 | 10     |     |
| 16 |            | 12" x 3" x 3/8" | 0 | 7 1/2  |     |
| 8  | Sheet Lead | 24" x 1/2"      | 2 | 19 1/2 |     |

**BR. C-Redwood**  
SHOES, BED PLATES & BILL OF MATERIAL.  
POWDER RIVER BRIDGE #157  
2 TRACKS SINGLE TRACK, 70'-0" DECK PLATE GIRDER  
N.P.R.R.  
(Yellow River Division)  
SCALE 1/2" = 1'-0"  
Pittsburgh Iron Works  
Bridge & Construction Dept.  
Pittsburgh, Pa. Aug. 4, 1896

## Appendix C — Prevailing Wage Rates

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**Overtime Codes**

**Overtime calculations** are based on the hourly rate actually paid to the worker. On public works projects, the hourly rate must be not less than the prevailing rate of wage minus the hourly rate of the cost of fringe benefits actually provided for the worker.

1. ALL HOURS WORKED IN EXCESS OF EIGHT (8) HOURS PER DAY OR FORTY (40) HOURS PER WEEK SHALL BE PAID AT ONE AND ONE-HALF TIMES THE HOURLY RATE OF WAGE.
  - B. All hours worked on Saturdays shall be paid at one and one-half times the hourly rate of wage. All hours worked on Sundays and holidays shall be paid at double the hourly rate of wage.
  - C. The first two (2) hours after eight (8) regular hours Monday through Friday and the first ten (10) hours on Saturday shall be paid at one and one-half times the hourly rate of wage. All other overtime hours and all hours worked on Sundays and holidays shall be paid at double the hourly rate of wage.
  - D. The first two (2) hours before or after a five-eight (8) hour workweek day or a four-ten (10) hour workweek day and the first eight (8) hours worked the next day after either workweek shall be paid at one and one-half times the hourly rate of wage. All additional hours worked and all worked on Sundays and holidays shall be paid at double the hourly rate of wage.
  - E. The first two (2) hours after eight (8) regular hours Monday through Friday and the first eight (8) hours on Saturday shall be paid at one and one-half times the hourly rate of wage. All other hours worked Monday through Saturday, and all hours worked on Sundays and holidays shall be paid at double the hourly rate of wage.
  - F. The first two (2) hours after eight (8) regular hours Monday through Friday and the first ten (10) hours on Saturday shall be paid at one and one-half times the hourly rate of wage. All other overtime hours worked, except Labor Day, shall be paid at double the hourly rate of wage. All hours worked on Labor Day shall be paid at three times the hourly rate of wage.
  - G. The first ten (10) hours worked on Saturdays and the first ten (10) hours worked on a fifth calendar weekday in a four-ten hour schedule, shall be paid at one and one-half times the hourly rate of wage. All hours worked in excess of ten (10) hours per day Monday through Saturday and all hours worked on Sundays and holidays shall be paid at double the hourly rate of wage.
  - H. All hours worked on Saturdays (except makeup days if work is lost due to inclement weather conditions or equipment breakdown) shall be paid at one and one-half times the hourly rate of wage. All hours worked Monday through Saturday over twelve (12) hours and all hours worked on Sundays and holidays shall be paid at double the hourly rate of wage.
  - I. All hours worked on Sundays and holidays shall also be paid at double the hourly rate of wage.
  - J. The first two (2) hours after eight (8) regular hours Monday through Friday and the first ten (10) hours on Saturday shall be paid at one and one-half times the hourly rate of wage. All hours worked over ten (10) hours Monday through Saturday, Sundays and holidays shall be paid at double the hourly rate of wage.
  - K. All hours worked on Saturdays and Sundays shall be paid at one and one-half times the hourly rate of wage. All hours worked on holidays shall be paid at double the hourly rate of wage.
  - M. All hours worked on Saturdays (except makeup days if work is lost due to inclement weather conditions) shall be paid at one and one-half times the hourly rate of wage. All hours worked on Sundays and holidays shall be paid at double the hourly rate of wage.
  - N. All hours worked on Saturdays (except makeup days) shall be paid at one and one-half times the hourly rate of wage. All hours worked on Sundays and holidays shall be paid at double the hourly rate of wage.

**Overtime Codes Continued**

1.
  - O. The first ten (10) hours worked on Saturday shall be paid at one and one-half times the hourly rate of wage. All hours worked on Sundays, holidays and after twelve (12) hours, Monday through Friday and after ten (10) hours on Saturday shall be paid at double the hourly rate of wage.
  - P. All hours worked on Saturdays (except makeup days if circumstances warrant) and Sundays shall be paid at one and one-half times the hourly rate of wage. All hours worked on holidays shall be paid at double the hourly rate of wage.
  - Q. The first two (2) hours after eight (8) regular hours Monday through Friday and up to ten (10) hours worked on Saturdays shall be paid at one and one-half times the hourly rate of wage. All hours worked in excess of ten (10) hours per day Monday through Saturday and all hours worked on Sundays and holidays (except Christmas day) shall be paid at double the hourly rate of wage. All hours worked on Christmas day shall be paid at two and one-half times the hourly rate of wage.
  - R. All hours worked on Sundays and holidays shall be paid at two times the hourly rate of wage.
  - S. The first two (2) hours after eight (8) regular hours Monday through Friday and the first eight (8) hours on Saturday shall be paid at one and one-half times the hourly rate of wage. All hours worked on holidays and all other overtime hours worked, except Labor Day, shall be paid at double the hourly rate of wage. All hours worked on Labor Day shall be paid at three times the hourly rate of wage.
  - U. All hours worked on Saturdays shall be paid at one and one-half times the hourly rate of wage. All hours worked on Sundays and holidays (except Labor Day) shall be paid at two times the hourly rate of wage. All hours worked on Labor Day shall be paid at three times the hourly rate of wage.
  - V. All hours worked on Sundays and holidays (except Thanksgiving Day and Christmas day) shall be paid at one and one-half times the hourly rate of wage. All hours worked on Thanksgiving Day and Christmas day shall be paid at double the hourly rate of wage.
  - W. All hours worked on Saturdays and Sundays (except make-up days due to conditions beyond the control of the employer)) shall be paid at one and one-half times the hourly rate of wage. All hours worked on holidays shall be paid at double the hourly rate of wage.
  - X. The first four (4) hours after eight (8) regular hours Monday through Friday and the first twelve (12) hours on Saturday shall be paid at one and one-half times the hourly rate of wage. All hours worked over twelve (12) hours Monday through Saturday, Sundays and holidays shall be paid at double the hourly rate of wage. When holiday falls on Saturday or Sunday, the day before Saturday, Friday, and the day after Sunday, Monday, shall be considered the holiday and all work performed shall be paid at double the hourly rate of wage.
  - Y. All hours worked outside the hours of 5:00 am and 5:00 pm (or such other hours as may be agreed upon by any employer and the employee) and all hours worked in excess of eight (8) hours per day (10 hours per day for a 4 x 10 workweek) and on Saturdays and holidays (except labor day) shall be paid at one and one-half times the hourly rate of wage. (except for employees who are absent from work without prior approval on a scheduled workday during the workweek shall be paid at the straight-time rate until they have worked 8 hours in a day (10 in a 4 x 10 workweek) or 40 hours during that workweek.) All hours worked Monday through Saturday over twelve (12) hours and all hours worked on Sundays and Labor Day shall be paid at double the hourly rate of wage.
  - Z. All hours worked on Saturdays and Sundays shall be paid at one and one-half times the hourly rate of wage. All hours worked on holidays shall be paid the straight time rate of pay in addition to holiday pay.
2. ALL HOURS WORKED IN EXCESS OF EIGHT (8) HOURS PER DAY OR FORTY (40) HOURS PER WEEK SHALL BE PAID AT ONE AND ONE-HALF TIMES THE HOURLY RATE OF WAGE.
  - B. All hours worked on holidays shall be paid at one and one-half times the hourly rate of wage.
  - C. All hours worked on Sundays shall be paid at one and one-half times the hourly rate of wage. All hours worked on holidays shall be paid at two times the hourly rate of wage.

**Overtime Codes Continued**

2. F. The first eight (8) hours worked on holidays shall be paid at the straight hourly rate of wage in addition to the holiday pay. All hours worked in excess of eight (8) hours on holidays shall be paid at double the hourly rate of wage.
- G. All hours worked on Sunday shall be paid at two times the hourly rate of wage. All hours worked on paid holidays shall be paid at two and one-half times the hourly rate of wage including holiday pay.
- H. All hours worked on Sunday shall be paid at two times the hourly rate of wage. All hours worked on holidays shall be paid at one and one-half times the hourly rate of wage.
- O. All hours worked on Sundays and holidays shall be paid at one and one-half times the hourly rate of wage.
- R. All hours worked on Sundays and holidays and all hours worked over sixty (60) in one week shall be paid at double the hourly rate of wage.
- U. All hours worked on Saturdays shall be paid at one and one-half times the hourly rate of wage. All hours worked over 12 hours in a day or on Sundays and holidays shall be paid at double the hourly rate of wage.
- W. The first two (2) hours after eight (8) regular hours Monday through Friday and the first eight (8) hours on Saturday shall be paid at one and one-half times the hourly rate of wage. All other hours worked Monday through Saturday, and all hours worked on Sundays and holidays shall be paid at double the hourly rate of wage. On a four-day, ten-hour weekly schedule, either Monday thru Thursday or Tuesday thru Friday schedule, all hours worked after ten shall be paid at double the hourly rate of wage. The first eight (8) hours worked on the fifth day shall be paid at one and one-half times the hourly rate of wage. All other hours worked on the fifth, sixth, and seventh days and on holidays shall be paid at double the hourly rate of wage.
3. ALL HOURS WORKED IN EXCESS OF EIGHT (8) HOURS PER DAY OR FORTY (40) HOURS PER WEEK SHALL BE PAID AT ONE AND ONE-HALF TIMES THE HOURLY RATE OF WAGE.
- A. Work performed in excess of eight (8) hours of straight time per day, or ten (10) hours of straight time per day when four ten (10) hour shifts are established, or forty (40) hours of straight time per week, Monday through Friday, or outside the normal shift, and all work on Saturdays shall be paid at time and one-half the straight time rate. Hours worked over twelve hours (12) in a single shift and all work performed after 6:00 pm Saturday to 6:00 am Monday and holidays shall be paid at double the straight time rate of pay. Any shift starting between the hours of 6:00 pm and midnight shall receive an additional one dollar (\$1.00) per hour for all hours worked that shift. The employer shall have the sole discretion to assign overtime work to employees. Primary consideration for overtime work shall be given to employees regularly assigned to the work to be performed on overtime situations. After an employee has worked eight (8) hours at an applicable overtime rate, all additional hours shall be at the applicable overtime rate until such time as the employee has had a break of eight (8) hours or more.
- C. Work performed in excess of eight (8) hours of straight time per day, or ten (10) hours of straight time per day when four ten (10) hour shifts are established, or forty (40) hours of straight time per week, Monday through Friday, or outside the normal shift, and all work on Saturdays shall be paid at one and one-half times the hourly rate of wage. All work performed after 6:00 pm Saturday to 5:00 am Monday and Holidays shall be paid at double the hourly rate of wage. After an employee has worked eight (8) hours at an applicable overtime rate, all additional hours shall be at the applicable overtime rate until such time as the employee has had a break of eight (8) hours or more.
- D. All hours worked between the hours of 6:00 pm and 6:00 am, Monday through Saturday, shall be paid at a premium rate of 15% over the hourly rate of wage. All other hours worked after 6:00 am on Saturdays, shall be paid at one and one-half times the hourly rate of wage. All hours worked on Sundays and holidays shall be paid at double the hourly rate of wage.
- E. All hours worked Sundays and holidays shall be paid at double the hourly rate of wage. Each week, once 40 hours of straight time work is achieved, then any hours worked over 10 hours per day Monday through Saturday shall be paid at double the hourly wage rate.

**Overtime Codes Continued**

3. F. All hours worked on Saturday shall be paid at one and one-half times the hourly rate of wage. All hours worked on Sunday shall be paid at two times the hourly rate of wage. All hours worked on paid holidays shall be paid at two and one-half times the hourly rate of wage including holiday pay.
- H. All work performed on Sundays between March 16th and October 14th and all Holidays shall be compensated for at two (2) times the regular rate of pay. Work performed on Sundays between October 15th and March 15th shall be compensated at one and one half (1-1/2) times the regular rate of pay.
- I. All hours worked on Saturdays shall be paid at one and one-half times the hourly rate of wage. In the event the job is down due to weather conditions during a five day work week (Monday through Friday,) or a four day-ten hour work week (Tuesday through Friday,) then Saturday may be worked as a voluntary make-up day at the straight time rate. However, Saturday shall not be utilized as a make-up day when a holiday falls on Friday. All hours worked Monday through Saturday over twelve (12) hours and all hours worked on Sundays and holidays shall be paid at double the hourly rate of wage.
4. ALL HOURS WORKED IN EXCESS OF EIGHT (8) HOURS PER DAY OR FORTY (40) HOURS PER WEEK SHALL BE PAID AT ONE AND ONE-HALF TIMES THE HOURLY RATE OF WAGE.
- A. All hours worked in excess of eight (8) hours per day or forty (40) hours per week shall be paid at double the hourly rate of wage. All hours worked on Saturdays, Sundays and holidays shall be paid at double the hourly rate of wage.
- B. All hours worked over twelve (12) hours per day and all hours worked on holidays shall be paid at double the hourly rate of wage.
- C. On Monday through Friday, the first four (4) hours of overtime after eight (8) hours of straight time work shall be paid at one and one half (1-1/2) times the straight time rate of pay, unless a four (4) day ten (10) hour workweek has been established. On a four (4) day ten (10) hour workweek scheduled Monday through Thursday, or Tuesday through Friday, the first two (2) hours of overtime after ten (10) hours of straight time work shall be paid at one and one half (1-1/2) times the straight time rate of pay. On Saturday, the first twelve (12) hours of work shall be paid at one and one half (1-1/2) times the straight time rate of pay, except that if the job is down on Monday through Friday due to weather conditions or other conditions outside the control of the employer, the first ten (10) hours on Saturday may be worked at the straight time rate of pay. All hours worked over twelve (12) hours in a day and all hours worked on Sunday and Holidays shall be paid at two (2) times the straight time rate of pay.
- D. All hours worked in excess of eight (8) hours per day or forty (40) hours per week shall be paid at double the hourly rate of wage. All hours worked on Saturday, Sundays and holidays shall be paid at double the hourly rate of pay. Rates include all members of the assigned crew.

**EXCEPTION:**

On all multipole structures and steel transmission lines, switching stations, regulating, capacitor stations, generating plants, industrial plants, associated installations and substations, except those substations whose primary function is to feed a distribution system, will be paid overtime under the following rates:

The first two (2) hours after eight (8) regular hours Monday through Friday of overtime on a regular workday, shall be paid at one and one-half times the hourly rate of wage. All hours in excess of ten (10) hours will be at two (2) times the hourly rate of wage. The first eight (8) hours worked on Saturday will be paid at one and one-half (1-1/2) times the hourly rate of wage. All hours worked in excess of eight (8) hours on Saturday, and all hours worked on Sundays and holidays will be at the double the hourly rate of wage.

All overtime eligible hours performed on the above described work that is energized, shall be paid at the double the hourly rate of wage.

**Overtime Codes Continued**

4. E. The first two (2) hours after eight (8) regular hours Monday through Friday and the first eight (8) hours on Saturday shall be paid at one and one-half times the hourly rate of wage. All other hours worked Monday through Saturday, and all hours worked on Sundays and holidays shall be paid at double the hourly rate of wage.
- On a four-day, ten-hour weekly schedule, either Monday thru Thursday or Tuesday thru Friday schedule, all hours worked after ten shall be paid at double the hourly rate of wage. The Monday or Friday not utilized in the normal four-day, ten hour work week, and Saturday shall be paid at one and one half (1½) times the regular shift rate for the first eight (8) hours. All other hours worked Monday through Saturday, and all hours worked on Sundays and holidays shall be paid at double the hourly rate of wage.
- F. All hours worked between the hours of 6:00 pm and 6:00 am, Monday through Saturday, shall be paid at a premium rate of 20% over the hourly rate of wage. All hours worked on Sundays shall be paid at one and one-half times the hourly rate of wage. All hours worked on holidays shall be paid at double the hourly rate of wage.
- G. All hours worked on Saturdays shall be paid at one and one-half times the hourly rate of wage. All hours worked Monday through Saturday over twelve (12) hours and all hours worked on Sundays and holidays shall be paid at double the hourly rate of wage.

**Holiday Codes**

5. A. Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, Friday after Thanksgiving Day, and Christmas Day (7).
- B. Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, Friday after Thanksgiving Day, the day before Christmas, and Christmas Day (8).
- C. Holidays: New Year's Day, Presidents' Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, the Friday after Thanksgiving Day, And Christmas Day (8).
- D. Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, the Friday and Saturday after Thanksgiving Day, And Christmas Day (8).
- H. Holidays: New Year's Day, Memorial Day, Independence Day, Thanksgiving Day, the Day after Thanksgiving Day, And Christmas (6).
- I. Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, and Christmas Day (6).
- J. Holidays: New Year's Day, Memorial Day, Independence Day, Thanksgiving Day, Friday after Thanksgiving Day, Christmas Eve Day, And Christmas Day (7).
- K. Holidays: New Year's Day, Presidents' Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, Friday After Thanksgiving Day, The Day Before Christmas, And Christmas Day (9).
- L. Holidays: New Year's Day, Martin Luther King Jr. Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, Friday after Thanksgiving Day, And Christmas Day (8).
- N. Holidays: New Year's Day, Presidents' Day, Memorial Day, Independence Day, Labor Day, Veterans' Day, Thanksgiving Day, The Friday After Thanksgiving Day, And Christmas Day (9).
- P. Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, Friday And Saturday After Thanksgiving Day, The Day Before Christmas, And Christmas Day (9). If A Holiday Falls On Sunday, The Following Monday Shall Be Considered As A Holiday.

**Holiday Codes Continued**

5. Q. Paid Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, and Christmas Day (6).
- R. Paid Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, Day After Thanksgiving Day, One-Half Day Before Christmas Day, And Christmas Day. (7 1/2).
- S. Paid Holidays: New Year's Day, Presidents' Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, And Christmas Day (7).
- T. Paid Holidays: New Year's Day, Washington's Birthday, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, The Friday After Thanksgiving Day, Christmas Day, And The Day Before Or After Christmas (9).
- Z. Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Veterans Day, Thanksgiving Day, the Friday after Thanksgiving Day, And Christmas Day (8).

**Holiday Codes Continued**

6. A. Paid Holidays: New Year's Day, Presidents' Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, the Friday after Thanksgiving Day, And Christmas Day (8).
- E. Paid Holidays: New Year's Day, Day Before Or After New Year's Day, Presidents Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, the Friday after Thanksgiving Day, Christmas Day, and a Half-Day On Christmas Eve Day. (9 1/2).
- G. Paid Holidays: New Year's Day, Martin Luther King Jr. Day, Presidents' Day, Memorial Day, Independence Day, Labor Day, Veterans' Day, Thanksgiving Day, the Friday after Thanksgiving Day, Christmas Day, and Christmas Eve Day (11).
- H. Paid Holidays: New Year's Day, New Year's Eve Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, Friday After Thanksgiving Day, Christmas Day, The Day After Christmas, And A Floating Holiday (10).
- I. Paid Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, Friday After Thanksgiving Day, And Christmas Day (7).
- T. Paid Holidays: New Year's Day, Presidents' Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, The Friday After Thanksgiving Day, The Last Working Day Before Christmas Day, And Christmas Day (9).
- Z. Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, Friday after Thanksgiving Day, And Christmas Day (7). If a holiday falls on Saturday, the preceding Friday shall be considered as the holiday. If a holiday falls on Sunday, the following Monday shall be considered as the holiday.

**Holiday Codes Continued**

7. A. Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, the Friday and Saturday after Thanksgiving Day, And Christmas Day (8). Any Holiday Which Falls On A Sunday Shall Be Observed As A Holiday On The Following Monday. If any of the listed holidays falls on a Saturday, the preceding Friday shall be a regular work day.

**Holiday Codes Continued**

7. B. Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, the Friday and Saturday after Thanksgiving Day, And Christmas Day (8). Any holiday which falls on a Sunday shall be observed as a holiday on the following Monday. Any holiday which falls on a Saturday shall be observed as a holiday on the preceding Friday.
- C. Holidays: New Year's Day, Martin Luther King Jr. Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, the Friday after Thanksgiving Day, And Christmas Day (8). Any holiday which falls on a Sunday shall be observed as a holiday on the following Monday. Any holiday which falls on a Saturday shall be observed as a holiday on the preceding Friday.
- D. Paid Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Veteran's Day, Thanksgiving Day, the Friday after Thanksgiving Day, And Christmas Day (8). Unpaid Holidays: President's Day. Any paid holiday which falls on a Sunday shall be observed as a holiday on the following Monday. Any paid holiday which falls on a Saturday shall be observed as a holiday on the preceding Friday.
- E. Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, the Friday after Thanksgiving Day, And Christmas Day (7). Any holiday which falls on a Sunday shall be observed as a holiday on the following Monday. Any holiday which falls on a Saturday shall be observed as a holiday on the preceding Friday.
- F. Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, the Friday after Thanksgiving Day, the last working day before Christmas day and Christmas day (8). Any holiday which falls on a Sunday shall be observed as a holiday on the following Monday. Any holiday which falls on a Saturday shall be observed as a holiday on the preceding Friday.
- G. Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, and Christmas Day (6). Any holiday which falls on a Sunday shall be observed as a holiday on the following Monday.
- H. Holidays: New Year's Day, Martin Luther King Jr. Day, Independence Day, Memorial Day, Labor Day, Thanksgiving Day, the Friday after Thanksgiving Day, the Last Working Day before Christmas Day and Christmas Day (9). Any holiday which falls on a Sunday shall be observed as a holiday on the following Monday. Any holiday which falls on a Saturday shall be observed as a holiday on the preceding Friday.
- I. Holidays: New Year's Day, President's Day, Independence Day, Memorial Day, Labor Day, Thanksgiving Day, The Friday After Thanksgiving Day, The Day Before Christmas Day And Christmas Day (9). Any holiday which falls on a Sunday shall be observed as a holiday on the following Monday. Any holiday which falls on a Saturday shall be observed as a holiday on the preceding Friday.
- J. Holidays: New Year's Day, Independence Day, Memorial Day, Labor Day, Thanksgiving Day and Christmas Day (6). Any holiday which falls on a Sunday shall be observed as a holiday on the following Monday. Any holiday which falls on a Saturday shall be observed as a holiday on the preceding Friday.
- K. Holidays: New Year's Day, Memorial Day, Independence Day, Thanksgiving Day, the Friday and Saturday after Thanksgiving Day, And Christmas Day (8). Any holiday which falls on a Sunday shall be observed as a holiday on the following Monday. Any holiday which falls on a Saturday shall be observed as a holiday on the preceding Friday.
- L. Holidays: New Year's Day, Memorial Day, Labor Day, Independence Day, Thanksgiving Day, the Last Work Day before Christmas Day, And Christmas Day (7). Any holiday which falls on a Sunday shall be observed as a holiday on the following Monday. Any holiday which falls on a Saturday shall be observed as a holiday on the preceding Friday.
- M. Paid Holidays: New Year's Day, The Day after or before New Year's Day, President's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, the Friday after Thanksgiving Day, Christmas Day, And the Day after or before Christmas Day (10). Any holiday which falls on a Sunday shall be observed as a holiday on the following Monday. Any holiday which falls on a Saturday shall be observed as a holiday on the preceding Friday.

