



**TECHNICAL COMMITTEE REPORT
TO THE PLANNING COMMISSION**

To: Planning Commission

From: Technical Committee

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Date: June 13, 2014

Project File Number: LAND-2014-00055

Project Name: Southeast Redmond Neighborhood Plan Update

Related File Numbers: LAND-2014-00956 (SEPA)

Applicant: City of Redmond

Applicant's Representative: Same as staff contacts

Reason for Proposal Adoption: Please see the initial Technical Committee Report for this topic dated January 31, 2014 and available online at www.redmond.gov/common/pages/UserFile.aspx?fileId=116285.

I. APPLICANT PROPOSAL

The City of Redmond proposes to update the Southeast Redmond Neighborhood Plan, which is part of the Redmond Comprehensive Plan, and to adopt implementing zoning regulations. This Technical Committee Report mainly concerns staff-recommended zoning regulations to implement neighborhood plan policies that the Planning

Commission reviewed in February and March of this year. This report also makes a recommendation on a proposed rezone near the corner of Redmond Way and 185th Ave NE. The Technical Committee Report concerning policies is available in the project file (LAND-2014-00055) at Redmond City Hall and online at www.redmond.gov/common/pages/UserFile.aspx?fileId=116285.

BACKGROUND AND REASON FOR PROPOSAL:

This report represents the second phase of the Southeast Redmond Neighborhood Plan update and primarily includes staff-recommended zoning regulations. These proposed regulations respond to the Redmond Planning Commission’s previous preliminary recommendations for updates to Southeast Redmond Neighborhood Plan policies. In February and March 2014, the Planning Commission reviewed policies identified in the aforementioned Technical Committee Report (LAND-2014-00055). Staff has developed recommended regulations for implementing these policies and for addressing zoning changes in the Evan Creek, Marymoor, and Northeast subareas; performance-based zoning in the Marymoor and Northeast subareas; and affordable housing and design standards.

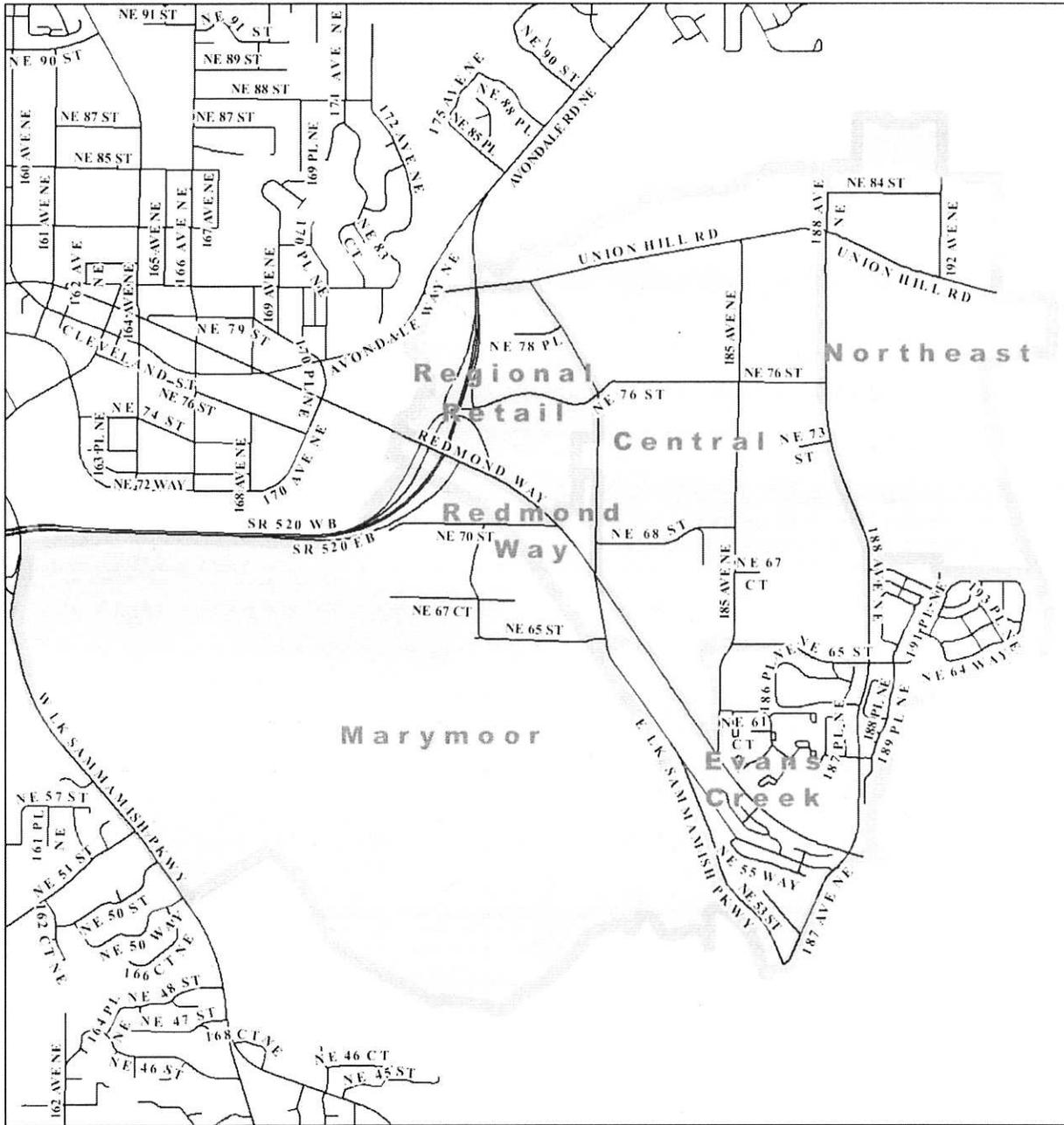
This phase of the Neighborhood Plan update also sets the stage for subsequent subarea and infrastructure planning in the Marymoor subarea and in association with future light rail and station area improvements. These subsequent planning processes will help refine regulations developed during this phase for the Marymoor subarea. Staff recommends adopting the regulations and zoning recommended for the subarea with the effective date delayed until January 1, 2018 to allow the completion of additional, site-specific subarea and infrastructure planning. These additional planning processes will address topics such as the subarea’s groundwater, transportation system, utilities, parks and development standards.

The City of Redmond undertakes neighborhood planning to maintain and enhance neighborhood vitality and quality of life for the long term. Specifically, neighborhood plans:

- Reflect the priorities of people who live, work and own property in the neighborhood.
- Describe important neighborhood goals and how to accomplish them.
- Give policy and regulatory direction for the neighborhood and City decision makers.
- Help to coordinate and indicate priorities for improvements in services and facilities.
- Are consistent with but do not duplicate the Comprehensive Plan.
- Help to implement the Comprehensive Plan.
- Serve as a tool to bring together those who live or work in an area to help address concerns and goals.

Comprehensive Plan policies NP-1 through NP-7 describe how Redmond plans for neighborhoods. Policy NP-1 directs the City to regularly review and update neighborhood plans, completing major updates every twelve years. The existing Southeast Redmond Neighborhood Plan dates from 1993, underscoring the timeliness for considering and acting on needed updates to this neighborhood plan. Current neighborhood policies, significant changes since the last plan update and the make-up of the Citizen Advisory Committee are detailed in the January 31, 2014 Technical Committee Report.

The following page contains a map of Southeast Redmond subareas.