**Holiday Codes Continued**

7. N. Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, the Friday after Thanksgiving Day, And Christmas Day (7). Any holiday which falls on a Sunday shall be observed as a holiday on the following Monday. When Christmas falls on a Saturday, the preceding Friday shall be observed as a holiday.
- P. Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, Friday after Thanksgiving Day, And Christmas Day (7). Any holiday which falls on a Sunday shall be observed as a holiday on the following Monday.
- Q. Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, the Friday after Thanksgiving Day, the Last Working Day before Christmas Day and Christmas Day (8). Any holiday which falls on a Sunday shall be observed as a holiday on the following Monday. If any of the listed holidays falls on a Saturday, the preceding Friday shall be a regular work day.
- R. Paid Holidays: New Year's Day, the day after or before New Year's Day, President's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, the Friday after Thanksgiving Day, Christmas Day, and the day after or before Christmas Day (10). If any of the listed holidays fall on Saturday, the preceding Friday shall be observed as the holiday. If any of the listed holidays falls on a Sunday, the day observed by the Nation shall be considered a holiday and compensated accordingly.
- S. Paid Holidays: New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, Friday after Thanksgiving Day, Christmas Day, the Day after Christmas, and A Floating Holiday (9). If any of the listed holidays falls on a Sunday, the day observed by the Nation shall be considered a holiday and compensated accordingly.
- T. Paid Holidays: New Year's Day, the Day after or before New Year's Day, President's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, the Friday after Thanksgiving Day, Christmas Day, and The Day after or before Christmas Day. (10). If any of the listed holidays falls on a Sunday, the day observed by the Nation shall be considered a holiday and compensated accordingly. Any holiday which falls on a Saturday shall be observed as a holiday on the preceding Friday.

**Note Codes**

8. A. In addition to the hourly wage and fringe benefits, the following depth premiums apply to depths of fifty feet or more:  
Over 50' To 100' -\$2.00 per Foot for Each Foot Over 50 Feet  
Over 100' To 150' -\$3.00 per Foot for Each Foot Over 100 Feet  
Over 150' To 220' -\$4.00 per Foot for Each Foot Over 150 Feet  
Over 220' -\$5.00 per Foot for Each Foot Over 220 Feet
- C. In addition to the hourly wage and fringe benefits, the following depth premiums apply to depths of fifty feet or more:  
Over 50' To 100' -\$1.00 per Foot for Each Foot Over 50 Feet  
Over 100' To 150' -\$1.50 per Foot for Each Foot Over 100 Feet  
Over 150' To 200' -\$2.00 per Foot for Each Foot Over 150 Feet  
Over 200' -Divers May Name Their Own Price
- D. Workers working with supplied air on hazmat projects receive an additional \$1.00 per hour.
- L. Workers on hazmat projects receive additional hourly premiums as follows -Level A: \$0.75, Level B: \$0.50, And Level C: \$0.25.
- M. Workers on hazmat projects receive additional hourly premiums as follows: Levels A & B: \$1.00, Levels C & D: \$0.50.

**Note Codes Continued**

Benefit Code Key – Effective 9/2/2015 thru 3/1/2016

8. N. Workers on hazmat projects receive additional hourly premiums as follows -Level A: \$1.00, Level B: \$0.75, Level C: \$0.50, And Level D: \$0.25.
- P. Workers on hazmat projects receive additional hourly premiums as follows -Class A Suit: \$2.00, Class B Suit: \$1.50, Class C Suit: \$1.00, And Class D Suit \$0.50.
- Q. The highest pressure registered on the gauge for an accumulated time of more than fifteen (15) minutes during the shift shall be used in determining the scale paid.
- R. Effective August 31, 2012 – A Traffic Control Supervisor shall be present on the project whenever flagging or spotting or other traffic control labor is being utilized. A Traffic Control Laborer performs the setup, maintenance and removal of all temporary traffic control devices and construction signs necessary to control vehicular, bicycle, and pedestrian traffic during construction operations. Flaggers and Spotters shall be posted where shown on approved Traffic Control Plans or where directed by the Engineer. All flaggers and spotters shall possess a current flagging card issued by the State of Washington, Oregon, Montana, or Idaho. These classifications are only effective on or after August 31, 2012.
- S. Effective August 31, 2012 – A Traffic Control Supervisor shall be present on the project whenever flagging or spotting or other traffic control labor is being utilized. Flaggers and Spotters shall be posted where shown on approved Traffic Control Plans or where directed by the Engineer. All flaggers and spotters shall possess a current flagging card issued by the State of Washington, Oregon, Montana, or Idaho. This classification is only effective on or after August 31, 2012.
- T. Effective August 31, 2012 – A Traffic Control Laborer performs the setup, maintenance and removal of all temporary traffic control devices and construction signs necessary to control vehicular, bicycle, and pedestrian traffic during construction operations. Flaggers and Spotters shall be posted where shown on approved Traffic Control Plans or where directed by the Engineer. All flaggers and spotters shall possess a current flagging card issued by the State of Washington, Oregon, Montana, or Idaho. This classification is only effective on or after August 31, 2012.
- U. Workers on hazmat projects receive additional hourly premiums as follows – Class A Suit: \$2.00, Class B Suit: \$1.50, And Class C Suit: \$1.00. Workers performing underground work receive an additional \$0.40 per hour for any and all work performed underground, including operating, servicing and repairing of equipment. The premium for underground work shall be paid for the entire shift worked. Workers who work suspended by a rope or cable receive an additional \$0.50 per hour. The premium for work suspended shall be paid for the entire shift worked. Workers who do “pioneer” work (break open a cut, build road, etc.) more than one hundred fifty (150) feet above grade elevation receive an additional \$0.50 per hour.

State of Washington  
 Department of Labor & Industries  
 Prevailing Wage Section - Telephone 360-902-5335  
 PO Box 44540, Olympia, WA 98504-4540

### Washington State Prevailing Wage

The PREVAILING WAGES listed here include both the hourly wage rate and the hourly rate of fringe benefits. On public works projects, worker's wage and benefit rates must add to not less than this total. A brief description of overtime calculation requirements are provided on the Benefit Code Key.

#### Journey Level Prevailing Wage Rates for the Effective Date: 1/13/2016

| <u>County</u> | <u>Trade</u>                               | <u>Job Classification</u>         | <u>Wage</u> | <u>Holiday</u> | <u>Overtime</u> | <u>Note</u> |
|---------------|--|-----------------------------------|-------------|----------------|-----------------|-------------|
| King          | <a href="#">Asbestos Abatement Workers</a> | Journey Level                     | \$43.95     | 5D             | 1H              |             |
| King          | <a href="#">Boilermakers</a>               | Journey Level                     | \$64.29     | 5N             | 1C              |             |
| King          | <a href="#">Brick Mason</a>                | Journey Level                     | \$52.82     | 5A             | 1M              |             |
| King          | <a href="#">Brick Mason</a>                | Pointer-Caulker-Cleaner           | \$52.82     | 5A             | 1M              |             |
| King          | <a href="#">Building Service Employees</a> | Janitor                           | \$22.09     | 5S             | 2F              |             |
| King          | <a href="#">Building Service Employees</a> | Traveling Waxer/Shampooer         | \$21.70     | 5S             | 2F              |             |
| King          | <a href="#">Building Service Employees</a> | Window Cleaner (Non-Scaffold)     | \$24.94     | 5S             | 2F              |             |
| King          | <a href="#">Building Service Employees</a> | Window Cleaner (Scaffold)         | \$25.80     | 5S             | 2F              |             |
| King          | <a href="#">Cabinet Makers (In Shop)</a>   | Journey Level                     | \$22.74     |                | 1               |             |
| King          | <a href="#">Carpenters</a>                 | Acoustical Worker                 | \$54.02     | 5D             | 4C              |             |
| King          | <a href="#">Carpenters</a>                 | Bridge, Dock And Wharf Carpenters | \$54.02     | 5D             | 4C              |             |
| King          | <a href="#">Carpenters</a>                 | Carpenter                         | \$54.02     | 5D             | 4C              |             |
| King          | <a href="#">Carpenters</a>                 | Carpenters on Stationary Tools    | \$54.15     | 5D             | 4C              |             |
| King          | <a href="#">Carpenters</a>                 | Creosoted Material                | \$54.12     | 5D             | 4C              |             |
| King          | <a href="#">Carpenters</a>                 | Floor Finisher                    | \$54.02     | 5D             | 4C              |             |
| King          | <a href="#">Carpenters</a>                 | Floor Layer                       | \$54.02     | 5D             | 4C              |             |
| King          | <a href="#">Carpenters</a>                 | Scaffold Erector                  | \$54.02     | 5D             | 4C              |             |
| King          | <a href="#">Cement Masons</a>              | Journey Level                     | \$53.95     | 7A             | 1M              |             |
| King          | <a href="#">Divers &amp; Tenders</a>       | Diver                             | \$107.22    | 5D             | 4C              | 8A          |
| King          | <a href="#">Divers &amp; Tenders</a>       | Diver On Standby                  | \$64.42     | 5D             | 4C              |             |
| King          | <a href="#">Divers &amp; Tenders</a>       | Diver Tender                      | \$58.33     | 5D             | 4C              |             |
| King          | <a href="#">Divers &amp; Tenders</a>       | Surface Rcv & Rov Operator        | \$58.33     | 5D             | 4C              |             |
| King          | <a href="#">Divers &amp; Tenders</a>       | Surface Rcv & Rov Operator Tender | \$54.27     | 5A             | 4C              |             |
| King          | <a href="#">Dredge Workers</a>             | Assistant Engineer                | \$56.44     | 5D             | 3F              |             |
| King          | <a href="#">Dredge Workers</a>             | Assistant Mate (Deckhand)         | \$56.00     | 5D             | 3F              |             |
| King          | <a href="#">Dredge Workers</a>             | Boatmen                           | \$56.44     | 5D             | 3F              |             |
| King          | <a href="#">Dredge Workers</a>             | Engineer Welder                   | \$57.51     | 5D             | 3F              |             |
| King          | <a href="#">Dredge Workers</a>             | Leverman, Hydraulic               | \$58.67     | 5D             | 3F              |             |

|      |  |  |         |           |           |
|------|--|--|---------|-----------|-----------|
| King | <a href="#">Dredge Workers</a>                                   | Mates                                      | \$56.44 | <u>5D</u> | <u>3F</u> |
| King | <a href="#">Dredge Workers</a>                                   | Oiler                                      | \$56.00 | <u>5D</u> | <u>3F</u> |
| King | <a href="#">Drywall Applicator</a>                               | Journey Level                              | \$54.02 | <u>5D</u> | <u>1H</u> |
| King | <a href="#">Drywall Tapers</a>                                   | Journey Level                              | \$54.07 | <u>5P</u> | <u>1E</u> |
| King | <a href="#">Electrical Fixture Maintenance Workers</a>           | Journey Level                              | \$26.59 | <u>5L</u> | <u>1E</u> |
| King | <a href="#">Electricians - Inside</a>                            | Cable Splicer                              | \$66.76 | <u>7C</u> | <u>4E</u> |
| King | <a href="#">Electricians - Inside</a>                            | Cable Splicer (tunnel)                     | \$71.67 | <u>7C</u> | <u>4E</u> |
| King | <a href="#">Electricians - Inside</a>                            | Certified Welder                           | \$64.54 | <u>7C</u> | <u>4E</u> |
| King | <a href="#">Electricians - Inside</a>                            | Certified Welder (tunnel)                  | \$69.22 | <u>7C</u> | <u>4E</u> |
| King | <a href="#">Electricians - Inside</a>                            | Construction Stock Person                  | \$37.19 | <u>7C</u> | <u>4E</u> |
| King | <a href="#">Electricians - Inside</a>                            | Journey Level                              | \$62.30 | <u>7C</u> | <u>4E</u> |
| King | <a href="#">Electricians - Inside</a>                            | Journey Level (tunnel)                     | \$66.76 | <u>7C</u> | <u>4E</u> |
| King | <a href="#">Electricians - Motor Shop</a>                        | Craftsman                                  | \$15.37 |           | <u>1</u>  |
| King | <a href="#">Electricians - Motor Shop</a>                        | Journey Level                              | \$14.69 |           | <u>1</u>  |
| King | <a href="#">Electricians - Powerline Construction</a>            | Cable Splicer                              | \$69.95 | <u>5A</u> | <u>4D</u> |
| King | <a href="#">Electricians - Powerline Construction</a>            | Certified Line Welder                      | \$63.97 | <u>5A</u> | <u>4D</u> |
| King | <a href="#">Electricians - Powerline Construction</a>            | Groundperson                               | \$43.62 | <u>5A</u> | <u>4D</u> |
| King | <a href="#">Electricians - Powerline Construction</a>            | Heavy Line Equipment Operator              | \$63.97 | <u>5A</u> | <u>4D</u> |
| King | <a href="#">Electricians - Powerline Construction</a>            | Journey Level Lineperson                   | \$63.97 | <u>5A</u> | <u>4D</u> |
| King | <a href="#">Electricians - Powerline Construction</a>            | Line Equipment Operator                    | \$53.81 | <u>5A</u> | <u>4D</u> |
| King | <a href="#">Electricians - Powerline Construction</a>            | Pole Sprayer                               | \$63.97 | <u>5A</u> | <u>4D</u> |
| King | <a href="#">Electricians - Powerline Construction</a>            | Powderperson                               | \$47.55 | <u>5A</u> | <u>4D</u> |
| King | <a href="#">Electronic Technicians</a>                           | Journey Level                              | \$31.00 |           | <u>1</u>  |
| King | <a href="#">Elevator Constructors</a>                            | Mechanic                                   | \$82.67 | <u>7D</u> | <u>4A</u> |
| King | <a href="#">Elevator Constructors</a>                            | Mechanic In Charge                         | \$89.40 | <u>7D</u> | <u>4A</u> |
| King | <a href="#">Fabricated Precast Concrete Products</a>             | All Classifications - In-Factory Work Only | \$15.90 | <u>5B</u> | <u>1R</u> |
| King | <a href="#">Fence Erectors</a>                                   | Fence Erector                              | \$15.18 |           | <u>1</u>  |
| King | <a href="#">Flaggers</a>   | Journey Level                              | \$37.26 | <u>7A</u> | <u>3I</u> |
| King | <a href="#">Glaziers</a>   | Journey Level                              | \$56.16 | <u>7L</u> | <u>1Y</u> |
| King | <a href="#">Heat &amp; Frost Insulators And Asbestos Workers</a> | Journeyman                                 | \$63.18 | <u>5J</u> | <u>1S</u> |
| King | <a href="#">Heating Equipment Mechanics</a>                      | Journey Level                              | \$72.83 | <u>7F</u> | <u>1E</u> |
| King | <a href="#">Hod Carriers &amp; Mason Tenders</a>                 | Journey Level                              | \$45.32 | <u>7A</u> | <u>3I</u> |
| King | <a href="#">Industrial Power Vacuum Cleaner</a>                  | Journey Level                              | \$9.47  |           | <u>1</u>  |
| King | <a href="#">Inland Boatmen</a>                                   | Boat Operator                              | \$56.78 | <u>5B</u> | <u>1K</u> |
| King | <a href="#">Inland Boatmen</a>                                   | Cook                                       | \$53.30 | <u>5B</u> | <u>1K</u> |
| King | <a href="#">Inland Boatmen</a>                                   | Deckhand                                   | \$53.30 | <u>5B</u> | <u>1K</u> |

|      |  |                                       |         |           |           |  |
|------|--|---------------------------------------|---------|-----------|-----------|--|
| King | <a href="#">Inland Boatmen</a>   | Deckhand Engineer                     | \$54.32 | <u>5B</u> | <u>1K</u> |  |
| King | <a href="#">Inland Boatmen</a>   | Launch Operator                       | \$55.57 | <u>5B</u> | <u>1K</u> |  |
| King | <a href="#">Inland Boatmen</a>   | Mate                                  | \$55.57 | <u>5B</u> | <u>1K</u> |  |
| King | <a href="#">Inspection/Cleaning/Sealing Of Sewer &amp; Water Systems By Remote Control</a> | Cleaner Operator, Foamer Operator     | \$31.49 |           | <u>1</u>  |  |
| King | <a href="#">Inspection/Cleaning/Sealing Of Sewer &amp; Water Systems By Remote Control</a> | Grout Truck Operator                  | \$11.48 |           | <u>1</u>  |  |
| King | <a href="#">Inspection/Cleaning/Sealing Of Sewer &amp; Water Systems By Remote Control</a> | Head Operator                         | \$24.91 |           | <u>1</u>  |  |
| King | <a href="#">Inspection/Cleaning/Sealing Of Sewer &amp; Water Systems By Remote Control</a> | Technician                            | \$19.33 |           | <u>1</u>  |  |
| King | <a href="#">Inspection/Cleaning/Sealing Of Sewer &amp; Water Systems By Remote Control</a> | Tv Truck Operator                     | \$20.45 |           | <u>1</u>  |  |
| King | <a href="#">Insulation Applicators</a>   | Journey Level                         | \$54.02 | <u>5D</u> | <u>4C</u> |  |
| King | <a href="#">Ironworkers</a>  | Journeyman                            | \$63.53 | <u>7N</u> | <u>10</u> |  |
| King | <a href="#">Laborers</a>   | Air, Gas Or Electric Vibrating Screed | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Airtrac Drill Operator                | \$45.32 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Ballast Regular Machine               | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Batch Weighman                        | \$37.26 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Brick Pavers                          | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Brush Cutter                          | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Brush Hog Feeder                      | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Burner                                | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Caisson Worker                        | \$45.32 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Carpenter Tender                      | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Caulker                               | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Cement Dumper-paving                  | \$44.76 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Cement Finisher Tender                | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Change House Or Dry Shack             | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Chipping Gun (under 30 Lbs.)          | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Chipping Gun(30 Lbs. And Over)        | \$44.76 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Choker Setter                         | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Chuck Tender                          | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Clary Power Spreader                  | \$44.76 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Clean-up Laborer                      | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Concrete Dumper/chute Operator        | \$44.76 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Concrete Form Stripper                | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Concrete Placement Crew               | \$44.76 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Concrete Saw Operator/core Driller    | \$44.76 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a>   | Crusher Feeder                        | \$37.26 | <u>7A</u> | <u>3I</u> |  |

|      |                          |  |         |           |           |  |
|------|--------------------------|--|---------|-----------|-----------|--|
| King | <a href="#">Laborers</a> | Curing Laborer   | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Demolition: Wrecking & Moving (incl. Charred Material)   | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Ditch Digger   | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Diver  | \$45.32 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Drill Operator (hydraulic, diamond)  | \$44.76 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Dry Stack Walls  | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Dump Person  | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Epoxy Technician   | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Erosion Control Worker   | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Faller & Bucker Chain Saw  | \$44.76 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Fine Graders   | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Firewatch  | \$37.26 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Form Setter  | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Gabian Basket Builders   | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | General Laborer  | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Grade Checker & Transit Person   | \$45.32 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Grinders   | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Grout Machine Tender   | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Groutmen (pressure)including Post Tension Beams  | \$44.76 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Guardrail Erector  | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Hazardous Waste Worker (level A)   | \$45.32 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Hazardous Waste Worker (level B)   | \$44.76 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Hazardous Waste Worker (level C)   | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | High Scaler  | \$45.32 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Jackhammer   | \$44.76 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Laserbeam Operator   | \$44.76 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Maintenance Person   | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Manhole Builder-mudman   | \$44.76 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Material Yard Person   | \$43.95 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Motorman-dinky Locomotive  | \$44.76 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Nozzleman (concrete Pump, Green Cutter When Using Combination Of High Pressure Air & Water On Concrete & Rock, Sandblast, Gunite, Shotcrete, Water Bla | \$44.76 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Pavement Breaker   | \$44.76 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Pilot Car  | \$37.26 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Pipe Layer Lead  | \$45.32 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Pipe Layer/tailor  | \$44.76 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Pipe Pot Tender  | \$44.76 | <u>7A</u> | <u>3I</u> |  |
| King | <a href="#">Laborers</a> | Pipe Reliner   | \$44.76 | <u>7A</u> | <u>3I</u> |  |

|      |                          |   |         |           |           |           |
|------|--------------------------|---|---------|-----------|-----------|-----------|
| King | <a href="#">Laborers</a> | Pipe Wrapper  | \$44.76 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Pot Tender  | \$43.95 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Powderman   | \$45.32 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Powderman's Helper                                  | \$43.95 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Power Jacks   | \$44.76 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Railroad Spike Puller - Power                       | \$44.76 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Raker - Asphalt                                     | \$45.32 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Re-timberman  | \$45.32 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Remote Equipment Operator                           | \$44.76 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Rigger/signal Person                                | \$44.76 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Rip Rap Person                                      | \$43.95 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Rivet Buster  | \$44.76 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Rodder  | \$44.76 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Scaffold Erector                                    | \$43.95 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Scale Person  | \$43.95 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Sloper (over 20")                                   | \$44.76 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Sloper Sprayer                                      | \$43.95 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Spreader (concrete)                                 | \$44.76 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Stake Hopper  | \$43.95 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Stock Piler   | \$43.95 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Tamper & Similar Electric, Air & Gas Operated Tools | \$44.76 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Tamper (multiple & Self-propelled)                  | \$44.76 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Timber Person - Sewer (lagger, Shorer & Cribber)    | \$44.76 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Toolroom Person (at Jobsite)                        | \$43.95 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Topper  | \$43.95 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Track Laborer                                       | \$43.95 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Track Liner (power)                                 | \$44.76 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Traffic Control Laborer                             | \$39.84 | <u>7A</u> | <u>3I</u> | <u>8R</u> |
| King | <a href="#">Laborers</a> | Traffic Control Supervisor                          | \$39.84 | <u>7A</u> | <u>3I</u> | <u>8R</u> |
| King | <a href="#">Laborers</a> | Truck Spotter                                       | \$43.95 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Tugger Operator                                     | \$44.76 | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a> | Tunnel Work-Compressed Air Worker 0-30 psi          | \$74.29 | <u>7A</u> | <u>3I</u> | <u>8Q</u> |
| King | <a href="#">Laborers</a> | Tunnel Work-Compressed Air Worker 30.01-44.00 psi   | \$79.32 | <u>7A</u> | <u>3I</u> | <u>8Q</u> |
| King | <a href="#">Laborers</a> | Tunnel Work-Compressed Air Worker 44.01-54.00 psi   | \$83.00 | <u>7A</u> | <u>3I</u> | <u>8Q</u> |
| King | <a href="#">Laborers</a> | Tunnel Work-Compressed Air Worker 54.01-60.00 psi   | \$88.70 | <u>7A</u> | <u>3I</u> | <u>8Q</u> |
| King | <a href="#">Laborers</a> | Tunnel Work-Compressed Air Worker 60.01-64.00 psi   | \$90.82 | <u>7A</u> | <u>3I</u> | <u>8Q</u> |
| King | <a href="#">Laborers</a> | Tunnel Work-Compressed Air Worker 64.01-68.00 psi   | \$95.92 | <u>7A</u> | <u>3I</u> | <u>8Q</u> |
| King | <a href="#">Laborers</a> | Tunnel Work-Compressed Air Worker 68.01-70.00 psi   | \$97.82 | <u>7A</u> | <u>3I</u> | <u>8Q</u> |

|      |  |   |          |           |           |           |
|------|--|---|----------|-----------|-----------|-----------|
| King | <a href="#">Laborers</a>                                   | Tunnel Work-Compressed Air Worker 70.01-72.00 psi | \$99.82  | <u>7A</u> | <u>3I</u> | <u>8Q</u> |
| King | <a href="#">Laborers</a>                                   | Tunnel Work-Compressed Air Worker 72.01-74.00 psi | \$101.82 | <u>7A</u> | <u>3I</u> | <u>8Q</u> |
| King | <a href="#">Laborers</a>                                   | Tunnel Work-Guage and Lock Tender                 | \$45.42  | <u>7A</u> | <u>3I</u> | <u>8Q</u> |
| King | <a href="#">Laborers</a>                                   | Tunnel Work-Miner                                 | \$45.42  | <u>7A</u> | <u>3I</u> | <u>8Q</u> |
| King | <a href="#">Laborers</a>                                   | Vibrator  | \$44.76  | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a>                                   | Vinyl Seamer                                      | \$43.95  | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a>                                   | Watchman  | \$33.86  | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a>                                   | Welder  | \$44.76  | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a>                                   | Well Point Laborer                                | \$44.76  | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers</a>                                   | Window Washer/cleaner                             | \$33.86  | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers - Underground Sewer &amp; Water</a>   | General Laborer & Topman                          | \$43.95  | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Laborers - Underground Sewer &amp; Water</a>   | Pipe Layer  | \$44.76  | <u>7A</u> | <u>3I</u> |           |
| King | <a href="#">Landscape Construction</a>                     | Irrigation Or Lawn Sprinkler Installers           | \$13.56  |           | <u>1</u>  |           |
| King | <a href="#">Landscape Construction</a>                     | Landscape Equipment Operators Or Truck Drivers    | \$28.17  |           | <u>1</u>  |           |
| King | <a href="#">Landscape Construction</a>                     | Landscaping or Planting Laborers                  | \$17.87  |           | <u>1</u>  |           |
| King | <a href="#">Lathers</a>                                    | Journey Level                                     | \$54.02  | <u>5D</u> | <u>1H</u> |           |
| King | <a href="#">Marble Setters</a>                             | Journey Level                                     | \$52.82  | <u>5A</u> | <u>1M</u> |           |
| King | <a href="#">Metal Fabrication (In Shop)</a>                | Fitter  | \$15.86  |           | <u>1</u>  |           |
| King | <a href="#">Metal Fabrication (In Shop)</a>                | Laborer   | \$9.78   |           | <u>1</u>  |           |
| King | <a href="#">Metal Fabrication (In Shop)</a>                | Machine Operator                                  | \$13.04  |           | <u>1</u>  |           |
| King | <a href="#">Metal Fabrication (In Shop)</a>                | Painter   | \$11.10  |           | <u>1</u>  |           |
| King | <a href="#">Metal Fabrication (In Shop)</a>                | Welder  | \$15.48  |           | <u>1</u>  |           |
| King | <a href="#">Millwright</a>                                 | Journey Level                                     | \$55.52  | <u>5D</u> | <u>4C</u> |           |
| King | <a href="#">Modular Buildings</a>                          | Cabinet Assembly                                  | \$11.56  |           | <u>1</u>  |           |
| King | <a href="#">Modular Buildings</a>                          | Electrician                                       | \$11.56  |           | <u>1</u>  |           |
| King | <a href="#">Modular Buildings</a>                          | Equipment Maintenance                             | \$11.56  |           | <u>1</u>  |           |
| King | <a href="#">Modular Buildings</a>                          | Plumber   | \$11.56  |           | <u>1</u>  |           |
| King | <a href="#">Modular Buildings</a>                          | Production Worker                                 | \$9.47   |           | <u>1</u>  |           |
| King | <a href="#">Modular Buildings</a>                          | Tool Maintenance                                  | \$11.56  |           | <u>1</u>  |           |
| King | <a href="#">Modular Buildings</a>                          | Utility Person                                    | \$11.56  |           | <u>1</u>  |           |
| King | <a href="#">Modular Buildings</a>                          | Welder  | \$11.56  |           | <u>1</u>  |           |
| King | <a href="#">Painters</a>                                   | Journey Level                                     | \$37.80  | <u>6Z</u> | <u>2B</u> |           |
| King | <a href="#">Pile Driver</a>                                | Journey Level                                     | \$54.27  | <u>5D</u> | <u>4C</u> |           |
| King | <a href="#">Plasterers</a>                                 | Journey Level                                     | \$51.68  | <u>7Q</u> | <u>1R</u> |           |
| King | <a href="#">Playground &amp; Park Equipment Installers</a> | Journey Level                                     | \$9.47   |           | <u>1</u>  |           |
| King | <a href="#">Plumbers &amp; Pipefitters</a>                 | Journey Level                                     | \$74.69  | <u>6Z</u> | <u>1G</u> |           |
| King | <a href="#">Power Equipment Operators</a>                  | Asphalt Plant Operators                           | \$56.94  | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a>                  | Assistant Engineer                                | \$53.57  | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a>                  | Barrier Machine (zipper)                          | \$56.44  | <u>7A</u> | <u>3C</u> | <u>8P</u> |

|      |   |  |         |           |           |           |
|------|---|--|---------|-----------|-----------|-----------|
| King | <a href="#">Power Equipment Operators</a> | Batch Plant Operator, Concrete   | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Bobcat   | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Brokk - Remote Demolition Equipment  | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Brooms   | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Bump Cutter  | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Cableways  | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Chipper  | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Compressor   | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Concrete Pump: Truck Mount With Boom Attachment Over 42 M                            | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Concrete Finish Machine -laser Screed  | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Concrete Pump - Mounted Or Trailer High Pressure Line Pump, Pump High Pressure.      | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Concrete Pump: Truck Mount With Boom Attachment Up To 42m                            | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Conveyors  | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Cranes Friction: 200 tons and over   | \$58.67 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Cranes: 20 Tons Through 44 Tons With Attachments                                     | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Cranes: 100 Tons Through 199 Tons, Or 150' Of Boom (Including Jib With Attachments)  | \$57.51 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Cranes: 200 tons- 299 tons, or 250' of boom including jib with attachments           | \$58.10 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Cranes: 300 tons and over or 300' of boom including jib with attachments             | \$58.67 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Cranes: 45 Tons Through 99 Tons, Under 150' Of Boom (including Jib With Attachments) | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Cranes: A-frame - 10 Tons And Under  | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Cranes: Friction cranes through 199 tons   | \$58.10 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Cranes: Through 19 Tons With Attachments A-frame Over 10 Tons                        | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Crusher  | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Deck Engineer/deck Winches (power)   | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Derricks, On Building Work   | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Dozers D-9 & Under   | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Drill Oilers: Auger Type, Truck Or Crane Mount                                       | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |

|      |   |  |         |           |           |           |
|------|---|--|---------|-----------|-----------|-----------|
| King | <a href="#">Power Equipment Operators</a> | Drilling Machine   | \$57.51 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Elevator And Man-lift:<br>Permanent And Shaft Type                           | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Finishing Machine, Bidwell And<br>Gamaco & Similar Equipment                 | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Forklift: 3000 Lbs And Over With<br>Attachments                              | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Forklifts: Under 3000 Lbs. With<br>Attachments                               | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Grade Engineer: Using Blue<br>Prints, Cut Sheets, Etc                        | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Gradechecker/stakeman  | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Guardrail Punch  | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Hard Tail End Dump Articulating<br>Off- Road Equipment 45 Yards. &<br>Over   | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Hard Tail End Dump Articulating<br>Off-road Equipment Under 45<br>Yards      | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Horizontal/directional Drill<br>Locator                                      | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Horizontal/directional Drill<br>Operator                                     | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Hydralifts/boom Trucks Over 10<br>Tons                                       | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Hydralifts/boom Trucks, 10 Tons<br>And Under                                 | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Loader, Overhead 8 Yards. &<br>Over  | \$57.51 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Loader, Overhead, 6 Yards. But<br>Not Including 8 Yards                      | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Loaders, Overhead Under 6<br>Yards   | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Loaders, Plant Feed  | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Loaders: Elevating Type Belt   | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Locomotives, All   | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Material Transfer Device   | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Mechanics, All (leadmen - \$0.50<br>Per Hour Over Mechanic)                  | \$57.51 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Motor Patrol Graders   | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Mucking Machine, Mole, Tunnel<br>Drill, Boring, Road Header<br>And/or Shield | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Oil Distributors, Blower<br>Distribution & Mulch Seeding<br>Operator         | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Outside Hoists (elevators And<br>Manlifts), Air Tuggers, strato              | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Overhead, Bridge Type Crane: 20<br>Tons Through 44 Tons                      | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |

|      |   |  |         |           |           |           |
|------|---|--|---------|-----------|-----------|-----------|
| King | <a href="#">Power Equipment Operators</a> | Overhead, Bridge Type: 100 Tons And Over                           | \$57.51 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Overhead, Bridge Type: 45 Tons Through 99 Tons                     | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Pavement Breaker   | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Pile Driver (other Than Crane Mount)                               | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Plant Oiler - Asphalt, Crusher                                     | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Posthole Digger, Mechanical  | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Power Plant  | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Pumps - Water  | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Quad 9, Hd 41, D10 And Over  | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Quick Tower - No Cab, Under 100 Feet In Height Based To Boom       | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Remote Control Operator On Rubber Tired Earth Moving Equipment     | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Rigger And Bellman   | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Rigger/Signal Person, Bellman (Certified)                          | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Rollagon   | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Roller, Other Than Plant Mix                                       | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Roller, Plant Mix Or Multi-lift Materials                          | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Roto-mill, Roto-grinder  | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Saws - Concrete  | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Scraper, Self Propelled Under 45 Yards                             | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Scrapers - Concrete & Carry All                                    | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Scrapers, Self-propelled: 45 Yards And Over                        | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Service Engineers - Equipment                                      | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Shotcrete/gunite Equipment   | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Shovel , Excavator, Backhoe, Tractors Under 15 Metric Tons.        | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Shovel, Excavator, Backhoe: Over 30 Metric Tons To 50 Metric Tons  | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Shovel, Excavator, Backhoes, Tractors: 15 To 30 Metric Tons        | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Shovel, Excavator, Backhoes: Over 50 Metric Tons To 90 Metric Tons | \$57.51 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Shovel, Excavator, Backhoes: Over 90 Metric Tons                   | \$58.10 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Slipform Pavers  | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Spreader, Topsider & Screedman                                     | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a> | Subgrader Trimmer  | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |

|      |   |   |         |           |           |           |
|------|---|---|---------|-----------|-----------|-----------|
| King | <a href="#">Power Equipment Operators</a>                               | Tower Bucket Elevators  | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a>                               | Tower Crane Up To 175' In Height Base To Boom                                   | \$57.51 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a>                               | Tower Crane: over 175' through 250' in height, base to boom                     | \$58.10 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a>                               | Tower Cranes: over 250' in height from base to boom                             | \$58.67 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a>                               | Transporters, All Track Or Truck Type   | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a>                               | Trenching Machines  | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a>                               | Truck Crane Oiler/driver - 100 Tons And Over                                    | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a>                               | Truck Crane Oiler/driver Under 100 Tons   | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a>                               | Truck Mount Portable Conveyor   | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a>                               | Welder  | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a>                               | Wheel Tractors, Farmall Type  | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators</a>                               | Yo Yo Pay Dozer   | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Asphalt Plant Operators   | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Assistant Engineer  | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Barrier Machine (zipper)  | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Batch Plant Operator, Concrete  | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Bobcat  | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Brokk - Remote Demolition Equipment   | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Brooms  | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Bump Cutter   | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Cableways   | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Chipper   | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Compressor  | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Concrete Pump: Truck Mount With Boom Attachment Over 42 M                       | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Concrete Finish Machine -laser Screed   | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Concrete Pump - Mounted Or Trailer High Pressure Line Pump, Pump High Pressure. | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Concrete Pump: Truck Mount With Boom Attachment Up To 42m                       | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |

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|------|---|--|---------|-----------|-----------|-----------|
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Conveyors  | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Cranes Friction: 200 tons and over   | \$58.67 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Cranes: 20 Tons Through 44 Tons With Attachments                                     | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Cranes: 100 Tons Through 199 Tons, Or 150' Of Boom (Including Jib With Attachments)  | \$57.51 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Cranes: 200 tons- 299 tons, or 250' of boom including jib with attachments           | \$58.10 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Cranes: 300 tons and over or 300' of boom including jib with attachments             | \$58.67 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Cranes: 45 Tons Through 99 Tons, Under 150' Of Boom (including Jib With Attachments) | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Cranes: A-frame - 10 Tons And Under  | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Cranes: Friction cranes through 199 tons   | \$58.10 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Cranes: Through 19 Tons With Attachments A-frame Over 10 Tons                        | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Crusher  | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Deck Engineer/deck Winches (power)   | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Derricks, On Building Work   | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Dozers D-9 & Under   | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Drill Oilers: Auger Type, Truck Or Crane Mount                                       | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Drilling Machine   | \$57.51 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Elevator And Man-lift: Permanent And Shaft Type                                      | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Finishing Machine, Bidwell And Gamaco & Similar Equipment                            | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Forklift: 3000 Lbs And Over With Attachments   | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Forklifts: Under 3000 Lbs. With Attachments  | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Grade Engineer: Using Blue Prints, Cut Sheets, Etc                                   | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Gradechecker/stakeman  | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Guardrail Punch  | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |

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|------|---|--|---------|-----------|-----------|-----------|
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Hard Tail End Dump Articulating Off- Road Equipment 45 Yards. & Over   | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Hard Tail End Dump Articulating Off-road Equipment Under 45 Yards      | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Horizontal/directional Drill Locator                                   | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Horizontal/directional Drill Operator                                  | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Hydralifts/boom Trucks Over 10 Tons                                    | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Hydralifts/boom Trucks, 10 Tons And Under                              | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Loader, Overhead 8 Yards. & Over                                       | \$57.51 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Loader, Overhead, 6 Yards. But Not Including 8 Yards                   | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Loaders, Overhead Under 6 Yards  | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Loaders, Plant Feed  | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Loaders: Elevating Type Belt   | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Locomotives, All   | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Material Transfer Device   | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Mechanics, All (leadmen - \$0.50 Per Hour Over Mechanic)               | \$57.51 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Motor Patrol Graders   | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Mucking Machine, Mole, Tunnel Drill, Boring, Road Header And/or Shield | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Oil Distributors, Blower Distribution & Mulch Seeding Operator         | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Outside Hoists (elevators And Manlifts), Air Tuggers, strato           | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Overhead, Bridge Type Crane: 20 Tons Through 44 Tons                   | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Overhead, Bridge Type: 100 Tons And Over                               | \$57.51 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Overhead, Bridge Type: 45 Tons Through 99 Tons                         | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Pavement Breaker   | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Pile Driver (other Than Crane Mount)                                   | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Plant Oiler - Asphalt, Crusher   | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |

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|------|---|--|---------|-----------|-----------|-----------|
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Posthole Digger, Mechanical  | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Power Plant  | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Pumps - Water  | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Quad 9, Hd 41, D10 And Over  | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Quick Tower - No Cab, Under 100 Feet In Height Based To Boom       | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Remote Control Operator On Rubber Tired Earth Moving Equipment     | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Rigger And Bellman   | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Rigger/Signal Person, Bellman (Certified)                          | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Rollagon   | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Roller, Other Than Plant Mix                                       | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Roller, Plant Mix Or Multi-lift Materials                          | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Roto-mill, Roto-grinder  | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Saws - Concrete  | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Scraper, Self Propelled Under 45 Yards                             | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Scrapers - Concrete & Carry All                                    | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Scrapers, Self-propelled: 45 Yards And Over                        | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Service Engineers - Equipment                                      | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Shotcrete/gunite Equipment   | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Shovel , Excavator, Backhoe, Tractors Under 15 Metric Tons.        | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Shovel, Excavator, Backhoe: Over 30 Metric Tons To 50 Metric Tons  | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Shovel, Excavator, Backhoes, Tractors: 15 To 30 Metric Tons        | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Shovel, Excavator, Backhoes: Over 50 Metric Tons To 90 Metric Tons | \$57.51 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Shovel, Excavator, Backhoes: Over 90 Metric Tons                   | \$58.10 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Slipform Pavers  | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |

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|------|---|---|---------|-----------|-----------|-----------|
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Spreader, Topsider & Screedman                              | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Subgrader Trimmer   | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Tower Bucket Elevators                                      | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Tower Crane Up To 175' In Height Base To Boom               | \$57.51 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Tower Crane: over 175' through 250' in height, base to boom | \$58.10 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Tower Cranes: over 250' in height from base to boom         | \$58.67 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Transporters, All Track Or Truck Type                       | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Trenching Machines  | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Truck Crane Oiler/driver - 100 Tons And Over                | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Truck Crane Oiler/driver Under 100 Tons                     | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Truck Mount Portable Conveyor                               | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Welder  | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Wheel Tractors, Farmall Type                                | \$53.57 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Equipment Operators-Underground Sewer &amp; Water</a> | Yo Yo Pay Dozer   | \$56.44 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Power Line Clearance Tree Trimmers</a>                      | Journey Level In Charge                                     | \$45.75 | <u>5A</u> | <u>4A</u> |           |
| King | <a href="#">Power Line Clearance Tree Trimmers</a>                      | Spray Person  | \$43.38 | <u>5A</u> | <u>4A</u> |           |
| King | <a href="#">Power Line Clearance Tree Trimmers</a>                      | Tree Equipment Operator                                     | \$45.75 | <u>5A</u> | <u>4A</u> |           |
| King | <a href="#">Power Line Clearance Tree Trimmers</a>                      | Tree Trimmer  | \$40.84 | <u>5A</u> | <u>4A</u> |           |
| King | <a href="#">Power Line Clearance Tree Trimmers</a>                      | Tree Trimmer Groundperson                                   | \$30.74 | <u>5A</u> | <u>4A</u> |           |
| King | <a href="#">Refrigeration &amp; Air Conditioning Mechanics</a>          | Journey Level   | \$73.51 | <u>6Z</u> | <u>1G</u> |           |
| King | <a href="#">Residential Brick Mason</a>                                 | Journey Level   | \$52.82 | <u>5A</u> | <u>1M</u> |           |
| King | <a href="#">Residential Carpenters</a>                                  | Journey Level   | \$28.20 |           | <u>1</u>  |           |
| King | <a href="#">Residential Cement Masons</a>                               | Journey Level   | \$22.64 |           | <u>1</u>  |           |
| King | <a href="#">Residential Drywall Applicators</a>                         | Journey Level   | \$40.64 | <u>5D</u> | <u>4C</u> |           |
| King | <a href="#">Residential Drywall Tapers</a>                              | Journey Level   | \$54.07 | <u>5P</u> | <u>1E</u> |           |
| King | <a href="#">Residential Electricians</a>                                | Journey Level   | \$30.44 |           | <u>1</u>  |           |
| King | <a href="#">Residential Glaziers</a>                                    | Journey Level   | \$37.30 | <u>7L</u> | <u>1H</u> |           |
| King | <a href="#">Residential Insulation Applicators</a>                      | Journey Level   | \$26.28 |           | <u>1</u>  |           |
| King | <a href="#">Residential Laborers</a>                                    | Journey Level   | \$23.03 |           | <u>1</u>  |           |