Legend	 Neighborhood Subareas
	<p> MAP NE-SER-## SOUTHEAST REDMOND NEIGHBORHOOD SUBAREAS Effective: Month ##, 2014 Note: </p>



II. RECOMMENDATION

The Technical Committee recommends adopting updated Southeast Redmond Neighborhood Plan policies, regulations, land use map and zoning map as shown in the included exhibits to this document. At the policy level key elements of the updated plan include:

- Retaining land for manufacturing and industry and introducing flexibility through performance-based zoning at the subarea level;
- Recognizing the importance of Redmond's ground water and surface water and supporting subarea, infrastructure, and citywide plans and policy to protect them;
- Planning for new residential and employment opportunities in the area north of Woodbridge and in the Marymoor subarea (described further in this report);
- Improving mobility for all modes by planning for new motorized and non-motorized connections;
- Prioritizing transportation corridors for efficient heavy-duty truck travel and freight transit;
- Promoting manufacturing and distribution related transportation away from single-family residential areas;
- Supporting housing affordability and choice;
- Completing planned trail connections and developing existing parks;
- Enhancing a sense of place and community through opportunities for gathering places, particularly as part of private development and design of the public realm;
- Renaming the Gateway Design District to the Regional Retail Subarea while maintaining the same land use intent in neighborhood plan policies; and
- Maintaining current zoning along Redmond Way, preserving the limits of the Manufacturing Park Overlay (described further in this report).

Key elements of staff-recommended implementing regulations include:

- Creating new opportunities for employment uses in the Northeast Subarea north of the Woodbridge residential development while retaining the ability to construct about 150 new homes north of Woodbridge.
- Creating new opportunities for small-scale neighborhood-serving retail at the corner of 188th Ave NE and Southeast Redmond Park.
- Rezoning about 6 acres of land near the southwest corner of 185th Ave NE and Redmond Way from GC and R-12 to R-30 (30 homes per acre).
- Creating new opportunities for about 700 multi-family homes near Marymoor Park and south of NE 65th St to compensate for lost capacity in the Northeast Subarea. This would not include human services as an allowed land use.
- Extending affordable housing regulations to Southeast Redmond.

- Amending Outdoor Storage and Transition Overlay regulations to take account of new zoning designations.
- Other minor changes to the zoning code to accomplish what is described above.

III. PRIMARY ISSUES CONSIDERED

A. ISSUES CONSIDERED AND ALTERNATIVES

This section of the report describes key recommended amendments to the Redmond Zoning Code and alternatives that were considered for each.

Northeast Subarea

The CAC and Technical Committee recommend significant changes to planned land uses and zoning in the Northeast Subarea, focusing on creating a transition from single-family uses in the south to industrial uses in the north. Among other standards such as the amount of impervious surface and required percentage of onsite landscaping, the Technical Committee recommended varying building height for enhanced transition between uses and in relationship to the rural uses located to the east of the neighborhood.

The Planning Commission reviewed policies addressing changes to planned land uses in the Northeast Subarea during their February and March 2014 review of the first phase of the Southeast Redmond neighborhood plan update. These changes focus on transitioning from single-family residential uses in the southern portion of the Northeast Design District to business uses that feature business campus, manufacturing, and industrial activities in the mid- and northern portion of the Design District. The Northeast Design District comprises the area of the Northeast Subarea from the northern edge of Woodbridge to Union Hill Road with the exception of the designated Industrial zone. In addition, the planned land use changes set the stage for relocating a majority of the currently planned housing capacity to the Marymoor Subarea, near the future East Link light rail station. The Planning Commission indicated preliminary support for those changes.

Zoning regulations recommended for the Northeast Subarea capitalize on performance-based zoning that supports the potential for design and use flexibility within three performance areas: PA-1, 2 and 3. The Technical Committee recommends creating these within the Northeast Design District, as follows and as shown in Exhibit B.

The PA-1 zone, located east of the planned alignment for 192nd Avenue NE and adjacent to the northern edge of Woodbridge, includes capacity for 140 to 170 residential units. Staff-recommended performance standards are summarized in the table below.

		Base	Maximum with incentives
Maximum	Height	2 stories	4 stories
	FAR	0.50	0.55
	Impervious surface area	70%	Same
Minimum	Setbacks	4-15 feet (see note)	Same
	Landscaping	40%	Same

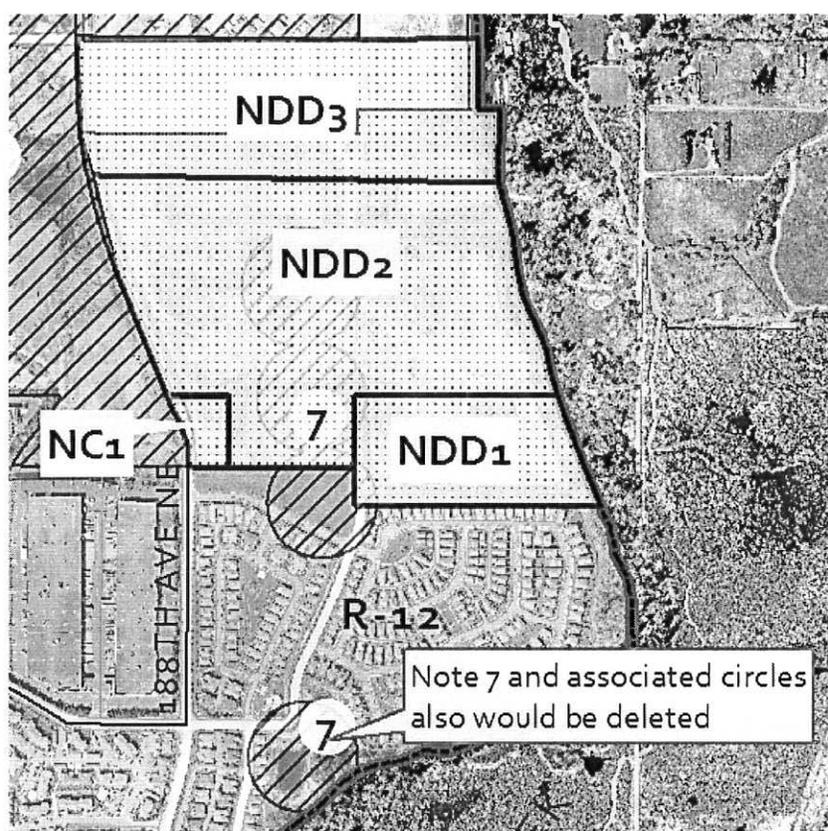
Note: All other setbacks within PA-1 would be determined through a required master planning process.

The PA-2 and 3 zones provide for business uses and identify those with lesser intensity as suitable in the southern portion of PA-2 and those with more intensive activities such as involving outdoor storage, Class C noise, and heavy-duty traffic as suitable in the northern portion of PA-2 and in PA-3. The recommended setback and street design standards work in tandem with other zoning code such as noise standards to focus impacts that would otherwise detract from residential character and quality of life to the mid- and northern portion of Northeast Design District. The table below summarizes staff-recommended performance standards for PA-2 and PA-3.

		Base	Maximum with incentives
Maximum	Height	2 stories	5 stories
	FAR	N/A	N/A
	Impervious surface area	70%	Same
Minimum	Setbacks	30-250 feet (see note)	Same
	Landscaping	40%	Same

Note: All other setbacks within PA-2 and PA-3 would be determined through a required master planning process.