|      |  |                                      |         |           |           |  |
|------|--|--------------------------------------|---------|-----------|-----------|--|
| King | <a href="#">Residential Marble Setters</a>                                 | Journey Level                        | \$24.09 |           | <u>1</u>  |  |
| King | <a href="#">Residential Painters</a>                                       | Journey Level                        | \$24.46 |           | <u>1</u>  |  |
| King | <a href="#">Residential Plumbers &amp; Pipefitters</a>                     | Journey Level                        | \$34.69 |           | <u>1</u>  |  |
| King | <a href="#">Residential Refrigeration &amp; Air Conditioning Mechanics</a> | Journey Level                        | \$73.51 | <u>6Z</u> | <u>1G</u> |  |
| King | <a href="#">Residential Sheet Metal Workers</a>                            | Journey Level (Field or Shop)        | \$43.46 | <u>7F</u> | <u>1R</u> |  |
| King | <a href="#">Residential Soft Floor Layers</a>                              | Journey Level                        | \$42.88 | <u>5A</u> | <u>3D</u> |  |
| King | <a href="#">Residential Sprinkler Fitters (Fire Protection)</a>            | Journey Level                        | \$42.48 | <u>5C</u> | <u>2R</u> |  |
| King | <a href="#">Residential Stone Masons</a>                                   | Journey Level                        | \$52.82 | <u>5A</u> | <u>1M</u> |  |
| King | <a href="#">Residential Terrazzo Workers</a>                               | Journey Level                        | \$47.46 | <u>5A</u> | <u>1M</u> |  |
| King | <a href="#">Residential Terrazzo/Tile Finishers</a>                        | Journey Level                        | \$21.46 |           | <u>1</u>  |  |
| King | <a href="#">Residential Tile Setters</a>                                   | Journey Level                        | \$25.17 |           | <u>1</u>  |  |
| King | <a href="#">Roofers</a>  | Journey Level                        | \$45.71 | <u>5A</u> | <u>3H</u> |  |
| King | <a href="#">Roofers</a>  | Using Irritable Bituminous Materials | \$48.71 | <u>5A</u> | <u>3H</u> |  |
| King | <a href="#">Sheet Metal Workers</a>  | Journey Level (Field or Shop)        | \$72.83 | <u>7F</u> | <u>1E</u> |  |
| King | <a href="#">Shipbuilding &amp; Ship Repair</a>                             | Boilermaker                          | \$39.82 | <u>7M</u> | <u>1H</u> |  |
| King | <a href="#">Shipbuilding &amp; Ship Repair</a>                             | Carpenter                            | \$40.41 | <u>7T</u> | <u>2B</u> |  |
| King | <a href="#">Shipbuilding &amp; Ship Repair</a>                             | Electrician                          | \$40.16 | <u>7T</u> | <u>4B</u> |  |
| King | <a href="#">Shipbuilding &amp; Ship Repair</a>                             | Heat & Frost Insulator               | \$63.18 | <u>5J</u> | <u>1S</u> |  |
| King | <a href="#">Shipbuilding &amp; Ship Repair</a>                             | Laborer                              | \$40.19 | <u>7T</u> | <u>4B</u> |  |
| King | <a href="#">Shipbuilding &amp; Ship Repair</a>                             | Machinist                            | \$40.47 | <u>7T</u> | <u>4B</u> |  |
| King | <a href="#">Shipbuilding &amp; Ship Repair</a>                             | Operator                             | \$40.11 | <u>7T</u> | <u>4B</u> |  |
| King | <a href="#">Shipbuilding &amp; Ship Repair</a>                             | Painter                              | \$40.16 | <u>7T</u> | <u>4B</u> |  |
| King | <a href="#">Shipbuilding &amp; Ship Repair</a>                             | Pipefitter                           | \$40.11 | <u>7T</u> | <u>4B</u> |  |
| King | <a href="#">Shipbuilding &amp; Ship Repair</a>                             | Rigger                               | \$40.19 | <u>7T</u> | <u>4B</u> |  |
| King | <a href="#">Shipbuilding &amp; Ship Repair</a>                             | Sheet Metal                          | \$40.14 | <u>7T</u> | <u>4B</u> |  |
| King | <a href="#">Shipbuilding &amp; Ship Repair</a>                             | Shipfitter                           | \$40.19 | <u>7T</u> | <u>4B</u> |  |
| King | <a href="#">Shipbuilding &amp; Ship Repair</a>                             | Trucker                              | \$40.03 | <u>7T</u> | <u>4B</u> |  |
| King | <a href="#">Shipbuilding &amp; Ship Repair</a>                             | Warehouse                            | \$40.08 | <u>7T</u> | <u>4B</u> |  |
| King | <a href="#">Shipbuilding &amp; Ship Repair</a>                             | Welder/Burner                        | \$40.19 | <u>7T</u> | <u>4B</u> |  |
| King | <a href="#">Sign Makers &amp; Installers (Electrical)</a>                  | Sign Installer                       | \$22.92 |           | <u>1</u>  |  |
| King | <a href="#">Sign Makers &amp; Installers (Electrical)</a>                  | Sign Maker                           | \$21.36 |           | <u>1</u>  |  |
| King | <a href="#">Sign Makers &amp; Installers (Non-Electrical)</a>              | Sign Installer                       | \$27.28 |           | <u>1</u>  |  |
| King | <a href="#">Sign Makers &amp; Installers (Non-Electrical)</a>              | Sign Maker                           | \$33.25 |           | <u>1</u>  |  |
| King | <a href="#">Soft Floor Layers</a>  | Journey Level                        | \$42.88 | <u>5A</u> | <u>3D</u> |  |
| King | <a href="#">Solar Controls For Windows</a>                                 | Journey Level                        | \$12.44 |           | <u>1</u>  |  |
| King | <a href="#">Sprinkler Fitters (Fire Protection)</a>                        | Journey Level                        | \$69.74 | <u>5C</u> | <u>1X</u> |  |
| King | <a href="#">Stage Rigging Mechanics (Non Structural)</a>                   | Journey Level                        | \$13.23 |           | <u>1</u>  |  |

|      |  |  |         |           |           |           |
|------|--|--|---------|-----------|-----------|-----------|
| King | <a href="#">Stone Masons</a>                           | Journey Level                                      | \$52.82 | <u>5A</u> | <u>1M</u> |           |
| King | <a href="#">Street And Parking Lot Sweeper Workers</a> | Journey Level                                      | \$19.09 |           | <u>1</u>  |           |
| King | <a href="#">Surveyors</a>                              | Assistant Construction Site Surveyor               | \$56.00 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Surveyors</a>                              | Chainman   | \$55.47 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Surveyors</a>                              | Construction Site Surveyor                         | \$56.94 | <u>7A</u> | <u>3C</u> | <u>8P</u> |
| King | <a href="#">Telecommunication Technicians</a>          | Journey Level                                      | \$22.76 |           | <u>1</u>  |           |
| King | <a href="#">Telephone Line Construction - Outside</a>  | Cable Splicer                                      | \$37.60 | <u>5A</u> | <u>2B</u> |           |
| King | <a href="#">Telephone Line Construction - Outside</a>  | Hole Digger/Ground Person                          | \$20.79 | <u>5A</u> | <u>2B</u> |           |
| King | <a href="#">Telephone Line Construction - Outside</a>  | Installer (Repairer)                               | \$36.02 | <u>5A</u> | <u>2B</u> |           |
| King | <a href="#">Telephone Line Construction - Outside</a>  | Special Aparatus Installer I                       | \$37.60 | <u>5A</u> | <u>2B</u> |           |
| King | <a href="#">Telephone Line Construction - Outside</a>  | Special Apparatus Installer II                     | \$36.82 | <u>5A</u> | <u>2B</u> |           |
| King | <a href="#">Telephone Line Construction - Outside</a>  | Telephone Equipment Operator (Heavy)               | \$37.60 | <u>5A</u> | <u>2B</u> |           |
| King | <a href="#">Telephone Line Construction - Outside</a>  | Telephone Equipment Operator (Light)               | \$34.94 | <u>5A</u> | <u>2B</u> |           |
| King | <a href="#">Telephone Line Construction - Outside</a>  | Telephone Lineperson                               | \$34.93 | <u>5A</u> | <u>2B</u> |           |
| King | <a href="#">Telephone Line Construction - Outside</a>  | Television Groundperson                            | \$19.73 | <u>5A</u> | <u>2B</u> |           |
| King | <a href="#">Telephone Line Construction - Outside</a>  | Television Lineperson/Installer                    | \$26.31 | <u>5A</u> | <u>2B</u> |           |
| King | <a href="#">Telephone Line Construction - Outside</a>  | Television System Technician                       | \$31.50 | <u>5A</u> | <u>2B</u> |           |
| King | <a href="#">Telephone Line Construction - Outside</a>  | Television Technician                              | \$28.23 | <u>5A</u> | <u>2B</u> |           |
| King | <a href="#">Telephone Line Construction - Outside</a>  | Tree Trimmer                                       | \$34.93 | <u>5A</u> | <u>2B</u> |           |
| King | <a href="#">Terrazzo Workers</a>                       | Journey Level                                      | \$47.46 | <u>5A</u> | <u>1M</u> |           |
| King | <a href="#">Tile Setters</a>                           | Journey Level                                      | \$21.65 |           | <u>1</u>  |           |
| King | <a href="#">Tile, Marble &amp; Terrazzo Finishers</a>  | Finisher   | \$38.29 | <u>5A</u> | <u>1B</u> |           |
| King | <a href="#">Traffic Control Stripers</a>               | Journey Level                                      | \$43.73 | <u>7A</u> | <u>1K</u> |           |
| King | <a href="#">Truck Drivers</a>                          | Asphalt Mix Over 16 Yards (W. WA-Joint Council 28) | \$49.85 | <u>5D</u> | <u>3A</u> | <u>8L</u> |
| King | <a href="#">Truck Drivers</a>                          | Asphalt Mix To 16 Yards (W. WA-Joint Council 28)   | \$49.01 | <u>5D</u> | <u>3A</u> | <u>8L</u> |
| King | <a href="#">Truck Drivers</a>                          | Dump Truck & Trailer                               | \$49.85 | <u>5D</u> | <u>3A</u> | <u>8L</u> |
| King | <a href="#">Truck Drivers</a>                          | Dump Truck (W. WA-Joint Council 28)                | \$49.01 | <u>5D</u> | <u>3A</u> | <u>8L</u> |
| King | <a href="#">Truck Drivers</a>                          | Other Trucks (W. WA-Joint Council 28)              | \$49.85 | <u>5D</u> | <u>3A</u> | <u>8L</u> |
| King | <a href="#">Truck Drivers</a>                          | Transit Mixer                                      | \$43.23 |           | <u>1</u>  |           |

|      |  |                           |         |  |          |  |
|------|--|---------------------------|---------|--|----------|--|
| King | <a href="#">Well Drillers &amp; Irrigation Pump Installers</a> | Irrigation Pump Installer | \$17.71 |  | <u>1</u> |  |
| King | <a href="#">Well Drillers &amp; Irrigation Pump Installers</a> | Oiler                     | \$12.97 |  | <u>1</u> |  |
| King | <a href="#">Well Drillers &amp; Irrigation Pump Installers</a> | Well Driller              | \$18.00 |  | <u>1</u> |  |

General Decision Number: WA160001 01/08/2016 WA1

Superseded General Decision Number: WA20150001

State: Washington

Construction Type: Highway

Counties: Washington Statewide.

HIGHWAY (Excludes D.O.E. Hanford Site in Benton and Franklin Counties)

Note: Under Executive Order (EO) 13658, an hourly minimum wage of **\$10.15 for calendar year 2016 applies to all contracts** subject to the Davis-Bacon Act for which the solicitation was issued on or after January 1, 2015. If this contract is covered by the EO, the contractor must pay all workers in any classification listed on this wage determination at least \$10.15 (or the applicable wage rate listed on this wage determination, if it is higher) for all hours spent performing on the contract **in calendar year 2016**. The EO minimum wage rate will be adjusted annually. Additional information on contractor requirements and worker protections under the EO is available at [www.dol.gov/whd/govcontracts](http://www.dol.gov/whd/govcontracts).

|                     |                   |
|---------------------|-------------------|
| Modification Number | Publication Date  |
| <b>0</b>            | <b>01/08/2016</b> |

CARP0001-008 06/01/2015

|              | Rates    | Fringes |
|--------------|----------|---------|
| CARPENTER    |          |         |
| GROUP 1..... | \$ 27.61 | 14.00   |
| GROUP 2..... | \$ 41.86 | 14.49   |
| GROUP 3..... | \$ 32.97 | 14.00   |
| GROUP 4..... | \$ 31.94 | 14.00   |
| GROUP 5..... | \$ 73.44 | 14.00   |
| GROUP 6..... | \$ 35.02 | 14.00   |
| GROUP 7..... | \$ 36.72 | 14.00   |
| GROUP 8..... | \$ 33.27 | 14.00   |
| GROUP 9..... | \$ 35.02 | 14.00   |

## CARPENTER &amp; DIVER CLASSIFICATIONS:

GROUP 1: Carpenter  
 GROUP 2: Millwright, machine erector  
 GROUP 3: Piledriver - includes driving, pulling, cutting, placing collars, setting, welding, or creosote treated material, on all piling  
 GROUP 4: Bridge carpenters  
 GROUP 5: Diver Wet  
 GROUP 6: Diver Tender, Manifold Operator, ROV Operator  
 GROUP 7: Diver Standby, Bell/Vehicle or Submersible operator Not Under Pressure  
 GROUP 8: Assistant Tender, ROV Tender/Technician  
 GROUP 9: Manifold Operator-Mixed Gas

## ZONE PAY:

|        |                |                 |
|--------|----------------|-----------------|
| ZONE 1 | 0-40 MILES     | FREE            |
| ZONE 2 | 41-65 MILES    | \$2.25/PER HOUR |
| ZONE 3 | 66-100 MILES   | \$3.25/PER HOUR |
| ZONE 4 | OVER 100 MILES | \$4.75/PER HOUR |

## DISPATCH POINTS:

CARPENTERS/MILLWRIGHTS: PASCO (515 N Neel Street) or Main Post Office of established residence of employee (Whichever is closest to the worksite).

CARPENTERS/PILEDRIVER: SPOKANE (127 E. AUGUSTA AVE.) or Main Post Office of established residence of employee (Whichever is closest to the worksite).

CARPENTERS: WENATCHEE (27 N. CHELAN) or Main Post Office of established residence of employee (Whichever is closest to the worksite).

CARPENTERS: COEUR D' ALENE (1839 N. GOVERNMENT WAY) or Main Post Office of established residence of employee (Whichever is closest to the worksite).

CARPENTERS: MOSCOW (302 N. JACKSON) or Main Post Office of established residence of employee (Whichever is closest to the worksite).

DEPTH PAY FOR DIVERS BELOW WATER SURFACE:

50-100 feet \$2.00 per foot  
101-150 feet \$3.00 per foot  
151-220 feet \$4.00 per foot  
221 feet and deeper \$5.00 per foot

PREMIUM PAY FOR DIVING IN ENCLOSURES WITH NO VERTICAL ASCENT:

0-25 feet Free  
26-300 feet \$1.00 per Foot

SATURATION DIVING:

The standby rate applies until saturation starts. The saturation diving rate applies when divers are under pressure continuously until work task and decompression are complete. the diver rate shall be paid for all saturation hours.

WORK IN COMBINATION OF CLASSIFICATIONS:

Employees working in any combination of classifications within the diving crew (except dive supervisor) in a shift are paid in the classification with the highest rate for that shift.

HAZMAT PROJECTS:

Anyone working on a HAZMAT job (task), where HAZMAT certification is required, shall be compensated at a premium, in addition to the classification working in as follows:

LEVEL D + \$.25 per hour - This is the lowest level of protection. No respirator is used and skin protection is minimal.

LEVEL C + \$.50 per hour - This level uses an air purifying respirator or additional protective clothing.

LEVEL B + \$.75 per hour - Uses same respirator protection as Level A. Supplied air line is provided in conjunction with a chemical "splash suit".

LEVEL A +\$1.00 per hour - This level utilizes a fully encapsulated suit with a self-contained breathing apparatus or a supplied air line.

CARP0003-006 10/01/2011

SOUTHWEST WASHINGTON: CLARK, COWLITZ, KLICKITAT, LEWIS(Piledriver only), PACIFIC (South of a straight line made by extending the north boundary line of Wahkiakum County west to Willapa Bay to the Pacific Ocean), SKAMANIA AND WAHAKIYAKUM COUNTIES and INCLUDES THE ENTIRE PENINSULA WEST OF WILLAPA BAY

SEE ZONE DESCRIPTION FOR CITIES BASE POINTS

ZONE 1:

|                     | Rates    | Fringes |
|---------------------|----------|---------|
| Carpenters:         |          |         |
| CARPENTERS.....     | \$ 32.04 | 14.18   |
| DIVERS TENDERS..... | \$ 36.34 | 14.18   |
| DIVERS.....         | \$ 77.08 | 14.18   |
| DRYWALL.....        | \$ 27.56 | 14.18   |
| MILLWRIGHTS.....    | \$ 32.19 | 14.18   |
| PILEDRIVERS.....    | \$ 33.04 | 14.18   |

DEPTH PAY:

50 TO 100 FEET \$1.00 PER FOOT OVER 50 FEET  
 101 TO 150 FEET \$1.50 PER FOOT OVER 101 FEET  
 151 TO 200 FEET \$2.00 PER FOOT OVER 151 FEET

Zone Differential (Add up Zone 1 rates):

Zone 2 - \$0.85  
 Zone 3 - 1.25  
 Zone 4 - 1.70  
 Zone 5 - 2.00  
 Zone 6 - 3.00

BASEPOINTS: ASTORIA, LONGVIEW, PORTLAND, THE DALLES, AND VANCOUVER, (NOTE: All dispatches for Washington State Counties: Cowlitz, Wahkiakum and Pacific shall be from Longview Local #1707 and mileage shall be computed from that point.)

ZONE 1: Projects located within 30 miles of the respective city hall of the above mentioned cities  
 ZONE 2: Projects located more than 30 miles and less than 40 miles of the respective city of the above mentioned cities  
 ZONE 3: Projects located more than 40 miles and less than 50 miles of the respective city of the above mentioned cities  
 ZONE 4: Projects located more than 50 miles and less than 60 miles of the respective city of the above mentioned cities.  
 ZONE 5: Projects located more than 60 miles and less than 70 miles of the respective city of the above mentioned cities  
 ZONE 6: Projects located more than 70 miles of the respected city of the above mentioned cities

CARP0770-003 06/01/2015

|                            | Rates    | Fringes |
|----------------------------|----------|---------|
| CARPENTER                  |          |         |
| CENTRAL WASHINGTON:        |          |         |
| CHELAN, DOUGLAS (WEST OF   |          |         |
| THE 120TH MERIDIAN),       |          |         |
| KITTTITAS, OKANOGAN (WEST  |          |         |
| OF THE 120TH MERIDIAN) AND |          |         |
| YAKIMA COUNTIES            |          |         |
| CARPENTERS ON CREOSOTE     |          |         |
| MATERIAL.....              | \$ 40.46 | 13.66   |
| CARPENTERS.....            | \$ 40.36 | 13.66   |
| DIVERS TENDER.....         | \$ 35.02 | 14.00   |
| DIVERS.....                | \$ 73.44 | 14.00   |
| MILLWRIGHT AND MACHINE     |          |         |
| ERECTORS.....              | \$ 41.86 | 13.66   |
| PILEDRIIVER, DRIVING,      |          |         |
| PULLING, CUTTING, PLACING  |          |         |
| COLLARS, SETTING, WELDING  |          |         |
| OR CRESOTE TREATED         |          |         |
| MATERIAL, ALL PILING.....  | \$ 40.61 | 13.66   |

(HOURLY ZONE PAY: WESTERN AND CENTRAL WASHINGTON - ALL CLASSIFICATIONS EXCEPT MILLWRIGHTS AND PILEDRIIVERS

Hourly Zone Pay shall be paid on jobs located outside of the free zone computed from the city center of the following listed cities:

|                  |              |              |
|------------------|--------------|--------------|
| Seattle          | Olympia      | Bellingham   |
| Auburn           | Bremerton    | Anacortes    |
| Renton           | Shelton      | Yakima       |
| Aberdeen-Hoquiam | Tacoma       | Wenatchee    |
| Ellensburg       | Everett      | Port Angeles |
| Centralia        | Mount Vernon | Sunnyside    |
| Chelan           | Pt. Townsend |              |

Zone Pay:

|                      |             |
|----------------------|-------------|
| 0 -25 radius miles   | Free        |
| 26-35 radius miles   | \$1.00/hour |
| 36-45 radius miles   | \$1.15/hour |
| 46-55 radius miles   | \$1.35/hour |
| Over 55 radius miles | \$1.55/hour |

(HOURLY ZONE PAY: WESTERN AND CENTRAL WASHINGTON - MILLWRIGHT AND PILEDRIIVER ONLY)

Hourly Zone Pay shall be computed from Seattle Union Hall, Tacoma City center, and Everett City center

Zone Pay:

|                      |             |
|----------------------|-------------|
| 0 -25 radius miles   | Free        |
| 26-45 radius miles   | \$ .70/hour |
| Over 45 radius miles | \$1.50/hour |

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 CARP0770-006 06/01/2015

|  | Rates    | Fringes |
|--|----------|---------|
| CARPENTER  |          |         |
| WESTERN WASHINGTON:  |          |         |
| CLALLAM, GRAYS HARBOR, ISLAND, JEFFERSON, KING, KITSAP, LEWIS (excludes piledrivers only), MASON, PACIFIC (North of a straight line made by extending the north boundary line of Wahkiakum County west to the Pacific Ocean), PIERCE, SAN JUAN, SKAGIT, SNOHOMISH, THURSTON AND WHATCOM COUNTIES |          |         |
| BRIDGE CARPENTERS.....   | \$ 40.36 | 13.66   |
| CARPENTERS ON CREOSOTE   |          |         |
| MATERIAL.....  | \$ 40.46 | 13.66   |
| CARPENTERS.....  | \$ 40.36 | 13.66   |
| DIVERS TENDER.....   | \$ 44.67 | 13.66   |
| DIVERS.....  | \$ 93.56 | 13.66   |
| MILLWRIGHT AND MACHINE   |          |         |
| ERECTORS.....  | \$ 41.86 | 13.66   |
| PILEDRIVER, DRIVING,<br>PULLING, CUTTING, PLACING<br>COLLARS, SETTING, WELDING<br>OR CRESOTE TREATED   |          |         |
| MATERIAL, ALL PILING.....  | \$ 40.61 | 13.66   |

(HOURLY ZONE PAY: WESTERN AND CENTRAL WASHINGTON - ALL CLASSIFICATIONS EXCEPT MILLWRIGHTS AND PILEDRIVERS)

Hourly Zone Pay shall be paid on jobs located outside of the free zone computed from the city center of the following listed cities:

|                  |              |              |
|------------------|--------------|--------------|
| Seattle          | Olympia      | Bellingham   |
| Auburn           | Bremerton    | Anacortes    |
| Renton           | Shelton      | Yakima       |
| Aberdeen-Hoquiam | Tacoma       | Wenatchee    |
| Ellensburg       | Everett      | Port Angeles |
| Centralia        | Mount Vernon | Sunnyside    |
| Chelan           | Pt. Townsend |              |

Zone Pay:

|                      |             |
|----------------------|-------------|
| 0 -25 radius miles   | Free        |
| 26-35 radius miles   | \$1.00/hour |
| 36-45 radius miles   | \$1.15/hour |
| 46-55 radius miles   | \$1.35/hour |
| Over 55 radius miles | \$1.55/hour |

(HOURLY ZONE PAY: WESTERN AND CENTRAL WASHINGTON - MILLWRIGHT AND PILEDRIVER ONLY)

Hourly Zone Pay shall be computed from Seattle Union Hall, Tacoma City center, and Everett City center

Zone Pay:

|                      |             |
|----------------------|-------------|
| 0 -25 radius miles   | Free        |
| 26-45 radius miles   | \$ .70/hour |
| Over 45 radius miles | \$1.50/hour |

WA160001 Modification 0  
 Federal Wage Determinations for Highway Construction

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 ELEC0046-001 02/04/2013

CALLAM, JEFFERSON, KING AND KITSAP COUNTIES

|                    | Rates    | Fringes  |
|--------------------|----------|----------|
| CABLE SPLICER..... | \$ 46.87 | 3%+15.96 |
| ELECTRICIAN.....   | \$ 42.61 | 3%+15.96 |

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\* ELEC0048-003 01/01/2015

CLARK, KLICKITAT AND SKAMANIA COUNTIES

|                    | Rates    | Fringes |
|--------------------|----------|---------|
| CABLE SPLICER..... | \$ 44.22 | 21.50   |
| ELECTRICIAN.....   | \$ 40.20 | 21.50   |

HOURLY ZONE PAY:

Hourly Zone Pay shall be paid on jobs located outside of the free zone computed from the city center of the following listed cities:

Portland, The Dalles, Hood River, Tillamook, Seaside and Astoria

Zone Pay:

Zone 1: 31-50 miles \$1.50/hour  
 Zone 2: 51-70 miles \$3.50/hour  
 Zone 3: 71-90 miles \$5.50/hour  
 Zone 4: Beyond 90 miles \$9.00/hour

\*These are not miles driven. Zones are based on Delorme Street Atlas USA 2006 plus.

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 ELEC0048-029 01/01/2015

COWLITZ AND WAHKIAKUM COUNTY

|                    | Rates    | Fringes |
|--------------------|----------|---------|
| CABLE SPLICER..... | \$ 44.22 | 21.50   |
| ELECTRICIAN.....   | \$ 40.20 | 21.50   |

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ELEC0073-001 07/01/2015

ADAMS, FERRY, LINCOLN, PEND OREILLE, SPOKANE, STEVENS, WHITMAN  
COUNTIES

|                    | Rates    | Fringes |
|--------------------|----------|---------|
| CABLE SPLICER..... | \$ 34.10 | 16.68   |
| ELECTRICIAN.....   | \$ 31.00 | 16.68   |

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ELEC0076-002 09/01/2014

GRAYS HARBOR, LEWIS, MASON, PACIFIC, PIERCE, AND THURSTON  
COUNTIES

|                    | Rates    | Fringes |
|--------------------|----------|---------|
| CABLE SPLICER..... | \$ 37.94 | 23.36   |
| ELECTRICIAN.....   | \$ 34.49 | 23.36   |

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ELEC0112-005 06/01/2015

ASOTIN, BENTON, COLUMBIA, FRANKLIN, GARFIELD, KITTITAS, WALLA  
WALLA, YAKIMA COUNTIES

|                    | Rates    | Fringes |
|--------------------|----------|---------|
| CABLE SPLICER..... | \$ 40.74 | 18.65   |
| ELECTRICIAN.....   | \$ 38.80 | 18.59   |

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ELEC0191-003 06/01/2014

ISLAND, SAN JUAN, SNOHOMISH, SKAGIT AND WHATCOM COUNTIES

|                    | Rates    | Fringes |
|--------------------|----------|---------|
| CABLE SPLICER..... | \$ 44.23 | 17.73   |
| ELECTRICIAN.....   | \$ 40.21 | 17.73   |

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ELEC0191-004 06/01/2014

CHELAN, DOUGLAS, GRANT AND OKANOGAN COUNTIES

|                    | Rates    | Fringes |
|--------------------|----------|---------|
| CABLE SPLICER..... | \$ 40.82 | 17.63   |
| ELECTRICIAN.....   | \$ 37.11 | 17.63   |

ENGI0302-003 06/01/2014

CHELAN (WEST OF THE 120TH MERIDIAN), CLALLAM, DOUGLAS (WEST OF THE 120TH MERIDIAN), GRAYS HARBOR, ISLAND, JEFFERSON, KING, KITSAP, KITTITAS, MASON, OKANOGAN (WEST OF THE 120TH MERIDIAN), SAN JUNA, SKAGIT, SNOHOMISH, WHATCOM AND YAKIMA (WEST OF THE 120TH MERIDIAN) COUNTIES

PROJECTS: CATEGORY A PROJECTS (EXCLUDES CATEGORY B PROJECTS, AS SHOWN BELOW)

Zone 1 (0-25 radius miles):

|                            | Rates    | Fringes |
|----------------------------|----------|---------|
| Power equipment operators: |          |         |
| Group 1A.....              | \$ 38.39 | 17.39   |
| Group 1AA.....             | \$ 38.96 | 17.39   |
| Group 1AAA.....            | \$ 39.52 | 17.39   |
| Group 1.....               | \$ 37.84 | 17.39   |
| Group 2.....               | \$ 37.35 | 17.39   |
| Group 3.....               | \$ 36.93 | 17.39   |
| Group 4.....               | \$ 34.57 | 17.39   |

Zone Differential (Add to Zone 1 rates):

Zone 2 (26-45 radius miles) - \$1.00

Zone 3 (Over 45 radius miles) - \$1.30

BASEPOINTS: Aberdeen, Bellingham, Bremerton, Everett, Kent, Mount Vernon, Port Angeles, Port Townsend, Seattle, Shelton, Wenatchee, Yakima

#### POWER EQUIPMENT OPERATORS CLASSIFICATIONS

GROUP 1AAA - Cranes-over 300 tons, or 300 ft of boom (including jib with attachments)

GROUP 1AA - Cranes 200 to 300 tons, or 250 ft of boom (including jib with attachments); Tower crane over 175 ft in height, base to boom

GROUP 1A - Cranes, 100 tons thru 199 tons, or 150 ft of boom (including jib with attachments); Crane-overhead, bridge type, 100 tons and over; Tower crane up to 175 ft in height base to boom; Loaders-overhead, 8 yards and over; Shovels, excavator, backhoes-6 yards and over with attachments

GROUP 1 - Cableway; Cranes 45 tons thru 99 tons, under 150 ft of boom (including jib with attachments); Crane-overhead, bridge type, 45 tons thru 99 tons; Derricks on building work; Excavator, shovel, backhoes over 3 yards and under 6 yards; Hard tail end dump articulating off-road equipment 45 yards and over; Loader- overhead 6 yards to, but not including 8 yards; Mucking machine, mole, tunnel, drill and/or shield; Quad 9, HD 41, D-10; Remote control operator on rubber tired earth moving equipment; Rollagon; Scrapers-self propelled 45 yards and over; Slipform pavers; Transporters, all truck or track type

GROUP 2 - Barrier machine (zipper); Batch Plant Operator- Concrete; Bump Cutter; Cranes, 20 tons thru 44 tons with attachments; Crane-overhead, bridge type-20 tons through 44 tons; Chipper; Concrete Pump-truck mount with boom attachment; Crusher; Deck Engineer/Deck Winches (power); Drilling machine; Excavator, shovel, backhoe-3yards and under; Finishing Machine, Bidwell, Gamaco and similar equipment; Guardrail punch; Horizontal/directional drill operator; Loaders-overhead under 6 yards; Loaders-plant feed; Locomotives-all; Mechanics-all; Mixers-asphalt plant; Motor patrol graders-finishing; Piledriver (other than crane mount); Roto-mill, roto-grinder; Screedman, spreader, topside operator-Blaw Knox, Cedar Rapids, Jaeger, Caterpillar, Barbar Green; Scraper-self propelled, hard tail end dump, articulating off-road equipment-under 45 yards; Subgrade trimmer; Tractors, backhoes-over 75 hp; Transfer material service machine-shuttle buggy, blaw knox-roadtec; Truck crane oiler/driver-100 tons and over; Truck Mount portable conveyor; Yo Yo Pay dozer

GROUP 3 - Conveyors; Cranes-thru 19 tons with attachments; A-frame crane over 10 tons; Drill oilers-auger type, truck or crane mount; Dozers-D-9 and under; Forklift-3000 lbs. and over with attachments; Horizontal/directional drill locator; Outside hoists-(elevators and manlifts), air tuggers, strato tower bucket elevators; Hydralifts/boom trucks over 10 tons; Loader-elevating type, belt; Motor patrol grader-nonfinishing; Plant oiler- asphalt, crusher; Pumps-concrete; Roller, plant mix or multi-lift materials; Saws-concrete; Scrapers-concrete and carry-all; Service engineer-equipment; Trenching machines; Truck Crane Oiler/Driver under 100 tons; Tractors, backhoe 75 hp and under

GROUP 4 - Assistant Engineer; Bobcat; Brooms; Compressor; Concrete finish machine-laser screed; Cranes-A frame-10 tons and under; Elevator and Manlift-permanent or shaft type; Gradechecker, Stakehop; Forklifts under 3000 lbs. with attachments; Hydralifts/boom trucks, 10 tons and under; Oil distributors, blower distribution and mulch seeding operator; Pavement breaker; Posthole digger, mechanical; Power plant; Pumps, water; Rigger and Bellman; Roller-other than plant mix; Wheel Tractors, farmall type; Shotcrete/gunite equipment operator

Category B Projects: 95% of the basic hourly reate for each group plus full fringe benefits applicable to category A projects shall apply to the following projects. A Reduced rates may be paid on the following:

1. Projects involving work on structures such as buildings and bridges whose total value is less than \$1.5 million excluding mechanical, electrical, and utility portions of the contract.
2. Projects of less than \$1 million where no building is involved. Surfacing and paving included, but utilities excluded.
3. Marine projects (docks, wharfs, etc.) less than \$150,000.

#### HANDLING OF HAZARDOUS WASTE MATERIALS:

Personnel in all craft classifications subject to working inside a federally designated hazardous perimeter shall be elgible for compensation in accordance with the following group schedule relative to the level of hazardous waste as outlined in the specific hazardous waste project site safety plan.

H-1 Base wage rate when on a hazardous waste site when not outfitted with protective clothing

H-2 Class "C" Suit - Base wage rate plus \$ .25 per hour.

H-3 Class "B" Suit - Base wage rate plus \$ .50 per hour.

H-4 Class "A" Suit - Base wage rate plus \$ .75 per hour.

#### Zone Differential (Add to Zone 1 rates):

Zone 2 (26-45 radius miles) - \$ .70

Zone 3 (Over 45 radius miles) - \$1.00

BASEPOINTS: Aberdeen, Bellingham, Bremerton, Everett, Kent, Mount Vernon, Port Angeles, Port Townsend, Seattle, Shelton, Wenatchee, Yakima

## POWER EQUIPMENT OPERATORS CLASSIFICATIONS

GROUP 1AAA - Cranes-over 300 tons, or 300 ft of boom  
(including jib with attachments)

GROUP 1AA - Cranes 200 to 300 tons, or 250 ft of boom  
(including jib with attachments); Tower crane over 175 ft in  
height, base to boom

GROUP 1A - Cranes, 100 tons thru 199 tons, or 150 ft of boom  
(including jib with attachments); Crane-overhead, bridge  
type, 100 tons and over; Tower crane up to 175 ft in height  
base to boom; Loaders-overhead, 8 yards and over; Shovels,  
excavator, backhoes-6 yards and over with attachments

GROUP 1 - Cableway; Cranes 45 tons thru 99 tons, under 150 ft  
of boom (including jib with attachments); Crane-overhead,  
bridge type, 45 tons thru 99 tons; Derricks on building work;  
Excavator, shovel, backhoes over 3 yards and under 6 yards;  
Hard tail end dump articulating off-road equipment 45 yards  
and over; Loader- overhead 6 yards to, but not including 8  
yards; Mucking machine, mole, tunnel, drill and/or shield;  
Quad 9, HD 41, D-10; Remote control operator on rubber tired  
earth moving equipment; Rollagon; Scrapers-self propelled 45  
yards and over; Slipform pavers; Transporters, all truck or  
track type

GROUP 2 - Barrier machine (zipper); Batch Plant Operaor-  
Concrete; Bump Cutter; Cranes, 20 tons thru 44 tons with  
attachments; Crane-overhead, bridge type-20 tons through 44  
tons; Chipper; Concrete Pump-truck mount with boom  
attachment; Crusher; Deck Engineer/Deck Winches (power);  
Drilling machine; Excavator, shovel, backhoe-3 yards and  
under; Finishing Machine, Bidwell, Gamaco and similar  
equipment; Guardrail punch; Horizontal/directional drill  
operator; Loaders-overhead under 6 yards; Loaders-plant feed;  
Locomotives-all; Mechanics-all; Mixers-asphalt plant; Motor  
patrol graders-finishing; Piledriver (other than crane  
mount); Roto-mill, roto-grinder; Screedman, spreader, topside  
operator-Blaw Knox, Cedar Rapids, Jaeger, Caterpillar, Barbar  
Green; Scraper-self propelled, hard tail end dump,  
articulating off-road equipment-under 45 yards; Subgrade  
trimmer; Tractors, backhoes-over 75 hp; Transfer material  
service machine-shuttle buggy, blaw knox-roadtec; Truck crane  
oiler/driver-100 tons and over; Truck Mount portable  
conveyor; Yo Yo Pay dozer

GROUP 3 - Conveyors; Cranes-thru 19 tons with attachments; A-frame crane over 10 tons; Drill oilers-auger type, truck or crane mount; Dozers-D-9 and under; Forklift-3000 lbs. and over with attachments; Horizontal/directional drill locator; Outside hoists-(elevators and manlifts), air tuggers, strato tower bucket elevators; Hydralifts/boom trucks over 10 tons; Loader-elevating type, belt; Motor patrol grader-nonfinishing; Plant oiler- asphalt, crusher; Pumps-concrete; Roller, plant mix or multi-lift materials; Saws-concrete; Scrpers-concrete and carry-all; Service engineer-equipment; Trenching machines; Truck Crane Oiler/Driver under 100 tons; Tractors, backhoe 75 hp and under

GROUP 4 - Assistant Engineer; Bobcat; Brooms; Compressor; Concrete finish mahine-laser screed; Cranes-A frame-10 tons and under; Elevator and Manlift-permanent or shaft type; Gradechecker, Stakehop; Forklifts under 3000 lbs. with attachments; Hydralifts/boom trucks, 10 tons and under; Oil distributors, blower distribution and mulch seeding operator; Pavement breaker; Posthole digger, mechanical; Power plant; Pumps, water; Rigger and Bellman; Roller-other than plant mix; Wheel Tractors, farmall type; Shotcrete/gunite equipment operator

CATEGORY B PROJECTS: 95% OF THE BASIC HOURLY RATE FOR EACH GROUP PLUS FULL FRINGE BENEFITS APPLICABLE TO CATEGORY A PROJECTS SHALL APPLY TO THE FOLLOWING PROJECTS. REDUCED RATES MAY BE PAID ON THE FOLLOWING:

1. Projects involving work on structures such as buildings and bridges whose total value is less than \$1.5 million excluding mechanical, electrical, and utility portions of the contract.
2. Projects of less than \$1 million where no building is involved. Surfacing and paving including, but utilities excluded.
3. Marine projects (docks, wharfs, ect.) less than \$150,000.

HANDLING OF HAZARDOUS WASTE MATERIALS: Personnel in all craft classifications subject to working inside a federally designed hazardous perimeter shall be elgible for compensation in accordance with the following group schedule relative to the level of hazardous waste as outlined in the specific hazardous waste project site safety plan.

H-1 Base wage rate when on a hazardous waste site when not outfitted with protective clothing.

H-2 Class "C" Suit - Base wage rate plus \$.25 per hour.

H-3 Class "B" Suit - Base wage rate plus \$.50 per hour.

H-4 Class "A" Suit - Base wage rate plus \$.75 per hour.

ENGI0370-002 06/01/2015

ADAMS, ASOTIN, BENTON, CHELAN (EAST OF THE 120TH MERIDIAN),  
COLUMBIA, DOUGLAS (EAST OF THE 120TH MERIDIAN), FERRY,  
FRANKLIN, GARFIELD, GRANT, LINCOLN, OKANOGAN (EAST OF THE 120TH  
MERIDIAN), PEND OREILLE, SPOKANE, STEVENS, WALLA WALLA, WHITMAN  
AND YAKIMA (EAST OF THE 120TH MERIDIAN) COUNTIES

ZONE 1:

|                          | Rates    | Fringes |
|--------------------------|----------|---------|
| POWER EQUIPMENT OPERATOR |          |         |
| GROUP 1.....             | \$ 26.16 | 13.55   |
| GROUP 2.....             | \$ 26.48 | 13.55   |
| GROUP 3.....             | \$ 27.09 | 13.55   |
| GROUP 4.....             | \$ 27.25 | 13.55   |
| GROUP 5.....             | \$ 27.41 | 13.55   |
| GROUP 6.....             | \$ 27.69 | 13.55   |
| GROUP 7.....             | \$ 27.96 | 13.55   |
| GROUP 8.....             | \$ 29.06 | 13.55   |

ZONE DIFFERENTIAL (Add to Zone 1 rate): Zone 2 - \$2.00

Zone 1: Within 45 mile radius of Spokane, Pasco, Washington;  
Lewiston, Idaho

Zone 2: Outside 45 mile radius of Spokane, Pasco,  
Washington; Lewiston, Idaho

POWER EQUIPMENT OPERATORS CLASSIFICATIONS

GROUP 1: Bit Grinders; Bolt Threading Machine; Compressors  
(under 2000 CFM, gas, diesel, or electric power); Deck  
Hand; Fireman & Heater Tender; Hydro-seeder, Mulcher,  
Nozzleman; Oiler Driver, & Cable Tender, Mucking Machine;  
Pumpman; Rollers, all types on subgrade, including seal and  
chip coatings (farm type, Case, John Deere & similar, or  
Compacting Vibrator), except when pulled by Dozer with  
operable blade; Welding Machine; Crane Oiler-Driver (CLD  
required) & Cable Tender, Mucking Machine

GROUP 2: A-frame Truck (single drum); Assistant Refrigeration Plant (under 1000 ton); Assistant Plant Operator, Fireman or Pugmixer (asphalt); Bagley or Stationary Scraper; Belt Finishing Machine; Blower Operator (cement); Cement Hog; Compressor (2000 CFM or over, 2 or more, gas diesel or electric power); Concrete Saw (multiple cut); Distributor Leverman; Ditch Witch or similar; Elevator Hoisting Materials; Dope Pots (power agitated); Fork Lift or Lumber Stacker, hydra-lift & similar; Gin Trucks (pipeline); Hoist, single drum; Loaders (bucket elevators and conveyors); Longitudinal Float; Mixer (portable-concrete); Pavement Breaker, Hydra-Hammer & similar; Power Broom; Railroad Ballast Regulation Operator (self-propelled); Railroad Power Tamper Operator (self-propelled); Railroad Tamper Jack Operator (self-propelled); Spray Curing Machine (concrete); Spreader Box (self-propelled); Straddle Buggy (Ross & similar on construction job only); Tractor (Farm type R/T with attachment, except Backhoe); Tugger Operator

GROUP 3: A-frame Truck (2 or more drums); Assistant Refrigeration Plant & Chiller Operator (over 1000 ton); Backfillers (Cleveland & similar); Batch Plant & Wet Mix Operator, single unit (concrete); Belt-Crete Conveyors with power pack or similar; Belt Loader (Kocal or similar); Bending Machine; Bob Cat (Skid Steer); Boring Machine (earth); Boring Machine (rock under 8 inch bit) (Quarry Master, Joy or similar); Bump Cutter (Wayne, Saginaw or similar); Canal Lining Machine (concrete); Chipper (without crane); Cleaning & Doping Machine (pipeline); Deck Engineer; Elevating Belt-type Loader (Euclid, Barber Green & similar); Elevating Grader-type Loader (Dumor, Adams or similar); Generator Plant Engineers (diesel or electric); Gunnite Combination Mixer & Compressor; Locomotive Engineer; Mixermobile; Mucking Machine; Posthole Auger or Punch; Pump (grout or jet); Soil Stabilizer (P & H or similar); Spreader Machine; Dozer/Tractor (up to D-6 or equivalent) and Traxcavator; Traverse Finish Machine; Turnhead Operator

GROUP 4: Concrete Pumps (squeeze-crete, flow-crete, pump-crete, Whitman & similar); Curb Extruder (asphalt or concrete); Drills (churn, core, calyx or diamond); Equipment Serviceman; Greaser & Oiler; Hoist (2 or more drums or Tower Hoist); Loaders (overhead & front-end, under 4 yds. R/T); Refrigeration Plant Engineer (under 1000 ton); Rubber-tired Skidders (R/T with or without attachments); Surface Heater & Plant Machine; Trenching Machines (under 7 ft. depth capacity); Turnhead (with re-screening); Vacuum Drill (reverse circulation drill under 8 inch bit)

GROUP 5: Backhoe (under 45,000 gw); Backhoe & Hoe Ram (under 3/4 yd.); Carrydeck & Boom Truck (under 25 tons); Cranes (25 tons & under), all attachments including clamshell, dragline; Derricks & Stifflegs (under 65 tons); Drilling Equipment(8 inch bit & over) (Robbins, reverse circulation & similar); Hoe Ram; Piledriving Engineers; Paving (dual drum); Railroad Track Liner Operatoer (self-propelled); Refrigeration Plant Engineer (1000 tons & over); Signalman (Whirleys, Highline Hammerheads or similar); Grade Checker

GROUP 6: Asphalt Plant Operator; Automatic Subgrader (Ditches & Trimmers)(Autograde, ABC, R.A. Hansen & similar on grade wire); Backhoe (45,000 gw and over to 110,000 gw); Backhoes & Hoe Ram (3/4 yd. to 3 yd.); Batch Plant (over 4 units); Batch & Wet Mix Operator (multiple units, 2 & incl. 4); Blade Operator (motor patrol & attachments); Cable Controller (dispatcher); Compactor (self-propelled with blade); Concrete Pump Boom Truck; Concrete Slip Form Paver; Cranes (over 25 tons, to and including 45 tons), all attachments including clamshell, dragline; Crusher, Grizzle & Screening Plant Operator; Dozer, 834 R/T & similar; Drill Doctor; Loader Operator (front-end & overhead, 4 yds. incl. 8 yds.); Multiple Dozer Units with single blade; Paving Machine (asphalt and concrete); Quad-Track or similar equipment; Roller (finishing asphalt pavement); Roto Mill (pavement grinder); Scrapers, all, rubber-tired; Screed Operator; Shovel(under 3 yds.); Trenching Machines (7 ft. depth & over); Tug Boat Operator Vactor guzzler, super sucker; Lime Batch Tank Operator (REcycle Train); Lime Brain Operator (Recycle Train); Mobile Crusher Operator (Recycle Train)

GROUP 7: Backhoe (over 110,000 gw); Backhoes & Hoe Ram (3 yds & over); Blade (finish & bluetop) Automatic, CMI, ABC, Finish Athey & Huber & similar when used as automatic; Cableway Operators; Concrete Cleaning/Decontamination machine operator; Cranes (over 45 tons to but not including 85 tons), all attachments including clamshell and dragline; Derricks & Stiffleys (65 tons & over); Elevating Belt (Holland type); Heavy equipment robotics operator; Loader (360 degrees revolving Koehring Scooper or similar); Loaders (overhead & front-end, over 8 yds. to 10 yds.); Rubber-tired Scrapers (multiple engine with three or more scrapers); Shovels (3 yds. & over); Whirleys & Hammerheads, ALL; H.D. Mechanic; H.D. Welder; Hydraulic Platform Trailers (Goldhofer, Shaurerly and Similar); Ultra High Pressure Waterjet Cutting Tool System Operator (30,000 psi); Vacuum Blasting Machine Operator

GROUP 8: Cranes (85 tons and over, and all climbing, overhead, rail and tower), all attachments including clamshell, dragline; Loaders (overhead and front-end, 10 yards and over); Helicopter Pilot

BOOM PAY: (All Cranes, Including Tower)

180 ft to 250 ft \$ .50 over scale

Over 250 ft \$ .80 over scale

NOTE:

In computing the length of the boom on Tower Cranes, they shall be measured from the base of the Tower to the point of the boom.