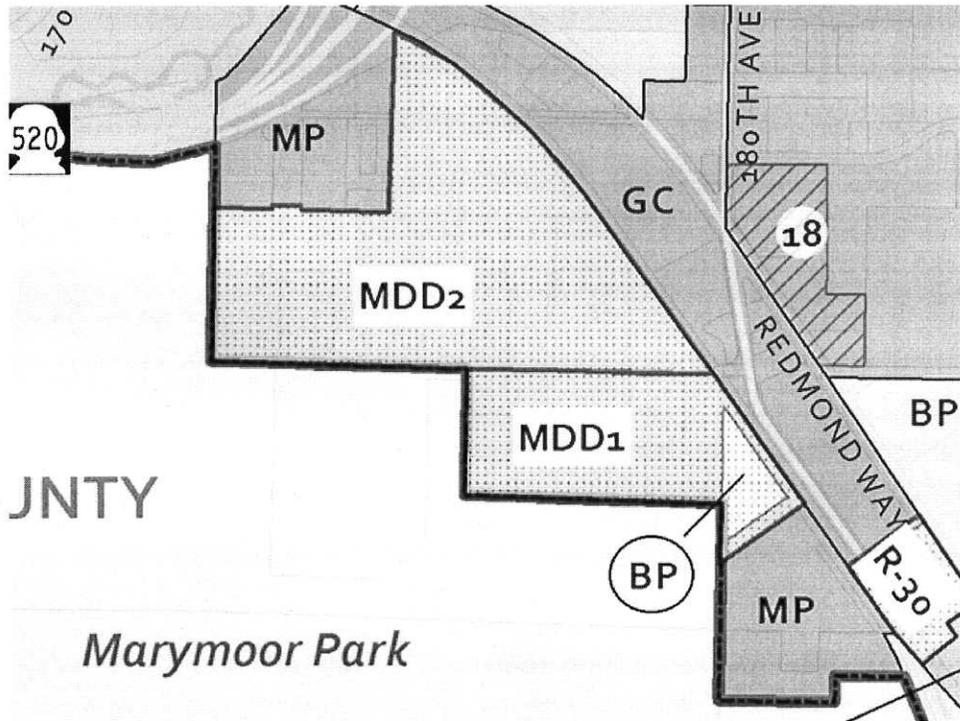
The Northeast Subarea is also recommended to include one location for Neighborhood Commercial zoning (NC-1). Located adjacent to Southeast Redmond Neighborhood Park and within close proximity to residential and employment areas, this site would provide local access to daily goods and services and would be limited to one acre of commercial use. Establishing this zone would remove three overlay alternatives for siting Neighborhood Commercial in the neighborhood by fixing the land use and zone as a designated area on the City's Comprehensive Land Use map and Zoning map as shown below. As well, this zoning designation would be within Neighborhood Commercial Overlay Zone Area #6, identified on Comprehensive Plan Map LU-2.



Marymoor Subarea

In February and March 2014 the Planning Commission reviewed policies addressing significant changes to planned land uses in the Marymoor Subarea, focusing on economic resiliency, preparing for light rail transit and creating new housing opportunities near Marymoor Park and the light rail station to offset loss of housing capacity proposed in the Northeast Subarea. The Planning Commission indicated preliminary support for those changes.

Staff is recommending zoning regulations for the Marymoor Design District (MDD) that incorporate elements of performance zoning. Consistent with recommended neighborhood plan policies, the Technical Committee recommends creating two “performance areas” within the MDD, known as MDD1 and MDD2. MDD1 is the southern portion of the design district while MDD2 is the northern portion (see map below).



The MDD1 zone is intended primarily for multi-family residential development while making provisions for parks and open spaces, family day care providers, home businesses, and roadside produce stands. Staff-recommended draft performance standards are summarized in the table below.

		Base	Maximum with incentives
Maximum	Height	4 stories	5 stories
	FAR	0.90	1.35
	Lot coverage by structures	55%	Same
	Impervious surface area	70%	75%
Minimum	Setbacks	4-15 feet	Same
	Landscaping	40%	Same

Another performance zoning element is the openness in the types of housing permitted. Rather than regulate structure type or density the Technical Committee is recommending an FAR approach where the total amount of development is regulated and developers build according to what the market demands and the site can accommodate.

The MDD includes a recommended incentive program in order to help achieve neighborhood objectives. In MDD1, increasing a development site’s ecological score, incorporating community gardens, increasing the amount of affordable housing provided, and reducing total impervious surface are techniques that could be used to obtain additional height, FAR and impervious surface area. Impervious surface area could be increased via the incentive program (to 75%) and is encouraged to be decreased (to 60%) at the same time. It will be up to each developer to determine which direction to go based on the desired type of development and each site’s characteristics.

The staff-recommended MDD2 zone continues to emphasize manufacturing and wholesale trade and research and development (as under current MP zoning), but also would make limited allowance for general sales and service, arts, and other uses recognizing that this area is expected to undergo significant change over time. The use standards are paired with the proposed performance standards in the table below.

		Base	Maximum with incentives
Maximum	Height	2 stories	5 stories
	FAR	0.50	1.0
	Lot coverage by structures	55%	Same
	Impervious surface area	70%	75%
Minimum	Setbacks	10 feet	Same
	Landscaping	30%	Same

In MDD2, incorporating structured parking, art (or an in lieu contribution), a gathering place or park, or reducing impervious surface area are proposed as techniques that can be used to obtain additional height or impervious surface area.

Proposed development intensities for MDD2 are lower than for MDD1 for the time being. The Technical Committee expects to revisit this area, including allowed uses and development intensity, as part of station area

planning that is called-for in neighborhood policies, and as part of additional infrastructure planning, as described below.

One of the policies that the Commission supported speaks to an infrastructure planning study to better understand infrastructure needs in the Marymoor Subarea to support planned land uses. Such a study is to address utilities, parks, transportation and land use, including considering a minimum residential FAR to ensure housing capacity is absorbed over time. Through the course of drafting implementing regulations it became clear that a phased implementation approach would best serve neighborhood plan objectives. In particular, the Marymoor Subarea faces several area-specific challenges. The first is that the area lacks a stormwater outfall, which means that, unless conditions change, all stormwater must be infiltrated. A related challenge is a high groundwater table, in some areas just 5.5 feet below the ground. This also impacts how stormwater is managed and how buildings are designed. The Technical Committee believes that a subarea stormwater strategy should be developed before allowing proposed zoning regulations to become effective.

Additionally, the Marymoor Subarea faces a traffic access challenge, given the limited existing access points into the neighborhood, and the need to develop a new neighborhood sewer and water plan, consistent with the proposed zoning regulations. The Technical Committee believes it is important for transportation access to be addressed and these new water and sewer plans to be developed before allowing proposed zoning regulations to become effective.

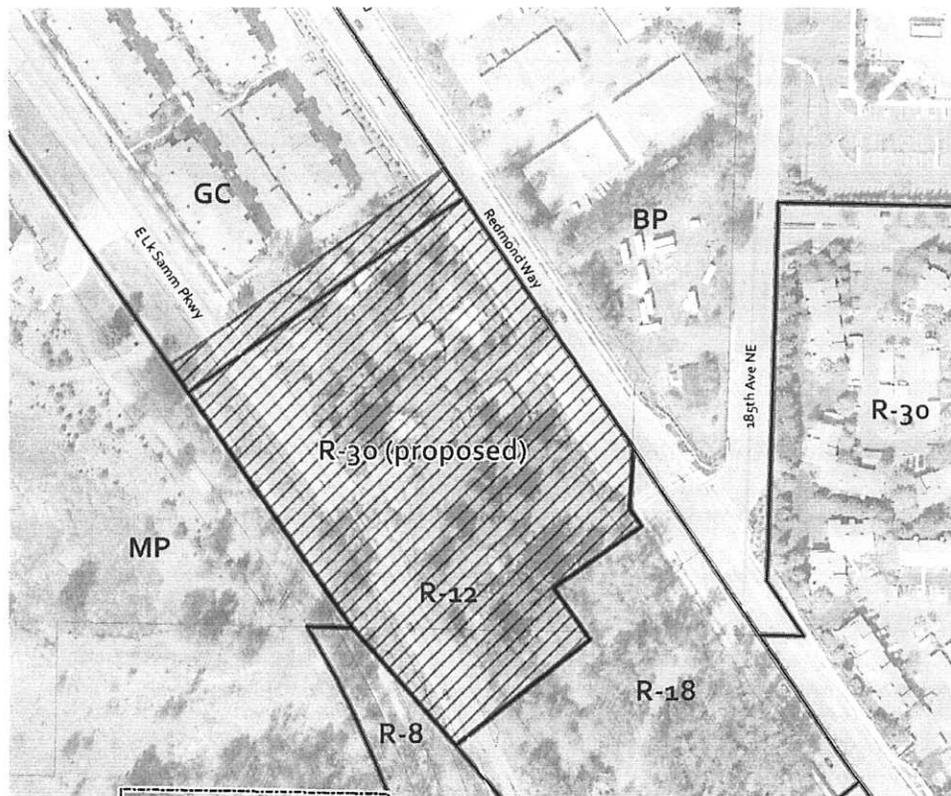
At the same time, the Technical Committee believes it is important to set property owner expectations and express commitment to the neighborhood plan by adopting Marymoor Design District regulations that would take effect at a date certain. During the interim the City would complete an infrastructure planning study. Should the study suggest the need for revisions to the regulations then City staff would propose such revisions for Planning Commission and City Council review before the date that the Marymoor Design District regulations would take effect.