HAZMAT:

Anyone working on HAZMAT jobs, working with supplied air shall receive \$1.00 an hour above classification.

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ENGI0612-012 06/01/2014

LEWIS, PIERCE, PACIFIC (portion lying north of a parallel line extending west from the northern boundary of Wahkaikum County to the sea) AND THURSTON COUNTIES

ON PROJECTS DESCRIBED IN FOOTNOTE A BELOW, THE RATE FOR EACH GROUP SHALL BE 90% OF THE BASE RATE PLUS FULL FRINGE BENEFITS. ON ALL OTHER WORK, THE FOLLOWING RATES APPLY.

Zone 1 (0-25 radius miles):

|                          | Rates    | Fringes |
|--------------------------|----------|---------|
| POWER EQUIPMENT OPERATOR |          |         |
| GROUP 1A.....            | \$ 38.39 | 17.40   |
| GROUP 1AA.....           | \$ 38.96 | 17.40   |
| GROUP 1AAA.....          | \$ 39.52 | 17.40   |
| GROUP 1.....             | \$ 37.84 | 17.40   |
| GROUP 2.....             | \$ 37.35 | 17.40   |
| GROUP 3.....             | \$ 36.93 | 17.40   |
| GROUP 4.....             | \$ 34.57 | 17.40   |

Zone Differential (Add to Zone 1 rates):

Zone 2 (26-45 radius miles) = \$1.00

Zone 3 (Over 45 radius miles) - \$1.30

BASEPOINTS: CENTRALIA, OLYMPIA, TACOMA

## POWER EQUIPMENT OPERATORS CLASSIFICATIONS

GROUP 1 AAA - Cranes-over 300 tons or 300 ft of boom  
(including jib with attachments)

GROUP 1AA - Cranes- 200 tonsto 300 tons, or 250 ft of boom  
(including jib with attachments; Tower crane over 175 ft in  
height, bas to boom

GROUP 1A - Cranes, 100 tons thru 199 tons, or 150 ft of boom  
(including jib with attachments); Crane-overhead, bridge  
type, 100 tons and over; Tower crane up to 175 ft in height  
base to boom; Loaders-overhead, 8 yards and over; Shovels,  
excavator, backhoes-6 yards and over with attachments

GROUP 1 - Cableway; Cranes 45 tons thru 99 tons under 150 ft  
of boom (including jib with attachments); Crane-overhead,  
bridge type, 45 tons thru 99 tons; Derricks on building  
work; Excavator, shovel, backhoes over 3 yards and under 6  
yards; Hard tail end dump articulating off-road equipment  
45 yards and over; Loader- overhead, 6 yards to, but not  
including, 8 yards; Mucking machine, mole, tunnel, drill  
and/or shield; Quad 9 HD 41, D-10; Remote control operator  
on rubber tired earth moving equipment; Rollagon; Scrapers-  
self-propelled 45 yards and over; Slipform pavers;  
Transporters, all track or truck type

GROUP 2 - Barrier machine (zipper); Batch Plant Operator-  
concrete; Bump Cutter; Cranes, 20 tons thru 44 tons with  
attachments; Crane-Overhead, bridge type, 20 tons through  
44 tons; Chipper; Concrete pump-truck mount with boom  
attachment; Crusher; Deck engineer/deck winches (power);  
Drilling machine; Excavator, shovel, backhoe-3 yards and  
under; Finishing machine, Bidwell, Gamaco and similar  
equipment; Guardrail punch; Loaders, overhead under 6  
yards; Loaders-plant feed; Locomotives-all; Mechanics- all;  
Mixers, asphalt plant; Motor patrol graders, finishing;  
Piledriver (other than crane mount); Roto-mill, roto-  
grinder; Screedman, spreader, topside operator-Blaw Knox,  
Cedar Rapids, Jaeger, Caterpillar, Barbar Green;  
Scraper-self- propelled, hard tail end dump, articulating  
off-road equipment- under 45 yards; Subgrader trimmer;  
Tractors, backhoe over 75 hp; Transfer material service  
machine-shuttle buggy, Blaw Knox- Roadtec; Truck Crane  
oiler/driver-100 tons and over; Truck Mount Portable  
Conveyor; Yo Yo pay

GROUP 3 - Conveyors; Cranes through 19 tons with attachments; Crane-A-frame over 10 tons; Drill oilers-auger type, truck or crane mount; Dozer-D-9 and under; Forklift-3000 lbs. and over with attachments; Horizontal/directional drill locator; Outside Hoists-(elevators and manlifts), air tuggers, strato tower bucket elevators; Hydralifts/boom trucks over 10 tons; Loaders-elevating type, belt; Motor patrol grader-nonfinishing; Plant oiler- asphalt, crusher; Pump-Concrete; Roller, plant mix or multi-lfit materials; Saws-concrete; Scrapers, concrete and carry all; Service engineers-equipment; Trenching machines; Truck crane oiler/driver under 100 tons; Tractors, backhoe under 75 hp

GROUP 4 - Assistant Engineer; Bobcat; Brooms; Compressor; Concrete Finish Machine-laser screed; Cranes A-frame 10 tons and under; Elevator and manlift (permanent and shaft type); Forklifts-under 3000 lbs. with attachments; Gradechecker, stakehop; Hydralifts/boom trucks, 10 tons and under; Oil distributors, blower distribution and mulch seeding operator; Pavement breaker; Posthole digger-mechanical; Power plant; Pumps-water; Rigger and Bellman; Roller-other than plant mix; Wheel Tractors, farmall type; Shotcrete/gunite equipment operator

FOOTNOTE A- Reduced rates may be paid on the following:

1. Projects involving work on structures such as buildings and bridges whose total value is less than \$1.5 million excluding mechanical, electrical, and utility portions of the contract.
2. Projects of less than \$1 million where no building is involved. Surfacing and paving included, but utilities excluded.
3. Marine projects (docks, wharfs, etc.) less than \$150,000.

HANDLING OF HAZARDOUS WASTE MATERIALS: Personnel in all craft classifications subject to working inside a federally designated hazardous perimeter shall be eligible for compensation in accordance with the following group schedule relative to the level of hazardous waste as outlined in the specific hazardous waste project site safety plan.

H-1 Base wage rate when on a hazardous waste site when not outfitted with protective clothing, Class "D" Suit - Base wage rate plus \$ .50 per hour.

H-2 Class "C" Suit - Base wage rate plus \$1.00 per hour.

H-3 Class "B" Suit - Base wage rate plus \$1.50 per hour.

H-4 Class "A" Suit - Base wage rate plus \$2.00 per hour.

ENGI0701-002 01/01/2015

CLARK, COWLITZ, KLICKKITAT, PACIFIC (SOUTH), SKAMANIA, AND  
WAHAKIAKUM COUNTIES

POWER EQUIPMENT OPERATORS: ZONE 1

|                          | Rates    | Fringes |
|--------------------------|----------|---------|
| POWER EQUIPMENT OPERATOR |          |         |
| GROUP 1.....             | \$ 39.47 | 14.10   |
| GROUP 1A.....            | \$ 41.44 | 14.10   |
| GROUP 1B.....            | \$ 43.42 | 14.10   |
| GROUP 2.....             | \$ 37.58 | 14.10   |
| GROUP 3.....             | \$ 36.44 | 14.10   |
| GROUP 4.....             | \$ 35.36 | 14.10   |
| GROUP 5.....             | \$ 34.13 | 14.10   |
| GROUP 6.....             | \$ 30.94 | 14.10   |

Zone Differential (add to Zone 1 rates):

Zone 2 - \$3.00

Zone 3 - \$6.00

For the following metropolitan counties: MULTNOMAH;  
CLACKAMAS; MARION; WASHINGTON; YAMHILL; AND COLUMBIA;  
CLARK; AND COWLITZ COUNTY, WASHINGTON WITH MODIFICATIONS AS  
INDICATED:

All jobs or projects located in Multnomah, Clackamas and Marion Counties, West of the western boundary of Mt. Hood National Forest and West of Mile Post 30 on Interstate 84 and West of Mile Post 30 on State Highway 26 and West of Mile Post 30 on Highway 22 and all jobs or projects located in Yamhill County, Washington County and Columbia County and all jobs or projects located in Clark & Cowlitz County, Washington except that portion of Cowlitz County in the Mt. St. Helens "Blast Zone" shall receive Zone I pay for all classifications.

All jobs or projects located in the area outside the identified boundary above, but less than 50 miles from the Portland City Hall shall receive Zone II pay for all classifications.

All jobs or projects located more than 50 miles from the Portland City Hall, but outside the identified border above, shall receive Zone III pay for all classifications.

For the following cities: ALBANY; BEND; COOS BAY; EUGENE;  
GRANTS PASS; KLAMATH FALLS; MEDFORD; ROSEBURG

All jobs or projects located within 30 miles of the respective city hall of the above mentioned cities shall receive Zone I pay for all classifications.

All jobs or projects located more than 30 miles and less than 50 miles from the respective city hall of the above mentioned cities shall receive Zone II pay for all classifications.

All jobs or projects located more than 50 miles from the respective city hall of the above mentioned cities shall receive Zone III pay for all classifications.

## POWER EQUIPMENT OPERATORS CLASSIFICATIONS

## Group 1

Concrete Batch Plant and or Wet mix three (3) units or more; Crane, Floating one hundred and fifty (150) ton but less than two hundred and fifty (250) ton; Crane, two hundred (200) ton through two hundred ninety nine (299) ton with two hundred foot (200') boom or less (including jib, inserts and/or attachments); Crane, ninety (90) ton through one hundred ninety nine (199) ton with over two hundred (200') boom Including jib, inserts and/or attachments); Crane, Tower Crane with one hundred seventy five foot (175') tower or less and with less than two hundred foot (200') jib; Crane, Whirley ninety (90) ton and over; Helicopter when used in erecting work

## Group 1A

Crane, floating two hundred fifty (250) ton and over; Crane, two hundred (200) ton through two hundred ninety nine (299) ton, with over two hundred foot (200') boom (including jib, inserts and/or attachments); Crane, three hundred (300) ton through three hundred ninety nine (399) ton; Crane, Tower Crane with over one hundred seventy five foot (175') tower or over two hundred foot (200') jib; Crane, tower Crane on rail system or 2nd tower or more in work radius

## Group 1B

Crane, three hundred (300) ton through three hundred ninety nine (399) ton, with over two hundred foot (200') boom (including jib, inserts and/or attachments); Floating crane, three hundred fifty (350) ton and over; Crane, four hundred (400) ton and over

## Group 2

Asphalt Plant (any type); Asphalt Roto-Mill, pavement profiler eight foot (8') lateral cut and over; Auto Grader or "Trimmer"; Blade, Robotic; Bulldozer, Robotic Equipment (any type); Bulldozer, over one hundred twenty thousand (120,000) lbs. and above; Concrete Batch Plant and/or Wet Mix one (1) and two (2) drum; Concrete Diamond Head Profiler; Canal Trimmer; Concrete, Automatic Slip Form Paver (Assistant to the Operator required); Crane, Boom Truck fifty (50) ton and with over one hundred fifty foot (150') boom and over; Crane, Floating (derrick barge) thirty (30) ton but less than one hundred fifty (150) ton; Crane, Cableway twenty-five (25) ton and over; Crane, Floating Clamshell three (3) cu. Yds. And over; Crane, ninety (90) ton through one hundred ninety nine (199) ton up to and including two hundred foot (200') of boom (including jib inserts and/or attachments); Crane, fifty (50) ton through eighty nine (89) ton with over one hundred fifty foot (150') boom (including jib inserts and/or attachments); Crane, Whirley under ninety (90) ton; Crusher Plant; Excavator over one hundred thirty thousand (130,000) lbs.; Loader one hundred twenty thousand (120,000) lbs. and above; Remote Controlled Earth Moving Equipment; Shovel, Dragline, Clamshell, five (5) cu. Yds. And over; Underwater Equipment remote or otherwise, when used in construction work; Wheel Excavator any size

## Group 3

Bulldozer, over seventy thousand (70,000) lbs. up to and including one hundred twenty thousand (120,000) lbs.; Crane, Boom Truck fifty (50) ton and over with less than one hundred fifty foot (150') boom; Crane, fifty (50) ton through eighty nine (89) ton with one hundred fifty foot (150') boom or less (including jib inserts and/or attachments); Crane, Shovel, Dragline or Clamshell three (3) cu. yds. but less than five (5) cu. Yds.; Excavator over eighty thousand (80,000) lbs. through one hundred thirty thousand (130,000) lbs.; Loader sixty thousand (60,000) lbs. and less than one hundred twenty thousand (120,000) lbs.

## Group 4

Asphalt, Screed; Asphalt Paver; Asphalt Roto-Mill, pavement profiler, under eight foot (8') lateral cut; Asphalt, Material Transfer Vehicle Operator; Back Filling Machine; Backhoe, Robotic, track and wheel type up to and including twenty thousand (20,000) lbs. with any attachments; Blade (any type); Boatman; Boring Machine; Bulldozer over twenty thousand (20,000) lbs. and more than one hundred (100) horse up to seventy thousand (70,000) lbs.; Cable-Plow (any type); Cableway up to twenty five (25) ton; Cat Drill (John Henry); Chippers; Compactor, multi-engine; Compactor, Robotic; Compactor with blade self-propelled; Concrete, Breaker; Concrete, Grout Plant; Concrete, Mixer Mobile; Concrete, Paving Road Mixer; Concrete, Reinforced Tank Banding Machine; Crane, Boom Truck twenty (20) ton and under fifty (50) ton; Crane, Bridge Locomotive, Gantry and Overhead; Crane, Carry Deck; Crane, Chicago Boom and similar types; Crane, Derrick Operator, under one hundred (100) ton; Crane, Floating Clamshell, Dragline, etc. Operator, under three (3) cu. yds. Or less than thirty (30) ton; Crane, under fifty (50) ton; Crane, Quick Tower under one hundred foot (100') in height and less than one hundred fifty foot (150') jib (on rail included); Diesel-Electric Engineer (Plant or Floating); Directional Drill over twenty thousand (20,000) lbs. pullback; Drill Cat Operator; Drill Doctor and/or Bit Grinder; Driller, Percussion, Diamond, Core, Cable, Rotary and similar type; Excavator Operator over twenty thousand (20,000) lbs. through eighty thousand (80,000) lbs.; Generator Operator; Grade-all; Guardrail Machines, i.e. punch, auger, etc.; Hammer Operator (Piledriver); Hoist, stiff leg, guy derrick or similar type, fifty (50) ton and over; Hoist, two (2) drums or more; Hydro Axe (loader mounted or similar type); Jack Operator, Elevating Barges, Barge Operator, self-unloading; Loader Operator, front end and overhead, twenty five thousand (25,000) lbs. and less than sixty thousand (60,000) lbs.; Log Skidders; Piledriver Operator (not crane type); Pipe, Bending, Cleaning, Doping and Wrapping Machines; Rail, Ballast Tamper Multi-Purpose; Rubber-tired Dozers and Pushers; Scraper, all types; Side-Boom; Skip Loader, Drag Box; Strump Grinder (loader mounted or similar type); Surface Heater and Planer; Tractor, rubber-tired, over fifty (50) HP Flywheel; Trenching Machine three foot (3') depth and deeper; Tub Grinder (used for wood debris);

Tunnel Boring Machine Mechanic; Tunnel, Mucking Machine;  
 Ultra High Pressure Water Jet Cutting Tool System Operator;  
 Vacuum Blasting Machine Operator; Water pulls, Water wagons  
 Group 5

Asphalt, Extrusion Machine; Asphalt, Roller (any asphalt mix); Asphalt, Roto-Mill pavement profiler ground man; Bulldozer, twenty thousand (20,000) lbs. or less, or one hundred (100) horse or less; Cement Pump; Chip Spreading Machine; Churn Drill and Earth Boring Machine; Compactor, self-propelled without blade; Compressor, (any power) one thousand two hundred fifty (1,250) cu. ft. and over, total capacity; Concrete, Batch Plant Quality control; Concrete, Combination Mixer and compressor operator, gunite work; Concrete, Curb Machine, Mechanical Berm, Curb and/or Curb and Gutter; Concrete, Finishing Machine; Concrete, Grouting Machine; Concrete, Internal Full Slab Vibrator Operator; Concrete, Joint Machine; Concrete, Mixer single drum, any capacity; Concrete, Paving Machine eight foot (8') or less; Concrete, Planer; Concrete, Pump; Concrete, Pump Truck; Concrete, Pumpcrete Operator (any type); Concrete, Slip Form Pumps, power driven hydraulic lifting device for concrete forms; Conveyored Material Hauler; Crane, Boom Truck under twenty (20) tons; Crane, Boom Type lifting device, five (5) ton capacity or less; Drill, Directional type less than twenty thousand (20,000) lbs. pullback; Fork Lift, over ten (10) ton or Robotic; Helicopter Hoist; Hoist Operator, single drum; Hydraulic Backhoe track type up to and including twenty thousand (20,000) lbs.; Hydraulic Backhoe wheel type (any make); Laser Screed; Loaders, rubber-tired type, less than twenty five thousand (25,000) lbs.; Pavement Grinder and/or Grooving Machine (riding type); Pipe, cast in place Pipe Laying Machine; Pulva-Mixer or similar types; Pump Operator, more than five (5) pumps (any size); Rail, Ballast Compactor, Regulator, or Tamper machines; Service Oiler (Greaser); Sweeper Self-Propelled; Tractor, Rubber-Tired, fifty (50) HP flywheel and under; Trenching Machine Operator, maximum digging capacity three foot (3') depth; Tunnel, Locomotive, Dinkey; Tunnel, Power Jumbo setting slip forms, etc.