The Technical Committee considered two other alternatives: implementing regulations immediately, as would be typical, and delaying all subarea-specific regulations. The risk associated with implementing regulations immediately is that there is a chance that the infrastructure planning work would uncover significant challenges that would result in significant changes to regulations. Staff wants to provide certainty to property owners rather than implement zoning that may be difficult for property owners to use and staff to administer. The risk associated with

delaying all subarea-specific regulations is that it causes plan implementation to lose momentum. The community expressed a clear future vision and staff believes it is important to begin to carry-out that vision as much as possible as part of this process.

Redmond Way Rezone from R-12 and GC to R-30

Earlier this year multiple property owners along Redmond Way indicated an interest in rezoning their properties from GC and R-12 to a zone that would allow additional density. Because the Southeast Redmond Neighborhood Plan Update was ongoing staff agreed to consider these requests as part of the update. The properties are shown in the graphic below.



Together the properties and adjacent right-of-way together measure about 6.0 acres. Of the non-right-of-way area, 3.4 acres is currently zoned R-12 and 0.3 acres is zoned General Commercial (GC). Each of the six lots contains one single-family home. The properties are located immediately southeast of the Reflections at Marymoor apartment community and immediately northwest of the Deer Creek Condominium project that is currently under construction.

Most of the subject area is already designated Multi-family Urban. The one parcel zoned GC is a remnant piece of General Commercial that was

not developed as part of the Reflections at Marymoor development in 2003. Its viability as General Commercial is questionable given its size and shape and the recommended policies in the Southeast Redmond Neighborhood Plan Update describe the Evans Creek Subarea, of which these properties are a part, as a mainly residential area.

The Technical Committee recommends re-designating the General Commercial property to Multi-family Urban, and rezoning all of the subject area to R-30. The proposed land use and zoning designations are consistent with the vision for Redmond and citywide land use policies. Giving the parcels the same land use designation and zoning will allow them to be redeveloped together. The proposal would accommodate approximately 60 additional homes compared to the existing condition, which would help offset the loss of capacity for 700 homes in the Northeast Subarea. Adjacent properties are developed at 34 and 16 units per acre.

The Technical Committee also considered not rezoning these properties or rezoning them to R-18 or R-20. As noted above, the GC parcel is a remnant and would be challenging to develop on its own. Furthermore it is in the Evans Creek Subarea, which policies describe as mainly a residential area. Therefore the Technical Committee believed it was appropriate to change the designation for that parcel to Multi-Family Urban.

The Technical Committee considered citywide objectives and the specific situation of these properties in developing a zoning recommendation. Citywide policy calls for making a variety of housing opportunities available in Redmond and maintaining a no-net-loss of housing. These properties can accommodate multi-family housing and as noted are adjacent to other multi-family developments developed at 16 to 34 units per acre. In addition the site is served by transit along Redmond Way. Therefore, zoning the properties R-30 is in keeping with the surrounding context and helps achieve the City's no-net-loss policy.

B. COMPARISON WITH OTHER COMMUNITIES, APPROACHES (as applicable)

Please see the January 31, 2014 Technical Committee Report for discussion of creating effective transitions and alternative zoning approaches.

IV. SUPPORTING ANALYSIS

A. EXISTING CONDITIONS

Please see the January 31, 2014 Technical Committee Report for a discussion on existing conditions in Southeast Redmond.

B. COMPLIANCE WITH CRITERIA FOR PLAN AMENDMENTS

Redmond Comprehensive Plan Policies PI-16 and LU-26 direct the City to take several considerations, as applicable, into account as part of decisions on proposed amendments to the Comprehensive Plan.

Items 1 through 6 apply to all proposed amendments. Items 7 through 9 apply when proposed amendments concern allowed land uses or densities, such as proposed amendments to the Land Use Plan Map, land use designations, allowed land uses, or zoning map.

The following is an analysis of how this proposal complies with the requirements for amendments.

1. Consistency with Growth Management Act (GMA), State of Washington Department of Commerce Procedural Criteria, VISION 2040 or its successor, and the King County Countywide Planning Policies.

Recommended policies and regulations are consistent with the above documents as described in the January 31, 2014 Technical Committee Report.

2. Consistency with Redmond's Comprehensive Plan, including the following sections as applicable:

a. Consistency with the goals contained in the Goals, Vision and Framework Policy Element.

Recommended policies and regulations are consistent with such goals as described in the January 31, 2014 Technical Committee Report.

b. Consistency with the preferred land use pattern as described in the Land Use Element.

Recommended policies and regulations are consistent with the preferred land use pattern as described in the January 31, 2014 Technical Committee Report.

c. Consistency with Redmond’s community character objectives as described in the Community Character/Historic Preservation Element or elsewhere in the Comprehensive Plan.

Recommended policies and regulations are consistent with community character objectives as described in the January 31, 2014 Technical Committee Report.

d. Consistency with other sections as applicable including the Natural Environment; Economic Vitality; Transportation; Parks, Arts, Recreation, Culture and Conservation Elements.

Recommended policies and regulations are consistent with other sections of the Comprehensive Plan as described in the January 31, 2014 Technical Committee Report.

3. Potential general impacts to the natural environment, such as impacts to critical areas and other natural resources, including whether development will be directed away from environmentally critical areas and other natural resources.

Redmond’s citywide policies and regulations address this topic at a citywide level. The Southeast Redmond plan focuses on greening the neighborhood and maintaining and enhancing the quality of ground water and surface water. The general character of this neighborhood has been established over many years with a majority of the development in the Central subarea taking place during the 1970s and 1980s. Today, Redmond’s Zoning Code and specifically design standards provide for site and right of way design that incorporates landscaping for aesthetics, visual screening, and as part of stormwater systems. These updated standards and codes will introduce additional natural resources to this neighborhood over time as part of new public and private investment.

In addition, the recommended updates to neighborhood regulations would enhance the capacity for beneficial vegetation such as part of low impact development, green building, ecological scores, and site design. In the Northeast Subarea, landscaping and setbacks would support transitions between land uses. In the Marymoor Subarea a subsequent infrastructure and subarea planning process will further address regulations and standards in relationship to stormwater and the City’s aquifer.

4. Potential general impacts to the capacity of public facilities and services. For land use related amendments, whether public facilities and services can be provided cost-effectively and adequately at the proposed density/intensity.

This criterion is generally addressed in the January 31, 2014 Technical Committee Report.

Specific to the rezone proposed near Redmond Way and 185th Ave NE, that area is already served with transportation and utility infrastructure because it is infill development. The developers will be responsible for site-specific infrastructure.

5. Potential general economic impacts, such as impacts for business, residents, property owners, or City Government.

Recommended changes to land use and zoning will have both positive and negative impacts on local area businesses. In the Northeast Subarea, a change from residential zoning to the Northeast Design District and Performance Areas 2 and 3 would provide additional opportunity for relocating and for establishing new businesses. This Design District includes flexibility through performance-based zoning that would support a business adapting to its needs and growing within the same location. The reduction of residential density in this Subarea would be accommodated in the Marymoor Subarea, nearer to the future East Link light rail station as well as in close proximity to the Redmond Central Connector and East Lake Sammamish Trail, local and regional trail networks. Siting the residential capacity in this Subarea would impact businesses currently operating within the existing Manufacturing zone over time existing structures reach the end of their economic lives and are replaced with residential uses. Neighborhood policy calls for economic development assistance for supporting these businesses as they transition to places in the neighborhood or in other part of the City.

6. For issues that have been considered within the last four annual updates, whether there has been a change in circumstances that makes the proposed amendment appropriate or whether the amendment is needed to remedy a mistake.

The topics addressed in the Southeast Redmond Neighborhood Plan have not been addressed in the last four years.

The following items apply when proposed amendments concern allowed land uses or densities, such as proposed amendments to the Land Use Plan Map, land use designations, allowed land uses, or zoning map.

7. General suitability of the area for the proposed land use or density, taking into account considerations such as adjacent land uses and the

surrounding development pattern, and the zoning standards under the potential zoning classifications.

This criterion is addressed at a policy level in the January 31, 2014 Technical Committee Report. It is addressed at a regulatory level below.

Northeast Subarea

The proposed zoning standards for the Northeast Design District implement recommended neighborhood policies. The Technical Committee recommends maintaining capacity for 140 to 170 residential units north of Woodbridge and east of the alignment for 192nd Avenue NE. Development of additional and limited residential capacity would continue to provide for housing that supports the City's goal of balancing jobs and housing supplies while siting the use in an area away from existing and future higher intensity business uses such as industrial and manufacturing.

To transition from the residential uses to the industrial uses along Union Hill Road, the regulations for the Design District require a master planning process whereby uses, site design, and infrastructure work together to support adjacent uses in their own context. For example, uses sited adjacent to residential zones would be set back a greater distance, would incorporate vegetated screening, and would adhere to a noise standard compatible with residential zones. As well, in the northern portion of the Design District, street design for 192nd Avenue NE would accommodate layovers of large and heavy-duty vehicles thereby lessening impacts of idling and center lane impacts near and in residential areas.

Marymoor Subarea

The proposed zoning standards for the Marymoor Design District implement recommended neighborhood policies. The transportation infrastructure necessary to support proposed land uses at proposed densities does not exist today. The Technical Committee has proposed, and the Planning Commission has reviewed, a neighborhood connections map that outlines the transportation improvements that would need to take place over time in order to accommodate planned uses and intensities.

Stormwater generally infiltrates in the Marymoor Subarea today. This may also be the best strategy for the future, but the overall topic of a subarea stormwater strategy is something the Technical Committee believes should be explored before putting proposed zoning standards into effect.

Redmond Way Rezone from R-12 and GC to R-30

The area proposed for a rezone is within the mainly residential Evans Creek portion of Southeast Redmond, and more specifically is adjacent to multiple multi-family developments.

8. Whether the proposed land use designation, zoning, or uses are compatible with nearby land use designations, zoning or uses. Whether there are opportunities to achieve compatibility with surrounding land uses through design or through separation by topography or buffers.

This criterion is addressed at a policy level in the January 31, 2014 Technical Committee Report. It is addressed at a regulatory level below.

Northeast Subarea

Recommendations for the Northeast Design District specifically call for land uses and zoning that supports transitions between uses of different intensities. Siting lower intensity uses such as residential in the southeast of the Design District would help prevent conflicts regarding higher intensity business uses such as industrial and manufacturing.

Transitional uses would gradually increase in intensity from residential uses sited adjacent to the same uses, park and neighborhood commercial use sited between residential and business uses, business park and campus oriented business sited with a larger setback near residential uses, and heavier intensity uses such as manufacturing sited between business uses and industrial uses. Vegetated screening, setbacks, height limitations, and vegetated non-motorized connections would also support the transitional effect between the various land use and zoning designations.

Marymoor Subarea

Over the years the Marymoor Design District (MDD) will be home to a more diverse mix of uses. The Technical Committee recommends focusing multi-family housing in the southern portion of the MDD with the option to add housing capacity near the future light rail station in the future. To aid in compatibility the code includes minimum landscaping and ecological score requirements – adding green to the neighborhood, some limitations on nighttime business activities near homes, and design requirements to create a permeable edge at the boundary with Marymoor Park.

Redmond Way Rezone from R-12 and GC to R-30

The proposed rezone would add an R-30 zone that is adjacent to R-18, GC (essentially developed as R-30), MP, BP, and R-8. Other R-20 and R-30 zones are also nearby. The land is already zoned primarily for multi-

family housing and this action would increase the density at which that housing could develop.

- 9. If the amendment proposes a change in allowed uses or densities in an area:**
- a. The need and demand for the land uses that would be allowed and whether the change would result in the loss of capacity to accommodate other needed land uses, especially whether the proposed amendment complies with policy HO-17, the City's policy of no net loss of housing capacity;**

Though a portion of residential capacity will transfer between neighborhood subareas, the recommended plan maintains and provides for anticipated residential growth, recognizing that further study and investment will be required to realize housing growth in the Marymoor Subarea. A variety of housing types and levels of affordability will be established in the Marymoor and Northeast subareas.

The transfer of residential capacity allows for additional employment capacity in the Northeast subarea. Within the Marymoor subarea, the receiving area will allow for a denser form of residential design and permit an eventual increase in employment capacity as well. Overall, the recommended plan takes into account current and future demand for land use, particularly in the sense that many portions of the neighborhood are presently underdeveloped based on current zoning provisions.

The Technical Committee also recommends delaying the effectiveness of the land use and zoning changes to the Marymoor Subarea. Though residential capacity would be reduced by 700 units in the Northeast Subarea, the concurrent change to accommodate this capacity in the Marymoor Subarea would be delayed until a date certain so that a subsequent subarea and infrastructure planning process for the Marymoor Subarea can be completed. This planning process will further refine regulations specific to stormwater and other utilities, the City's aquifer and local high groundwater, transportation systems, site and low impact design standards, parks, open space, and community gathering places. Several aspects of the subsequent planning process would incorporate information regarding the East Link light rail station as its alignment and station area planning continues to emerge.

b. Implications of the proposed amendment for the balance between the amount and type of employment in Redmond and the amount and type of housing in Redmond.

The zoning proposal adds about 36 acres of land for jobs, with a small amount of that space possible for general sales and service uses. The balance would be in a variety of other sectors including manufacturing, research and development and communication. The same amount of land is lost for housing while total housing unit capacity is positive, recognizing that further study and investment will be required to realize housing growth in the Marymoor Subarea. Currently the neighborhood has capacity for about 850 homes at 12 units per acre (townhomes). The proposed neighborhood plan keeps capacity for 150 homes at this density and shifts the remaining capacity to land that would accommodate about 30 units per acre. The rezone along Redmond Way adds about 60 housing units of capacity, which creates a cushion in case developers choose to build fewer, larger units in the Marymoor Subarea.

C. RELATIONSHIP TO PENDING AMENDMENTS IN THE 2013-14 COMPREHENSIVE PLAN AMENDMENT PACKAGE.

Please see the January 31, 2014 Technical Committee Report for discussion on this item.

V. AUTHORITY AND ENVIRONMENTAL, PUBLIC AND AGENCY REVIEW

A. AMENDMENT PROCESS

RCDG Sections 21.76.070.AE and 21.76.050.K require that amendments to the Comprehensive Plan or Zoning Code (except zoning map amendments consistent with the Comprehensive Plan) be reviewed under the Type VI process. Under this process, the Planning Commission conducts a study session(s), an open record hearing(s) on the proposed amendment, and makes a recommendation to the City Council. The City Council is the decision-making body for this process.

B. SUBJECT MATTER JURISDICTION

The Redmond Planning Commission and the Redmond City Council have subject matter jurisdiction to hear and decide whether to adopt the proposed amendment.

C. WASHINGTON STATE ENVIRONMENTAL POLICY ACT (SEPA)

A SEPA threshold determination was issued on June 11, 2014 and is attached as Exhibit G.

D. 60-DAY STATE AGENCY REVIEW

State agencies were sent 60-day notice of this proposed amendment on June 13, 2014.