Group 6

Asphalt, Pugmill (any type); Asphalt, Raker; Asphalt, Truck Mounted Asphalt Spreader, with Screed; Auger Oiler; Boatman; Bobcat, skid steed (less than one (1) yard); Broom, self-propelled; Compressor Operator (any power) under 1,250 cu. ft. total capacity; Concrete Curing Machine (riding type); Concrete Saw; Conveyor Operator or Assistant; Crane, Tugger; Crusher Feeder; Crusher Oiler; Deckhand; Drill, Directional Locator; Fork Lift; Grade Checker; Guardrail Punch Oiler; Hydrographic Seeder Machine, straw, pulp or seed; Hydrostatic Pump Operator; Mixer Box (CTB, dry batch, etc.); Oiler; Plant Oiler; Pump (any power); Rail, Brakeman, Switchman, Motorman; Rail, Tamping Machine, mechanical, self-propelled; Rigger; Roller grading (not asphalt); Truck, Crane Oiler-Driver

IRON0014-005 07/01/2015

ADAMS, ASOTIN, BENTON, COLUMBIA, DOUGLAS, FERRY, FRANKLIN,  
GARFIELD, GRANT, LINCOLN, OKANOGAN, PEND ORIELLE, SPOKANE,  
STEVENS, WALLA WALLA AND WHITMAN COUNTIES

|                 | Rates    | Fringes |
|-----------------|----------|---------|
| IRONWORKER..... | \$ 32.76 | 23.19   |

IRON0029-002 07/01/2015

CLARK, COWLITZ, KLICKITAT, PACIFIC, SKAMANIA, AND WAHKAIKUM  
COUNTIES

|                 | Rates    | Fringes |
|-----------------|----------|---------|
| IRONWORKER..... | \$ 34.12 | 23.04   |

IRON0086-002 07/01/2015

YAKIMA, KITTITAS AND CHELAN COUNTIES

|                 | Rates    | Fringes |
|-----------------|----------|---------|
| IRONWORKER..... | \$ 32.76 | 23.19   |

IRON0086-004 07/01/2015

CLALLAM, GRAYS HARBOR, ISLAND, JEFFERSON, KING, KITSAP, LEWIS,  
MASON, PIERCE, SKAGIT, SNOHOMISH, THURSTON, AND WHATCOM COUNTIES

|                 | Rates    | Fringes |
|-----------------|----------|---------|
| IRONWORKER..... | \$ 40.04 | 23.19   |

LABO0001-002 06/01/2014

ZONE 1:

Rates Fringes

Laborers:

CALLAM, GRAYS HARBOR, ISLAND, JEFFERSON, KING, KITSAP, LEWIS, MASON, PACIFIC (NORTH OF STRAIGHT LINE MADE BY EXTENDING THE NORTH BOUNDARY WAHKIAKUM COUNTY WEST TO THE PACIFIC OCEAN), PIERCE, SAN JUAN, SKAGIT, SNOHOMISH, THURSTON AND WHATCOM COUNTIES

|              |          |       |
|--------------|----------|-------|
| GROUP 1..... | \$ 22.49 | 10.30 |
| GROUP 2..... | \$ 25.79 | 10.30 |
| GROUP 3..... | \$ 32.29 | 10.30 |
| GROUP 4..... | \$ 33.08 | 10.30 |
| GROUP 5..... | \$ 33.62 | 10.30 |

CHELAN, DOUGLAS (WEST OF THE 120TH MERIDIAN), KITTITAS AND YAKIMA COUNTIES

|              |          |       |
|--------------|----------|-------|
| GROUP 1..... | \$ 18.95 | 10.30 |
| GROUP 2..... | \$ 21.76 | 10.30 |
| GROUP 3..... | \$ 23.85 | 10.30 |
| GROUP 4..... | \$ 24.43 | 10.30 |
| GROUP 5..... | \$ 24.85 | 10.30 |

BASE POINTS: BELLINGHAM, MT. VERNON, EVERETT, SEATTLE, KENT, TACOMA, OLYMPIA, CENTRALIA, ABERDEEN, SHELTON, PT. TOWNSEND, PT. ANGELES, AND BREMERTON

- ZONE 1 - Projects within 25 radius miles of the respective city hall
- ZONE 2 - More than 25 but less than 45 radius miles from the respective city hall
- ZONE 3 - More than 45 radius miles from the respective city hall

ZONE DIFFERENTIAL (ADD TO ZONE 1 RATES):

- ZONE 2 - \$1.00
- ZONE 3 - \$1.30

BASE POINTS: CHELAN, SUNNYSIDE, WENATCHEE, AND YAKIMA

- ZONE 1 - Projects within 25 radius miles of the respective city hall
- ZONE 2 - More than 25 radius miles from the respective city hall

ZONE DIFFERENTIAL (ADD TO ZONE 1 RATES):

- ZONE 2 - \$2.25

## LABORERS CLASSIFICATIONS

GROUP 1: Landscaping and Planting; Watchman; Window Washer/Cleaner (detail clean-up, such as but not limited to cleaning floors, ceilings, walls, windows, etc., prior to final acceptance by the owner)

GROUP 2: Batch Weighman; Crusher Feeder; Fence Laborer; Flagman; Pilot Car

GROUP 3: General Laborer; Air, Gas, or Electric Vibrating Screed; Asbestos Abatement Laborer; Ballast Regulator Machine; Brush Cutter; Brush Hog Feeder; Burner; Carpenter Tender; Cement Finisher Tender; Change House or Dry Shack; Chipping Gun (under 30 lbs.); Choker Setter; Chuck Tender; Clean-up Laborer; Concrete Form Stripper; Curing Laborer; Demolition (wrecking and moving including charred material); Ditch Digger; Dump Person; Fine Graders; Firewatch; Form Setter; Gabian Basket Builders; Grout Machine Tender; Grinders; Guardrail Erector; Hazardous Waste Worker (Level C: uses a chemical "splash suit" and air purifying respirator); Maintenance Person; Material Yard Person; Pot Tender; Rip Rap Person; Riggers; Scale Person; Sloper Sprayer; Signal Person; Stock Piler; Stake Hopper; Toolroom Man (at job site); Topper-Tailer; Track Laborer; Truck Spotter; Vinyl Seamer

GROUP 4: Cement Dumper-Paving; Chipping Gun (over 30 lbs.); Clary Power Spreader; Concrete Dumper/Chute Operator; Concrete Saw Operator; Drill Operator (hydraulic, diamond, aiartrac); Faller and Bucker Chain Saw; Grade Checker and Transit Person; Groutmen (pressure) including post tension beams; Hazardous Waste Worker (Level B: uses same respirator protection as Level A. A supplied air line is provided in conjunction with a chemical "splash suit"); High Scaler; Jackhammer; Laserbeam Operator; Manhole Builder-Mudman; Nozzleman (concrete pump, green cutter when using combination of high pressure air and water on concrete and rock, sandblast, gunite, shotcrete, water blaster, vacuum blaster); Pavement Breaker; Pipe Layer and Caulker; Pipe Pot Tender; Pipe Reliner (not insert type); Pipe Wrapper; Power Jacks; Railroad Spike Puller-Power; Raker-Asphalt; Rivet Buster; Rodder; Sloper (over 20 ft); Spreader (concrete); Tamper and Similar electric, air and glas operated tool; Timber Person-sewer (lagger shorer and cribber); Track Liner Power; Tugger Operator; Vibrator; Well Point Laborer

GROUP 5: Caisson Worker; Miner; Mortarman and Hodcarrier; Powderman; Re-Timberman; Hazardous Waste Worker (Level A: utilizes a fully encapsulated suit with a self-contained breathing apparatus or a supplied air line).

LABO0238-004 06/01/2014

PASCO AREA: ADAMS, BENTON, COLUMBIA, DOUGLAS (East of 120th Meridian), FERRY, FRANKLIN, GRANT, OKANOGAN, WALLA WALLA

SPOKANE AREA: ASOTIN, GARFIELD, LINCOLN, PEND OREILLE, SPOKANE, STEVENS & WHITMAN COUNTIES

|                   | Rates    | Fringes |
|-------------------|----------|---------|
| LABORER (PASCO)   |          |         |
| GROUP 1.....      | \$ 22.25 | 10.95   |
| GROUP 2.....      | \$ 24.35 | 10.95   |
| GROUP 3.....      | \$ 24.62 | 10.95   |
| GROUP 4.....      | \$ 24.89 | 10.95   |
| GROUP 5.....      | \$ 25.17 | 10.95   |
| LABORER (SPOKANE) |          |         |
| GROUP 1.....      | \$ 21.95 | 10.95   |
| GROUP 2.....      | \$ 24.05 | 10.95   |
| GROUP 3.....      | \$ 24.32 | 10.95   |
| GROUP 4.....      | \$ 24.59 | 10.95   |
| GROUP 5.....      | \$ 24.87 | 10.95   |

Zone Differential (Add to Zone 1 rate): \$2.00

BASE POINTS: Spokane, Pasco, Lewiston

Zone 1: 0-45 radius miles from the main post office.

Zone 2: 45 radius miles and over from the main post office.

## LABORERS CLASSIFICATIONS

GROUP 1: Flagman; Landscape Laborer; Scaleman; Traffic Control Maintenance Laborer (to include erection and maintenance of barricades, signs and relief of flagperson); Window Washer/Cleaner (detail cleanup, such as, but not limited to cleaning floors, ceilings, walls, windows, etc. prior to final acceptance by the owner)

GROUP 2: Asbestos Abatement Worker; Brush Hog Feeder; Carpenter Tender; Cement Handler; Clean-up Laborer; Concrete Crewman (to include stripping of forms, hand operating jacks on slip form construction, application of concrete curing compounds, pumpcrete machine, signaling, handling the nozzle of squeezecrete or similar machine, 6 inches and smaller); Confined Space Attendant; Concrete Signalman; Crusher Feeder; Demolition (to include clean-up, burning, loading, wrecking and salvage of all material); Dumpman; Fence Erector; Firewatch; Form Cleaning Machine Feeder, Stacker; General Laborer; Grout Machine Header Tender; Guard Rail (to include guard rails, guide and reference posts, sign posts, and right-of-way markers); Hazardous Waste Worker, Level D (no respirator is used and skin protection is minimal); Miner, Class "A" (to include all bull gang, concrete crewman, dumpman and pumpcrete crewman, including distributing pipe, assembly & dismantle, and nipper); Nipper; Riprap Man; Sandblast Tailhoseman; Scaffold Erector (wood or steel); Stake Jumper; Structural Mover (to include separating foundation, preparation, cribbing, shoring, jacking and unloading of structures); Tailhoseman (water nozzle); Timber Bucker and Faller (by hand); Track Laborer (RR); Truck Loader; Well-Point Man; All Other Work Classifications Not Specially Listed Shall Be Classified As General Laborer

GROUP 3: Asphalt Roller, walking; Cement Finisher Tender; Concrete Saw, walking; Demolition Torch; Dope Pot Firemen, non-mechanical; Driller Tender (when required to move and position machine); Form Setter, Paving; Grade Checker using level; Hazardous Waste Worker, Level C (uses a chemical "splash suit" and air purifying respirator); Jackhammer Operator; Miner, Class "B" (to include brakeman, finisher, vibrator, form setter); Nozzleman (to include squeeze and flo-crete nozzle); Nozzleman, water, air or steam; Pavement Breaker (under 90 lbs.); Pipelayer, corrugated metal culvert; Pipelayer, multi-plate; Pot Tender; Power Buggy Operator; Power Tool Operator, gas, electric, pneumatic; Railroad Equipment, power driven, except dual mobile power spiker or puller; Railroad Power Spiker or Puller, dual mobile; Rodder and Spreader; Tamper (to include operation of Barco, Essex and similar tampers); Trencher, Shawnee; Tugger Operator; Wagon Drills; Water Pipe Liner; Wheelbarrow (power driven)

GROUP 4: Air and Hydraulic Track Drill; Asphalt Raker; Brush Machine (to include horizontal construction joint cleanup brush machine, power propelled); Caisson Worker, free air; Chain Saw Operator and Faller; Concrete Stack (to include laborers when laborers working on free standing concrete stacks for smoke or fume control above 40 feet high); Gunitite (to include operation of machine and nozzle); Hazardous Waste Worker, Level B (uses same respirator protection as Level A. A supplied air line is provided in conjunction with a chemical "splash suit"); High Scaler; Laser Beam Operator (to include grade checker and elevation control); Miner, Class C (to include miner, nozzleman for concrete, laser beam operator and rigger on tunnels); Monitor Operator (air track or similar mounting); Mortar Mixer; Nozzleman (to include jet blasting nozzleman, over 1,200 lbs., jet blast machine power propelled, sandblast nozzle); Pavement Breaker (90 lbs. and over); Pipelayer (to include working topman, caulker, collarman, jointer, mortarman, rigger, jacker, shorer, valve or meter installer); Pipewrapper; Plasterer Tender; Vibrators (all)

GROUP 5 - Drills with Dual Masts; Hazardous Waste Worker, Level A (utilizes a fully encapsulated suit with a self-contained breathing apparatus or a supplied air line); Miner Class "D", (to include raise and shaft miner, laser beam operator on riases and shafts)

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LABO0238-006 06/01/2014

COUNTIES EAST OF THE 120TH MERIDIAN: ADAMS, ASOTIN, BENTON, CHELAN, COLUMBIA, DOUGLAS, FERRY, FRANKLIN, GARFIELD, GRANT, LINCOLN, OKANOGAN, PEND OREILLE, STEVENS, SPOKANE, WALLA WALLA, WHITMAN

|                  | Rates    | Fringes |
|------------------|----------|---------|
| Hod Carrier..... | \$ 24.32 | 10.95   |

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LABO0335-001 06/01/2013

CLARK, COWLITZ, KLICKITAT, PACIFIC (SOUTH OF A STRAIGHT LINE MADE BY EXTENDING THE NORTH BOUNDARY LINE OF WAHKIAKUM COUNTY WEST TO THE PACIFIC OCEAN), SKAMANIA AND WAHKIAKUM COUNTIES

|              | Rates    | Fringes |
|--------------|----------|---------|
| Laborers:    |          |         |
| ZONE 1:      |          |         |
| GROUP 1..... | \$ 28.65 | 10.05   |
| GROUP 2..... | \$ 29.25 | 10.05   |
| GROUP 3..... | \$ 29.69 | 10.05   |
| GROUP 4..... | \$ 30.07 | 10.05   |
| GROUP 5..... | \$ 26.15 | 10.05   |
| GROUP 6..... | \$ 23.73 | 10.05   |
| GROUP 7..... | \$ 20.53 | 10.05   |

Zone Differential (Add to Zone 1 rates):

Zone 2 \$ 0.65  
 Zone 3 - 1.15  
 Zone 4 - 1.70  
 Zone 5 - 2.75

BASE POINTS: GOLDENDALE, LONGVIEW, AND VANCOUVER

ZONE 1: Projects within 30 miles of the respective city all.  
 ZONE 2: More than 30 miles but less than 40 miles from the  
 respective city hall.  
 ZONE 3: More than 40 miles but less than 50 miles from the  
 respective city hall.  
 ZONE 4: More than 50 miles but less than 80 miles from the  
 respective city hall.  
 ZONE 5: More than 80 miles from the respective city hall.

#### LABORERS CLASSIFICATIONS

GROUP 1: Asphalt Plant Laborers; Asphalt Spreaders; Batch Weighman; Broomers; Brush Burners and Cutters; Car and Truck Loaders; Carpenter Tender; Change-House Man or Dry Shack Man; Choker Setter; Clean-up Laborers; Curing, Concrete; Demolition, Wrecking and Moving Laborers; Dumpers, road oiling crew; Dumpmen (for grading crew); Elevator Feeders; Median Rail Reference Post, Guide Post, Right of Way Marker; Fine Graders; Fire Watch; Form Strippers (not swinging stages); General Laborers; Hazardous Waste Worker; Leverman or Aggregate Spreader (Flaherty and similar types); Loading Spotters; Material Yard Man (including electrical); Pittsburgh Chipper Operator or Similar Types; Railroad Track Laborers; Ribbon Setters (including steel forms); Rip Rap Man (hand placed); Road Pump Tender; Sewer Labor; Signalman; Skipman; Slopers; Spraymen; Stake Chaser; Stockpiler; Tie Back Shoring; Timber Faller and Bucker (hand labor); Toolroom Man (at job site); Tunnel Bullgang (above ground); Weight-Man- Crusher (aggregate when used)

GROUP 2: Applicator (including pot power tender for same), applying protective material by hand or nozzle on utility lines or storage tanks on project; Brush Cutters (power saw); Burners; Choker Splicer; Clary Power Spreader and similar types; Clean- up Nozzleman-Green Cutter (concrete, rock, etc.); Concrete Power Buggyman; Concrete Laborer; Crusher Feeder; Demolition and Wrecking Charred Materials; Gunite Nozzleman Tender; Gunite or Sand Blasting Pot Tender; Handlers or Mixers of all Materials of an irritating nature (including cement and lime); Tool Operators (includes but not limited to: Dry Pack Machine; Jackhammer; Chipping Guns; Paving Breakers); Pipe Doping and Wrapping; Post Hole Digger, air, gas or electric; Vibrating Screed; Tampers; Sand Blasting (Wet); Stake-Setter; Tunnel-Muckers, Brakemen, Concrete Crew, Bullgang (underground)

GROUP 3: Asbestos Removal; Bit Grinder; Drill Doctor; Drill Operators, air tracks, cat drills, wagon drills, rubber-mounted drills, and other similar types including at crusher plants; Gunitite Nozzleman; High Scalers, Strippers and Drillers (covers work in swinging stages, chairs or belts, under extreme conditions unusual to normal drilling, blasting, barring-down, or sloping and stripping); Manhole Builder; Powdermen; Concrete Saw Operator; Powdermen; Power Saw Operators (Bucking and Falling); Pumpcrete Nozzlemen; Sand Blasting (Dry); Sewer Timberman; Track Liners, Anchor Machines, Ballast Regulators, Multiple Tampers, Power Jacks, Tugger Operator; Tunnel-Chuck Tenders, Nippers and Timbermen; Vibrator; Water Blaster

GROUP 4: Asphalt Raker; Concrete Saw Operator (walls); Concrete Nozzelman; Grade Checker; Pipelayer; Laser Beam (pipelaying)-applicable when employee assigned to move, set up, align; Laser Beam; Tunnel Miners; Motorman-Dinky Locomotive-Tunnel; Powderman-Tunnel; Shield Operator-Tunnel

GROUP 5: Traffic Flaggers

GROUP 6: Fence Builders

GROUP 7: Landscaping or Planting Laborers

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LABO0335-019 09/01/2013

|                  | Rates    | Fringes |
|------------------|----------|---------|
| Hod Carrier..... | \$ 30.47 | 10.05   |

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PAIN0005-002 07/01/2015

STATEWIDE EXCEPT CLARK, COWLITZ, KLICKITAT, PACIFIC (SOUTH), SKAMANIA, AND WAHAKIYAKUM COUNTIES

|                            | Rates    | Fringes |
|----------------------------|----------|---------|
| Painters:<br>STRIPERS..... | \$ 27.44 | 14.09   |

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PAIN0005-004 03/01/2009

CLALLAM, GRAYS HARBOR, ISLAND, JEFFERSON, KING, KITSAP, LEWIS, MASON, PIERCE, SAN JUAN, SKAGIT, SNOHOMISH, THURSTON AND WHATCOM COUNTIES

|              | Rates    | Fringes |
|--------------|----------|---------|
| PAINTER..... | \$ 20.82 | 7.44    |

PAIN0005-006 07/01/2015

ADAMS, ASOTIN; BENTON AND FRANKLIN (EXCEPT HANFORD SITE);  
 CHELAN, COLUMBIA, DOUGLAS, FERRY, GARFIELD, GRANT, KITTITAS,  
 LINCOLN, OKANOGAN, PEND OREILLE, SPOKANE, STEVENS, WALLA WALLA,  
 WHITMAN AND YAKIMA COUNTIES

|   | Rates    | Fringes |
|---|----------|---------|
| PAINTER   |          |         |
| Application of Cold Tar<br>Products, Epoxies, Polyure<br>thanes, Acids, Radiation<br>Resistant Material, Water<br>and Sandblasting..... | \$ 28.15 | 10.85   |
| Over 30'/Swing Stage Work..   | \$ 22.20 | 7.98    |
| Brush, Roller, Striping,<br>Steam-cleaning and Spray....  | \$ 23.05 | 10.85   |
| Lead Abatement, Asbestos<br>Abatement.....  | \$ 21.50 | 7.98    |

\*\$.70 shall be paid over and above the basic wage rates  
 listed for work on swing stages and high work of over 30  
 feet.

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 PAIN0055-003 07/01/2015

CLARK, COWLITZ, KLICKITAT, PACIFIC, SKAMANIA, AND WAHAKIYAKUM  
 COUNTIES

|   | Rates    | Fringes |
|---|----------|---------|
| PAINTER                                       |          |         |
| Brush & Roller.....                           | \$ 22.47 | 10.13   |
| High work - All work 60<br>ft. or higher..... | \$ 23.22 | 10.13   |
| Spray and Sandblasting.....                   | \$ 23.07 | 10.13   |

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 PAIN0055-006 11/01/2014

CLARK, COWLITZ, KLICKITAT, SKAMANIA and WAHAKIYAKUM COUNTIES

|                                       | Rates    | Fringes |
|---------------------------------------|----------|---------|
| Painters:                             |          |         |
| HIGHWAY & PARKING LOT<br>STRIPER..... | \$ 33.43 | 11.08   |

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PLAS0072-004 06/01/2015

ADAMS, ASOTIN, BENTON, CHELAN, COLUMBIA, DOUGLAS, FERRY,  
FRANKLIN, GARFIELD, GRANT, KITTITAS, LINCOLN, OKANOGAN, PEND  
OREILLE, SPOKANE, STEVENS, WALLA WALLA, WHITMAN, AND YAKIMA  
COUNTIES

|                                | Rates    | Fringes |
|--------------------------------|----------|---------|
| CEMENT MASON/CONCRETE FINISHER |          |         |
| ZONE 1.....                    | \$ 27.01 | 12.59   |

Zone Differential (Add to Zone 1 rate): Zone 2 - \$2.00

BASE POINTS: Spokane, Pasco, Lewiston; Wenatchee  
Zone 1: 0 - 45 radius miles from the main post office  
Zone 2: Over 45 radius miles from the main post office

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PLAS0528-001 06/01/2015

CLALLAM, COWLITZ, GRAYS HARBOR, ISLAND, JEFFERSON, KING,  
KITSAP, LEWIS, MASON, PACIFIC, PIERCE, SAN JUAN, SKAGIT,  
SNOHOMISH, THURSTON, WAHKIAKUM AND WHATCOM COUNTIES

|  | Rates    | Fringes |
|--|----------|---------|
| CEMENT MASON   |          |         |
| CEMENT MASON.....  | \$ 38.52 | 15.43   |
| COMPOSITION, TROWEL<br>MACHINE, GRINDER, POWER<br>TOOLS, GUNNITE NOZZLE..... | \$ 39.02 | 15.43   |
| TROWLING MACHINE OPERATOR<br>ON COMPOSITION.....                             | \$ 39.02 | 15.43   |

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PLAS0555-002 06/01/2015

CLARK, KLICKITAT AND SKAMANIA COUNTIES

ZONE 1:

|  | Rates    | Fringes |
|--|----------|---------|
| CEMENT MASON   |          |         |
| CEMENT MASONS DOING BOTH<br>COMPOSITION/POWER<br>MACHINERY AND<br>SUSPENDED/HANGING SCAFFOLD.. | \$ 30.58 | 18.18   |
| CEMENT MASONS ON<br>SUSPENDED, SWINGING AND/OR<br>HANGING SCAFFOLD.....                        | \$ 30.58 | 18.18   |
| CEMENT MASONS.....   | \$ 29.98 | 18.18   |
| COMPOSITION WORKERS AND<br>POWER MACHINERY OPERATORS...  | \$ 31.18 | 18.18   |

## Zone Differential (Add To Zone 1 Rates):

Zone 2 - \$0.65  
 Zone 3 - 1.15  
 Zone 4 - 1.70  
 Zone 5 - 3.00

BASE POINTS: BEND, CORVALLIS, EUGENE, MEDFORD, PORTLAND,  
 SALEM, THE DALLES, VANCOUVER

ZONE 1: Projects within 30 miles of the respective city hall

ZONE 2: More than 30 miles but less than 40 miles from the  
 respective city hall.

ZONE 3: More than 40 miles but less than 50 miles from the  
 respective city hall.

ZONE 4: More than 50 miles but less than 80 miles from the  
 respective city hall.

ZONE 5: More than 80 miles from the respective city hall

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 TEAM0037-002 06/01/2014

CLARK, COWLITZ, KLICKITAT, PACIFIC (South of a straight line  
 made by extending the north boundary line of Wahkiakum County  
 west to the Pacific Ocean), SKAMANIA, AND WAHKIAKUM COUNTIES

|  | Rates | Fringes |
|--|-------|---------|
|--|-------|---------|

## Truck drivers:

|              |          |       |
|--------------|----------|-------|
| ZONE 1       |          |       |
| GROUP 1..... | \$ 26.90 | 14.37 |
| GROUP 2..... | \$ 27.02 | 14.37 |
| GROUP 3..... | \$ 27.15 | 14.37 |
| GROUP 4..... | \$ 27.41 | 14.37 |
| GROUP 5..... | \$ 27.63 | 14.37 |
| GROUP 6..... | \$ 27.79 | 14.37 |
| GROUP 7..... | \$ 27.99 | 14.37 |

## Zone Differential (Add to Zone 1 Rates):

Zone 2 - \$0.65  
 Zone 3 - 1.15  
 Zone 4 - 1.70  
 Zone 5 - 2.75

BASE POINTS: ASTORIA, THE DALLES, LONGVIEW AND VANCOUVER

ZONE 1: Projects within 30 miles of the respective city  
 hall.