E. PUBLIC INVOLVEMENT

The public has opportunities to comment on the proposed amendment through the Planning Commission review process and public hearing. A public hearing is scheduled for July 9, 2014. The hearing noticed is attached as Exhibit H.

During development of the recommended plan amendment, the neighborhood, stakeholders, and general public have had significant and a variety of opportunities to provide feedback and input regarding all aspects of the plan. A comprehensive summary of neighborhood, stakeholder and public engagement was provided in the January 31, 2014 Technical Committee Report on this topic. During development of the regulations staff consulted a number of development experts regarding zoning concepts including the proposed performance standards.

F. APPEALS

RZC 21.76.070.AE identifies Zoning Code Amendments as a Type VI permit. Final action is held by the City Council. The action of the City Council on a Type VI proposal may be appealed by filing a petition with the Growth Management Hearing Board pursuant to the requirements.

VI. LIST OF EXHIBITS

- Exhibit A: Recommended Zoning Code Amendments
 - Exhibit A1 RZC 21.08, Residential Regulations
 - Exhibit A2: RZC 21.14.010, Neighborhood Commercial 1
 - Exhibit A3: RZC 21.14.XXX, Marymoor Design District
 - Exhibit A4: RZC 21.14.XXX, Northeast Design District
 - Exhibit A5: RZC 21.20, Affordable Housing
 - Exhibit A6: RZC 21.32, Landscaping
 - Exhibit A7: RZC 21.38, Outdoor Storage, Retail Display, and Garbage and Recycling Enclosures

- Exhibit A8: RZC 21.50, Transition Overlay Areas
- Exhibit A9: RZC 21.60, Citywide Design Standards
- Exhibit A10: RZC 21.76.070.P, Master Planned Developments
- Exhibit B: Recommended Zoning Map
- Exhibit C: Recommended Southeast Redmond Neighborhood Plan Policies
- Exhibit D: Recommended Comprehensive Plan Land Use Map
- Exhibit E: Policy Issues Matrix as of March 19, 2014
- Exhibit F: SEPA Threshold Determination
- Exhibit G: Public Hearing Notice for July 9, 2014 Hearing

Additional resource: Technical Committee Report dated January 31, 2014, available in the project file (LAND-2014-00055) at Redmond City Hall and online at <http://www.redmond.gov/common/pages/UserFile.aspx?fileId=116285>.

Conclusion in Support of Recommendation: The Technical Committee has found the proposal to be in compliance with the Redmond Zoning Code, Redmond Comprehensive Plan, Redmond Municipal Code, and State Environmental Policy Act (SEPA).


ROBERT G. ODLE,
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Exhibit A: Recommended Zoning Code Amendments

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PLEASE NOTE

- All recommended changes are shown in track changes **except** Exhibits A3 and A4, which are completely new
- All recommended regulations for the Marymoor Design District are proposed to take effect on January 1, 2018

RZC 21.08 RESIDENTIAL REGULATIONS

No change to 21.08.010 through 21.08.160.

21.08.170 Site Requirements for Residential Zones

No change to 21.08.170.A through 21.08.170.D.

E. Minimum Average Lot Size.

1. Purpose. The purpose of the average lot size requirement is to:
 - a. Allow for the development of consistent and compatible land use patterns throughout Redmond's residential neighborhoods; and
 - b. Minimize public nuisances that may result from a lack of adequate open space and the over utilization of public facilities.
2. Requirements.
 - a. Explanation. The zone use charts in RZC [21.08.020](#) through [21.08.140](#) establish the minimum average lot size for each residential zone in Redmond. The average lot size of all lots created through the [subdivision](#) or [short subdivision](#) process must meet, at a minimum, this average lot size requirement. However, this requirement may be modified under the following circumstances:
 - i. Green Building and Green Infrastructure Program. The owner may participate in the Green Building and Green Infrastructure Incentive Program (see [RZC 21.67](#)), and create a lot or lots which do not meet the minimum average lot size for the underlying zone by meeting all program requirements; or
 - ii. Small Lot [Short Plats](#). The owner of any lot in the Bear Creek, Education Hill, Idylwood, [Southeast Redmond](#) or Overlake Residential Neighborhoods which is at least 200 percent of the required minimum average lot size in the underlying residential zone and which contains an existing detached dwelling unit may short subdivide the lot in order to create a separate fee simple lot which does not meet the minimum average lot size for the underlying zone if the dwelling unit to be constructed on the newly created lot meets all of the following requirements:
 - a. Only one detached dwelling unit shall be allowed on the lot.
 - b. The dwelling unit on the lot shall not exceed 1,000 square feet in total area, excluding any garage area. The dwelling unit and any garage shall not exceed 1,500 square feet in total area. A covenant shall be recorded against the title of the lot prohibiting expansion of the dwelling unit.

- c. The dwelling unit on the lot must conform to all setback, lot coverage restrictions and any other standards or regulations required of a detached dwelling unit in a residential zone.
 - d. The maximum height of any portion of the roof, except chimneys or cupolas shall not exceed 25 feet anywhere on the site.
 - e. Two off-street parking places are required. Parking spaces must be paved and may include private attached garages, carports, or other off-street areas reserved for **vehicles**. No detached garages are allowed.
 - f. The dwelling unit must be affordable to an **individual** or **family** that has an annual income that is 120 percent or less of the annual median income defined in **RZC 21.20**, Affordable Housing. (Ord. 2642)
- b. Limitations on Averaging.
- i. No lot shall be created as a result of lot averaging that results in a lot size that is less than 50 percent of the average lot size standard. For example, with an average lot size of 7,000 square feet in an R-4 zone, no single lot in a proposed subdivision in this zone may be sized at less than 3,500 square feet. For short subdivisions where three or fewer lots are created, no lot shall be created that is less than 75 percent of the average lot size standard.
 - ii. The following critical areas and shoreline areas shall not be included in the average lot size determination for all residential zones: Landslide Hazard Areas; Category I wetlands and their buffers; Class I streams and their buffers; the area waterward of the line of the ordinary high water mark on Lake Sammamish, regardless of the extent of ownership; and floodway areas.
 - iii. Nothing in this section shall be construed to allow for an increase in the allowed density as calculated in RZC 21.08.170.C.3 and as shown for all residential zones in the zone use charts in RZC **21.08.020** through **21.08.140**.
- c. Areas of Lots with **Access Corridor**. The calculation of lot area shall not include any area of the lot that serves as an access corridor.

No change to remainder of 21.08.170; no changes to 21.08.180 through 21.08.250.

21.08.260 Attached Dwelling Units

- A. **Purpose.** The purpose of this section is to:
- 1. Enhance opportunities for ownership housing;
 - 2. Reduce **development** costs related to **construction** and the provision of utilities which in turn may help to reduce housing prices in support of affordability goals;

3. More effectively set aside [critical areas](#) and natural resources as [open space](#) than would otherwise be allowed through the [subdivision](#) process; and
4. Achieve the planned density for a [site](#) that may not otherwise be met due to environmental and other physical constraints.

B. Applicability.

1. Generally. Unless otherwise specified in subsections B.2 and B.3 of this section, [attached dwelling units](#) are allowed through a conditional use permit process in zones R-4 through R-6. Attached [dwelling units](#) are allowed outright in zones R-8 through R-20, and three-unit attached dwelling units and four-unit attached dwelling units are allowed outright in the R-30 zone unless otherwise provided in subsections B.2 and B.3.
2. Willows/Rose Hill Neighborhood.
 - a. Two-unit attached dwelling units are an allowed use on individual [lots](#) in Single-Family Urban zones in the Willows/Rose Hill Neighborhood, provided, that a minimum of 70 percent of the total dwelling units within the single-family portion of each residential subarea of the Willows/Rose Hill Neighborhood shall remain detached single-family dwellings.
 - b. Three-unit attached dwelling units and four-unit attached dwelling units may be allowed as part of a preliminary plat application in Single-Family Urban zones only as part of the demonstration project provided for in Policy N-WR-E-4 of the Redmond [Comprehensive Plan](#) to evaluate compatibility with the Willows/Rose Hill Neighborhood.
3. Bear Creek, Education Hill, Grass Lawn, North Redmond, [Southeast Redmond](#) and Overlake Neighborhoods.
 - a. Two-unit attached dwelling units are an allowed use on individual lots in Single-Family Urban zones.
 - b. Three-unit attached dwelling units and four-unit attached dwelling units are allowed on individual lots in Single-Family Urban zones, with public notification and at least one neighborhood meeting required. The public notification and neighborhood meeting is not required in R-8 zones.

C. Requirements. Attached dwelling units are subject to all of the land use, density, site requirements and development standards of the underlying zone with the following exceptions:

1. Minimum Lot Size. The minimum lot size for attached dwelling units in R-4, R-5 and R-6 zones shall be based on a percentage of the average lot size of the underlying zone as presented in the zone use chart for the residential zone. (See RZC [21.08.020](#) through [21.08.140](#).)

- a. The minimum lot size for a two-unit attached dwelling unit is equal to 150 percent of the average lot size for the underlying zone.
 - b. The minimum lot size for a three-unit attached dwelling unit is equal to 200 percent of the average lot size for the underlying zone.
 - c. The minimum lot size for a four-unit attached dwelling unit is equal to 250 percent of the average lot size for the underlying zone.
2. Lot Division.
- a. For ground-oriented, side-by-side attached dwelling units, a single lot that meets the minimum lot size requirement of this section may be divided into separate lots and ownerships as part of the approval process. If separate lots are created, interior side [setback](#) standards no longer apply.
 - b. Where [structures](#) are built over [property lines](#), or property lines are created which divide structures, and ownership is or can be divided, the entire structure shall meet the requirements of the City's RMC Title 15, [Buildings](#) and Construction, based on the gross square footage of the structure before division and not based on the square footage of the individual units after division. A perpetual joint ownership and management agreement shall be created to manage contracts for the monitoring, maintenance, and emergency repair service for all fire protection systems for the entire structure.
3. Density.
- a. Bear Creek, Education Hill, Grass Lawn, North Redmond [and Southeast Redmond](#) Neighborhoods.
 - i. The allowed number of dwelling units for two-unit attached dwelling units shall be determined solely by the minimum lot size and lot division provisions of subsections C.1 and C.2 of this section.
 - ii. The allowed number of dwelling units for three-unit attached dwelling units and four-unit attached dwelling units shall not exceed the allowed number of detached single-family dwelling units, exclusive of any other bonuses.
 - b. All Other Areas. The allowed number of dwelling units for two-unit, three-unit, and four-unit attached dwelling units shall not exceed the allowed number of [detached dwelling units](#).
4. Design.
- a. All attached dwelling units in Single-Family Urban Zones shall meet the following design requirements in addition to those required by the City's adopted design

standards, RZC 21.08.180 Residential Development and Architectural, Site and Landscaping Design Regulations.

- i. Maintain the traditional character and quality of detached single-family dwelling units by using design elements, such as single points of entry noticeable from the street, pitched roofs, visible trim or framing around windows, porches, and chimneys.
 - ii. Be consistent in height, bulk, scale and style with nearby single-family residential uses.
 - iii. No side-by-side mirror image duplex designs shall be permitted.
 - iv. Locate surface parking for attached dwelling units in groups of no more than three stalls to appear more consistent with parking for single-family detached dwellings in the area. If parking areas include more than three stalls, they should be visually separated from the street or common areas through site planning, landscaping, or natural screening.
- b. New applications for three-unit and four-unit attached dwelling units in the Education Hill Neighborhood shall be accepted for lots no less than 500 feet of each other and new applications for duplex structures shall be accepted for lots no less than 250 feet of each other until an evaluation of compatibility with the neighborhood subarea is completed. See Education Hill Neighborhood Plan Policy N-EH-20 and N-EH-21 in the Redmond Comprehensive Plan.

5. Review and Decision Procedures.

- a. Bear Creek, Education Hill, Grass Lawn, North Redmond, Willows/Rose Hill, [Southeast Redmond](#) and Overlake Neighborhoods.
 - i. Review and decision for two-unit attached dwelling units shall occur through the Type I process.
 - ii. Review and decision for three-unit and four-unit attached dwelling units shall occur through the Type II process; in the Willows/Rose Hill Neighborhood it shall occur through the Type III process.

6. Affordable Housing Exception. In order to meet the City's objective of providing opportunities for the ownership of affordable family-size housing, the following exceptions to the requirements of [RZC 21.20](#), Affordable Housing, and some other requirements specifically provided for in this section apply:

- a. Two-unit attached dwelling units made affordable to [households](#) earning 80 percent or less of King County [median income](#) under the requirements specifically provided for in this section are allowed as part of a preliminary plat application for residential subdivisions of 10 units or more.

- b. Two-unit attached dwelling units meeting the affordability requirements of this section shall not be subject to the density requirements set forth in the zone summary for the residential zone district, or the minimum lot size requirements of subsection C.1 of this section, but shall be subject to the minimum lot size requirements of the underlying zone as set forth in the zone use chart for the residential zone. (See RZC [21.08.020](#) through [21.08.140](#).)
- c. A covenant agreement, as required by RZC 21.20.040 must be recorded for all two-unit attached dwelling units allowed under this section and meeting the affordability requirements of this section.

No changes to 21.08.270 through 21.08.280.

21.08.290 Cottage Housing Developments

No changes to 21.08.290.A through 21.08.290.E

F. Supplemental Neighborhood Requirements.

1. Applicability.

- a. Cottage housing developments are allowed in:
 - i. Single-Family Constrained and Single-Family Urban zones in the North Redmond and Willows/Rose Hill Neighborhood, except properties zoned R-8; and
 - ii. Single-Family Urban zones in the Bear Creek, Grass Lawn, Education Hill, [Southeast Redmond](#) and Overlake Residential Neighborhoods.
- b. Developments of less than four cottages are allowed in the Willows/Rose Hill and North Redmond Neighborhoods.
- c. Cottage housing developments shall not exceed eight cottages, not including accessory structures, within the East Education Hill subarea as defined by Map N-EH-2 in the Neighborhoods Element of the Redmond [Comprehensive Plan](#).

No changes to remainder of 21.08.290.F; no changes to remainder of 21.08.290; no changes to remainder of 21.08.

21.14.010 Neighborhood Commercial 1 (NC-1)

No changes to subsections A-D.

- E. **Allowed Uses and Basic Development Standards.** The following table contains the basic zoning regulations that apply to uses in the Neighborhood Commercial (NC-1) zone. To use the chart, read down the left-hand column titled “Use.” When you have located the use that interests you, read across to find regulations that apply to that use. Uses are permitted unless otherwise specified in the Special Regulations column. Permitted uses may require land use permit approval. See RZC 21.76.020, *Overview of the Development Process*, for more information. Uses not listed are not permitted.