ZONE 2: More than 30 miles but less than 40 miles from the  
 respective city hall.

ZONE 3: More than 40 miles but less than 50 miles from the  
 respective city hall.

ZONE 4: More than 50 miles but less than 80 miles from the  
 respective city hall.

ZONE 5: More than 80 miles from the respective city hall.

## TRUCK DRIVERS CLASSIFICATIONS

GROUP 1: A Frame or Hydra lift truck w/load bearing surface; Articulated Dump Truck; Battery Rebuilders; Bus or Manhaul Driver; Concrete Buggies (power operated); Concrete Pump Truck; Dump Trucks, side, end and bottom dumps, including Semi Trucks and Trains or combinations there of: up to and including 10 cu. yds.; Lift Jitneys, Fork Lifts (all sizes in loading, unloading and transporting material on job site); Loader and/or Leverman on Concrete Dry Batch Plant (manually operated); Pilot Car; Pickup Truck; Solo Flat Bed and misc. Body Trucks, 0-10 tons; Truck Tender; Truck Mechanic Tender; Water Wagons (rated capacity) up to 3,000 gallons; Transit Mix and Wet or Dry Mix - 5 cu. yds. and under; Lubrication Man, Fuel Truck Driver, Tireman, Wash Rack, Steam Cleaner or combinations; Team Driver; Slurry Truck Driver or Leverman; Tireman

GROUP 2: Boom Truck/Hydra-lift or Retracting Crane; Challenger; Dumpsters or similar equipment all sizes; Dump Trucks/Articulated Dumps 6 cu to 10 cu.; Flaherty Spreader Driver or Leverman; Lowbed Equipment, Flat Bed Semi-trailer or doubles transporting equipment or wet or dry materials; Lumber Carrier, Driver-Straddle Carrier (used in loading, unloading and transporting of materials on job site); Oil Distributor Driver or Leverman; Transit mix and wet or dry mix trucks: over 5 cu. yds. and including 7 cu. yds.; Vacuum Trucks; Water truck/Wagons (rated capacity) over 3,000 to 5,000 gallons

GROUP 3: Ammonia Nitrate Distributor Driver; Dump trucks, side, end and bottom dumps, including Semi Trucks and Trains or combinations thereof: over 10 cu. yds. and including 30 cu. yds. includes Articulated Dump Trucks; Self-Propelled Street Sweeper; Transit mix and wet or dry mix truck: over 7 cu yds. and including 11 cu yds.; Truck Mechanic-Welder-Body Repairman; Utility and Clean-up Truck; Water Wagons (rated capacity) over 5,000 to 10,000 gallons

GROUP 4: Asphalt Burner; Dump Trucks, side, end and bottom dumps, including Semi-Trucks and Trains or combinations thereof: over 30 cu. yds. and including 50 cu. yds. includes Articulated Dump Trucks; Fire Guard; Transit Mix and Wet or Dry Mix Trucks, over 11 cu. yds. and including 15 cu. yds.; Water Wagon (rated capacity) over 10,000 gallons to 15,000 gallons

GROUP 5: Composite Crewman; Dump Trucks, side, end and bottom dumps, including Semi Trucks and Trains or combinations thereof: over 50 cu. yds. and including 60 cu. yds. includes Articulated Dump Trucks

GROUP 6: Bulk Cement Spreader w/o Auger; Dry Pre-Batch concrete Mix Trucks; Dump trucks, side, end and bottom dumps, including Semi Trucks and Trains of combinations thereof: over 60 cu. yds. and including 80 cu. yds., and includes Articulated Dump Trucks; Skid Truck

GROUP 7: Dump Trucks, side, end and bottom dumps, including Semi Trucks and Trains or combinations thereof: over 80 cu. yds. and including 100 cu. yds., includes Articulated Dump Trucks; Industrial Lift Truck (mechanical tailgate)

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TEAM0174-001 06/01/2015

CLALLAM, GRAYS HARBOR, ISLAND, JEFFERSON, KING, KITSAP, LEWIS, MASON, PACIFIC (North of a straight line made by extending the north boundary line of Wahkiakum County west to the Pacific Ocean), PIERCE, SAN JUAN, SKAGIT, SNOHOMISH, THURSTON AND WHATCOM COUNTIES

|                | Rates    | Fringes |
|----------------|----------|---------|
| Truck drivers: |          |         |
| ZONE A:        |          |         |
| GROUP 1:.....  | \$ 32.18 | 17.27   |
| GROUP 2:.....  | \$ 31.34 | 17.27   |
| GROUP 3:.....  | \$ 28.53 | 17.27   |
| GROUP 4:.....  | \$ 23.56 | 17.27   |
| GROUP 5:.....  | \$ 31.73 | 17.27   |

ZONE B (25-45 miles from center of listed cities\*): Add \$.70 per hour to Zone A rates.

ZONE C (over 45 miles from centr of listed cities\*): Add \$1.00 per hour to Zone A rates.

\*Zone pay will be calculated from the city center of the following listed cities:

|            |               |            |           |
|------------|---------------|------------|-----------|
| BELLINGHAM | CENTRALIA     | RAYMOND    | OLYMPIA   |
| EVERETT    | SHELTON       | ANACORTES  | BELLEVUE  |
| SEATTLE    | PORT ANGELES  | MT. VERNON | KENT      |
| TACOMA     | PORT TOWNSEND | ABERDEEN   | BREMERTON |

#### TRUCK DRIVERS CLASSIFICATIONS

GROUP 1 - "A-frame or Hydralift" trucks and Boom trucks or similar equipment when "A" frame or "Hydralift" and Boom truck or similar equipment is used; Buggymobile; Bulk Cement Tanker; Dumpsters and similar equipment, Tournorockers, Tournowagon, Tournotrailer, Cat DW series, Terra Cobra, Le Tourneau, Westinghouse, Athye Wagon, Euclid Two and Four-Wheeled power tractor with trailer and similar top-loaded equipment transporting material: Dump Trucks, side, end and bottom dump, including semi-trucks and trains or combinations thereof with 16 yards to 30 yards capacity: Over 30 yards \$.15 per hour additional for each 10 yard increment; Explosive Truck (field mix) and similar equipment; Hyster Operators (handling bulk loose aggregates); Lowbed and Heavy Duty Trailer; Road Oil Distributor Driver; Spreader, Flaherty Transit mix used exclusively in heavy construction; Water Wagon and Tank Truck-3,000 gallons and over capacity

GROUP 2 - Bulllifts, or similar equipment used in loading or unloading trucks, transporting materials on job site; Dumpsters, and similar equipment, Tournorockers, Tournowagon, Turnotrailer, Cat. D.W. Series, Terra Cobra, Le Tourneau, Westinghouse, Athye wagon, Euclid two and four-wheeled power tractor with trailer and similar top-loaded equipment transporting material: Dump trucks, side, end and bottom dump, including semi-trucks and trains or combinations thereof with less than 16 yards capacity; Flatbed (Dual Rear Axle); Grease Truck, Fuel Truck, Greaser, Battery Service Man and/or Tire Service Man; Leverman and loader at bunkers and batch plants; Oil tank transport; Scissor truck; Slurry Truck; Sno-Go and similar equipment; Swampers; Straddler Carrier (Ross, Hyster) and similar equipment; Team Driver; Tractor (small, rubber-tired)(when used within Teamster jurisdiction); Vacuum truck; Water Wagon and Tank trucks-less than 3,000 gallons capacity; Winch Truck; Wrecker, Tow truck and similar equipment

GROUP 3 - Flatbed (single rear axle); Pickup Sweeper; Pickup Truck. (Adjust Group 3 upward by \$2.00 per hour for onsite work only)

GROUP 4 - Escort or Pilot Car

GROUP 5 - Mechanic

#### HAZMAT PROJECTS

Anyone working on a HAZMAT job, where HAZMAT certification is required, shall be compensated as a premium, in addition to the classification working in as follows:

LEVEL C: +\$.25 per hour - This level uses an air purifying respirator or additional protective clothing.

LEVEL B: +\$.50 per hour - Uses same respirator protection as Level A. Supplied air line is provided in conjunction with a chemical "splash suit."

LEVEL A: +\$.75 per hour - This level utilizes a fully-encapsulated suit with a self-contained breathing apparatus or a supplied air line.

TEAM0690-004 01/01/2014

ADAMS, ASOTIN, BENTON, CHELAN, COLUMBIA, DOUGLAS, FERRY,  
FRANKLIN, GARFIELD, GRANT KITTITAS, LINCOLN, OKANOGAN, PEND  
OREILLE, SPOKANE, STEVENS, WALLA WALLA, WHITMAN AND YAKIMA  
COUNTIES

Rates Fringes

SPOKANE ZONE CENTER: Adams, Chelan, Douglas, Ferry, Grant, Kittitas, Lincoln,  
Okanogan, Pen Oreille, Spokane, Stevens, and Whitman  
Counties

AREA 1: LEWISTON ZONE CENTER:  
Asotin, Columbia, and Garfield Counties

AREA 2: PASCO ZONE CENTER:  
Benton, Franklin, Walla Walla and Yakima Counties

|              |          |       |
|--------------|----------|-------|
| AREA 1:      |          |       |
| GROUP 1..... | \$ 20.17 | 15.19 |
| GROUP 2..... | \$ 22.44 | 15.19 |
| GROUP 3..... | \$ 22.94 | 15.19 |
| GROUP 4..... | \$ 23.27 | 15.19 |
| GROUP 5..... | \$ 23.38 | 15.19 |
| GROUP 6..... | \$ 23.55 | 15.19 |
| GROUP 7..... | \$ 24.08 | 15.19 |
| GROUP 8..... | \$ 24.44 | 15.19 |
| AREA 2       |          |       |
| GROUP 1..... | \$ 21.77 | 15.19 |
| GROUP 2..... | \$ 24.31 | 15.19 |
| GROUP 3..... | \$ 24.42 | 15.19 |
| GROUP 4..... | \$ 24.75 | 15.19 |
| GROUP 5..... | \$ 24.86 | 15.19 |
| GROUP 6..... | \$ 25.02 | 15.19 |
| GROUP 7..... | \$ 25.56 | 15.19 |
| GROUP 8..... | \$ 25.88 | 15.19 |

Zone Differential (Add to Zone 1 rate: Zone 1 + \$2.00)

BASE POINTS: Spokane, Pasco, Lewiston  
Zone 1: 0-45 radius miles from the main post office.  
Zone 2: Outside 45 radius miles from the main post office

#### TRUCK DRIVERS CLASSIFICATIONS

GROUP 1: Escort Driver or Pilot Car; Employee Haul; Power  
Boat Hauling Employees or Material

GROUP 2: Fish Truck; Flat Bed Truck; Fork Lift (3000 lbs. and  
under); Leverperson (loading trucks at bunkers); Trailer  
Mounted Hydro Seeder and Mulcher; Seeder & Mulcher;  
Stationary Fuel Operator; Tractor (small, rubber-tired,  
pulling trailer or similar equipment)

WA160001 Modification 0  
Federal Wage Determinations for Highway Construction

GROUP 3: Auto Crane (2000 lbs. capacity); Buggy Mobile & Similar; Bulk Cement Tanks & Spreader; Dumptor (6 yds. & under); Flat Bed Truck with Hydraulic System; Fork Lift (3001-16,000 lbs.); Fuel Truck Driver, Steamcleaner & Washer; Power Operated Sweeper; Rubber-tired Tunnel Jumbo; Scissors Truck; Slurry Truck Driver; Straddle Carrier (Ross, Hyster, & similar); Tireperson; Transit Mixers & Truck Hauling Concrete (3 yd. to & including 6 yds.); Trucks, side, end, bottom & articulated end dump (3 yards to and including 6 yds.); Warehouseperson (to include shipping & receiving); Wrecker & Tow Truck

GROUP 4: A-Frame; Burner, Cutter, & Welder; Service Greaser; Trucks, side, end, bottom & articulated end dump (over 6 yards to and including 12 yds.); Truck Mounted Hydro Seeder; Warehouseperson; Water Tank truck (0-8,000 gallons)

GROUP 5: Dumptor (over 6 yds.); Lowboy (50 tons & under); Self-loading Roll Off; Semi-Truck & Trailer; Tractor with Steer Trailer; Transit Mixers and Trucks Hauling Concrete (over 6 yds. to and including 10 yds.); Trucks, side, end, bottom and end dump (over 12 yds. to & including 20 yds.); Truck-Mounted Crane (with load bearing surface either mounted or pulled, up to 14 ton); Vacuum Truck (super sucker, guzzler, etc.)

GROUP 6: Flaherty Spreader Box Driver; Flowboys; Fork Lift (over 16,000 lbs.); Dumps (Semi-end); Mechanic (Field); Semi-end Dumps; Transfer Truck & Trailer; Transit Mixers & Trucks Hauling Concrete (over 10 yds. to & including 20 yds.); Trucks, side, end, bottom and articulated end dump (over 20 yds. to & including 40 yds.); Truck and Pup; Tournarocker, DWs & similar with 2 or more 4 wheel-power tractor with trailer, gallonage or yardage scale, whichever is greater Water Tank Truck (8,001- 14,000 gallons); Lowboy(over 50 tons)

GROUP 7: Oil Distributor Driver; Stringer Truck (cable operated trailer); Transit Mixers & Trucks Hauling Concrete (over 20 yds.); Truck, side, end, bottom end dump (over 40 yds. to & including 100 yds.); Truck Mounted Crane (with load bearing surface either mounted or pulled (16 through 25 tons);

GROUP 8: Prime Movers and Stinger Truck; Trucks, side, end, bottom and articulated end dump (over 100 yds.); Helicopter Pilot Hauling Employees or Materials

Footnote A - Anyone working on a HAZMAT job, where HAZMAT certification is required, shall be compensated as a premium, in additon to the classification working in as follows:

LEVEL C-D: - \$.50 PER HOUR (This is the lowest level of protection. This level may use an air purifying respirator or additional protective clothing.

LEVEL A-B: - \$1.00 PER HOUR (Uses supplied air in conjunction with a chemical splash suit or fully encapsulated suit with a self-contained breathing apparatus.

Employees shall be paid Hazmat pay in increments of four(4) and eight(8) hours.

NOTE:

Trucks Pulling Equipment Trailers: shall receive \$.15/hour over applicable truck rate

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WELDERS - Receive rate prescribed for craft performing operation to which welding is incidental.  
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Unlisted classifications needed for work not included within the scope of the classifications listed may be added after award only as provided in the labor standards contract clauses (29CFR 5.5 (a) (1) (ii)).  
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The body of each wage determination lists the classification and wage rates that have been found to be prevailing for the cited type(s) of construction in the area covered by the wage determination. The classifications are listed in alphabetical order of "identifiers" that indicate whether the particular rate is a union rate (current union negotiated rate for local), a survey rate (weighted average rate) or a union average rate (weighted union average rate).

Union Rate Identifiers

A four letter classification abbreviation identifier enclosed in dotted lines beginning with characters other than "SU" or "UAVG" denotes that the union classification and rate were prevailing for that classification in the survey. Example: PLUM0198-005 07/01/2014. PLUM is an abbreviation identifier of the union which prevailed in the survey for this classification, which in this example would be Plumbers. 0198 indicates the local union number or district council number where applicable, i.e., Plumbers Local 0198. The next number, 005 in the example, is an internal number used in processing the wage determination. 07/01/2014 is the effective date of the most current negotiated rate, which in this example is July 1, 2014.

Union prevailing wage rates are updated to reflect all rate changes in the collective bargaining agreement (CBA) governing this classification and rate.

### Survey Rate Identifiers

Classifications listed under the "SU" identifier indicate that no one rate prevailed for this classification in the survey and the published rate is derived by computing a weighted average rate based on all the rates reported in the survey for that classification. As this weighted average rate includes all rates reported in the survey, it may include both union and non-union rates. Example: SULA2012-007 5/13/2014. SU indicates the rates are survey rates based on a weighted average calculation of rates and are not majority rates. LA indicates the State of Louisiana. 2012 is the year of survey on which these classifications and rates are based. The next number, 007 in the example, is an internal number used in producing the wage determination. 5/13/2014 indicates the survey completion date for the classifications and rates under that identifier.

Survey wage rates are not updated and remain in effect until a new survey is conducted.

### Union Average Rate Identifiers

Classification(s) listed under the UAVG identifier indicate that no single majority rate prevailed for those classifications; however, 100% of the data reported for the classifications was union data. EXAMPLE: UAVG-OH-0010 08/29/2014. UAVG indicates that the rate is a weighted union average rate. OH indicates the state. The next number, 0010 in the example, is an internal number used in producing the wage determination. 08/29/2014 indicates the survey completion date for the classifications and rates under that identifier.

A UAVG rate will be updated once a year, usually in January of each year, to reflect a weighted average of the current negotiated/CBA rate of the union locals from which the rate is based.

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WAGE DETERMINATION APPEALS PROCESS

1.) Has there been an initial decision in the matter? This can be:

- \* an existing published wage determination
- \* a survey underlying a wage determination
- \* a Wage and Hour Division letter setting forth a position on a wage determination matter
- \* a conformance (additional classification and rate) ruling

On survey related matters, initial contact, including requests for summaries of surveys, should be with the Wage and Hour Regional Office for the area in which the survey was conducted because those Regional Offices have responsibility for the Davis-Bacon survey program. If the response from this initial contact is not satisfactory, then the process described in 2.) and 3.) should be followed.

With regard to any other matter not yet ripe for the formal process described here, initial contact should be with the Branch of Construction Wage Determinations. Write to:

Branch of Construction Wage Determinations  
Wage and Hour Division  
U.S. Department of Labor  
200 Constitution Avenue, N.W.  
Washington, DC 20210

2.) If the answer to the question in 1.) is yes, then an interested party (those affected by the action) can request review and reconsideration from the Wage and Hour Administrator (See 29 CFR Part 1.8 and 29 CFR Part 7). Write to:

Wage and Hour Administrator  
U.S. Department of Labor  
200 Constitution Avenue, N.W.  
Washington, DC 20210

The request should be accompanied by a full statement of the interested party's position and by any information (wage payment data, project description, area practice material, etc.) that the requestor considers relevant to the issue.

3.) If the decision of the Administrator is not favorable, an interested party may appeal directly to the Administrative Review Board (formerly the Wage Appeals Board). Write to:

Administrative Review Board  
U.S. Department of Labor  
200 Constitution Avenue, N.W.  
Washington, DC 20210

4.) All decisions by the Administrative Review Board are final.

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END OF GENERAL DECISION

## Appendix D — Contacts

This is the contact information known and available as of the date of Call for Bids. The Contractor is responsible for confirming these contacts, and adding any more as needed.

### City of Redmond

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City of Redmond  
Police Department  
8701 160th Avenue NE  
Redmond, Washington 98052  
Attn: Doug Krueger  
Phone: (425) 556-2570 (8am – 5pm)  
Email: dkrueger@redmond.gov

City of Redmond  
Police Department Flaggers  
Off Duty Police Officer Coordinator  
8701 160th Avenue NE  
Redmond, WA 98052  
Attn: Doug Krueger  
Phone: (425) 556-2570  
Email: dkrueger@redmond.gov

City of Redmond  
Fire Department  
15670 NE 85th Street  
MS 2SFD  
Redmond, Washington 98073  
Attn: Todd Short, Fire Marshall  
Phone: (425) 556-2242  
Email: tshort@redmond.gov

City of Redmond  
Redmond Traffic Signals  
18080 NE 76th Street  
Redmond, WA 98052  
Attn: John Schaefer  
Lead Signal Technician  
Phone: (425) 556-2830  
Email: jschaefer@redmond.gov

City of Redmond  
Street Maintenance Division  
Public Works  
18080 NE 76th Street  
Redmond, WA 98052  
Attn: Robin Brown  
Phone: (425) 556-2836  
Email: rbrown@redmond.gov

City of Redmond Utilities  
18080 NE 76th Street  
Redmond, WA 98052  
Attn: Rebecca Borker  
Maintenance and Operations Manager  
Phone: (425) 556-2706  
Email: rborker@redmond.gov

### Utilities

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Union Hill Water District  
5020 236th Avenue NE  
Redmond, WA 98053  
Attn: Don Erickson  
Phone: (425) 466-4289  
Email: Don@uhwa.org

King County Wastewater  
South Wastewater Treatment Plant  
Phone: 206-684-1280

Puget Sound Energy (Gas & Electric)  
Emergency Construction Contact  
Phone: 1-888-CALL-PSE

Puget Sound Energy  
355 110th Ave. NE EST-11W  
Bellevue, WA 98004  
Attn: Kelly Purnell  
Phone: (425) 462-3488  
Email: Kelly.purnell@pse.com

Frontier Communications  
Attn: Mike Hakahan  
Phone: (425) 263-4038  
Email: y.m.hakahan@ftr.com

Into Light  
Attn: Rawley Robins  
Sr. Technician  
Phone: (425) 456-2546  
Email: rawley.robins@pse.com

Comcast  
1525 75th Street SW, #200  
Everett, WA 98203  
Attn: Jeffrey Burris  
Phone: (425) 263-5353  
Email: Jeffrey\_Burris@cable.comcast.com

Underground Utilities Location Center  
("One-Call" Center)  
1-800-424-5555

#### Other

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Metro Transit  
Attn: Richard Garcia  
Construction Coordinator  
Phone: (206) 684-2732  
Email: [Construction.coord@kingcounty.gov](mailto:Construction.coord@kingcounty.gov)  
<http://www.kingcounty.gov/transportation/kcdot/MetroTransit/Construction.aspx>

Lake Washington School District  
Phone: (509) 710-8871

USPS  
Postmaster  
Attn: John Snider  
Route Master  
Phone: (425) 867-1066

Washington State Department of  
Transportation  
Traffic Impacts on King County Roads  
Attn: Juan Reyes  
Workzone Traffic Engineer  
Phone: (206) 440-4467  
Email: reyesj@wsdot.wa.gov

Pierce Transit (for Sound Transit)

Attn: Dixie Sciacqua

Phone: (253) 581-8101

Attn: Mark Davilla

Phone: (253) 377-5027

Attn: David Turissini (ST Bus Operations  
Mngr)

Email: david.turissini@soundtransit.org

Microsoft Connector

Attn: Aaron Edwards

Phone: (425) 538-7893

Email: v-aaroed@microsoft.com