Table 21.14.010C Allowed Uses and Basic Development Standards			
Section	Use	Parking ratio: unit of measure (required, allowed)	Special Regulations
Residential			
1	Residential	Unit (1.0, 2.25) plus 1 guest space per 4 units for projects of 6 units or more	Prohibited in NC-1 zones bordering 188th Ave NE in Southeast Redmond.
2	Mixed-use residential		
General Sales or Service			
3	Consumer goods sales or service, other than heavy or durable	1,000 sq ft gross floor area (2.0, 3.0)	
4	Grocery, food, beverage, and dairy		
5	Professional services		
6	Full-service restaurant		
7	Cafeteria or limited-service restaurant		
8	Personal services		
Transportation, Communication, Information, and Utilities			
9	Rapid charging station		Permitted only when three sides of site abut nonresidential zone.
10	Battery exchange station		
11	Communications and Information	Adequate to accommodate peak use	Only libraries are permitted.
12	Large Satellite Dishes/Amateur Radio Antenna(s)		Subject to RZC 21.56 , <i>Wireless Communication Facilities</i> .
13	Antenna Array and Base Stations		A Conditional Use Permit may be required; see RZC 21.56 , <i>Wireless Communication Facilities</i> , for specific development requirements.
14	Antenna Support Structures		Subject to RZC 21.56 , <i>Wireless Communication Facilities</i> . Conditional use permit required. See RZC 21.76.070.K , <i>Conditional Use Permit</i> .
15	Local utilities		
16	Regional utilities		Conditional use permit required. See RZC 21.76.070.K , <i>Conditional Use Permit</i> .
Arts, Entertainment, and Recreation			

Table 21.14.010C Allowed Uses and Basic Development Standards			
Section	Use	Parking ratio: unit of measure (required, allowed)	Special Regulations
17	Museums and other special purpose recreational institutions	1,000 sq ft gross floor area (2.0, 3.0)	
18	Amusement, sports, or recreation establishment		
19	Natural and other recreational parks	Adequate to accommodate peak use	
Education, Public Administration, Health Care, and other Institutions			
20	Government functions	Employee during maximum shift (1.0,1.0)	Limited to 1,000 square feet gross floor area per establishment. Excludes maintenance shops.
Other			
21	Water-enjoyment use	Adequate to accommodate peak use	Only allowed in the Bear Creek shoreline jurisdiction downstream of Avondale Road on Union Hill Road, Redmond Way, or SR 520; and in the Sammamish River shoreline jurisdiction at NE 85th Street or NE 90th Street.
22	Kiosk		Within the shoreline jurisdictions of Bear Creek and the Sammamish River, limited to uses associated with water enjoyment. Shall not locate in required parking, landscaping, or drive aisle area, or any area that would impede emergency access. Shall not reduce or interfere with functional use of walkway or plaza to below standards of Americans with Disabilities Act. Structures shall be secured to prevent tipping and endangering public safety. Maximum size is six feet wide by ten feet long. Administrative design review required for structures.
23	Vending cart		
24	Drive-up stand	1,000 sq ft gross floor area (2.0, 3.0)	Shall not locate in required parking, landscaping, or drive aisle area, or any area that would impede emergency access. Shall not reduce or interfere with functional use of walkway or plaza to below standards of Americans with Disabilities Act. Structures shall be secured to prevent tipping and endangering public safety. Maximum size is six feet wide by ten feet long. Administrative design review required for structures. Must submit circulation plan addressing queuing.

F. **Design Standards for NC-1 Zone in Southeast Redmond.**

1. Purpose. The purpose of this section is to establish design criteria specific to properties in NC-1 zones in Southeast Redmond in order to guide development to be functionally and aesthetically compatible with nearby residential and employment uses.

2. Development Along 188th Ave NE.

a. Intent. The NC-1 zone along 188th Ave NE is intended to provide small-scale sales and service uses primarily serving people who live or work nearby in a way that maintains

or enhances quality of life for those working or living in the immediate vicinity, and in a way that integrates with existing parks, residential development and the natural environment.

b. Design Criteria.

- i. Development in this zone shall provide direct non-motorized access to Southeast Redmond Park.
- ii. Vehicles shall access development in this zone exclusively from 188th Ave NE or a side street, but in no case shall vehicle access be from any street in a residential zone or NDD1.
- iii. At least half of any proposed open space and outdoor seating areas shall be oriented toward Southeast Redmond Neighborhood Park to have the effect of integrating the development with the park.
- iv. Non-pollution-generating impervious surfaces shall be infiltrated to the extent feasible.

G. **Cross-references.** For information on how to measure various site requirements like height and setbacks, see RZC 21.16.020, *How to Measure Site Requirements*. See RZC 21.16.030, *Other Applicable Regulations*, for information on other standards that may apply to you.

21.14.XXX, Marymoor Design District

A. Purpose

The purpose of the Marymoor Design District (MDD) is to implement policy calling for a walkable subarea that develops in a way that supports Bear Creek, Lake Sammamish, Redmond’s drinking water aquifer and other natural features. Shallow groundwater and lack of a stormwater outfall will require creative designs on the part of developers. To facilitate that, regulations are intended to allow developers to work within the natural constraints of the land. The MDD will feature opportunities for living, employment, community gathering, education, and small-scale shopping, all enhanced by proximity to Marymoor Park. Regulations for this design district support business growth and adaptation, cap general retail and service uses to encourage the location and growth of businesses in primary industries, and take advantage of the planned light rail station. The Marymoor Design District comprises two performances area called MDD1 and MDD2.

B. Maximum Development Yield

		Base	Bonuses Available	Maximum	Illustrations
MDD1	FAR	0.90	Required affordable housing: 0.09 Additional affordable housing: 0.45	0.99 with 10% affordable housing; 1.35 with 20% affordable housing	
	Height	4 stories	Incentive program: 1 story	5 stories	
MDD2	FAR	0.5	TDRs or GBP: 0.5	1.0	
	Height	2 stories	TDRs or GBP: 1 story Incentive	5 stories	

			program: 3 stories		
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C. Use Standards for MDD1

#	Use	Parking ratio: Unit of Measure (Min. required, Max. allowed)	Special Regulations
	Detached dwelling unit	Dwelling unit (2.0)	
	Size-limited dwelling		
	Cottage	Cottage (1.5, 2.0)	See RZC 21.08.290, Cottage Housing Developments, for specific site development requirements and supplemental neighborhood regulations that may apply.
	Accessory dwelling unit (ADU)	ADU (1.0)	See RZC 21.08.220, Accessory Dwelling Units, for specific regulations that may apply.
	Attached dwelling unit, 2-4 units	Dwelling unit (2.0)	See RZC 21.08.320, Designated Manufactured Homes, Manufactured Homes and Mobile Homes, for specific regulations that may apply.
	Manufactured home		
	Multifamily structure	Studio dwelling unit (1.0) 1-bedroom dwelling unit (1.25) 2-bedroom dwelling unit (1.25) 3+-bedroom dwelling unit (1.5)	

#	Use	Parking ratio: Unit of Measure (Min. required, Max. allowed)	Special Regulations
	Dormitory	Bed (0.5, 1)	
	Single room occupancy unit		
	Housing services for the elderly	See special regulations.	<p>1. Parking requirements are as follows:</p> <ul style="list-style-type: none"> a. Multifamily housing for senior citizens: Unit (0.5, 2.0) b. Nursing home or long-term care facility: 4 patient beds (1.0, 1.0) c. Retirement residence with no skilled nursing facility: Unit (1.0, 1.0) d. Retirement residence with skilled nursing facility: Worker on largest shift (1.25, 1.25) e. A traffic mitigation plan is required. The plan shall address traffic control; parking management, including mitigation of overflow parking into adjoining residential areas; and traffic movement to the arterial street system.
	Adult family home	Dwelling unit (2.0)	
	Parks, open space, trails and gardens	Adequate to accommodate peak use.	Permitted if public or noncommercial. A Conditional Use Permit is required for commercial facilities.
	Local utilities		
	Regional utilities		A conditional use permit is required.
	Large satellite dish	N/A	See RZC 21.56 , Wireless Communication Facilities, for specific development requirements.
	Amateur radio tower		
	Family day care provider	Vehicle used by the business (1.0).	Family day care providers are permitted as home businesses. See RZC 21.08.340 , Home Business, for specific regulations which may apply.
	Home business	Vehicle used by the business (1.0)	See RZC 21.08.340 , Home Business, for specific regulations that may apply.

#	Use	Parking ratio: Unit of Measure (Min. required, Max. allowed)	Special Regulations
	Roadside produce stand	N/A	

D. Performance Standards for MDD1

	Standard	Notes
Minimum setbacks	Front: 15 feet Side/interior: 5 feet Side street: 10 feet Rear: 10 feet Alley: 4 feet	
Maximum height	4 stories	Incentives can be used to increase to 5 stories.
Maximum FAR	Minimum: 0.90 Maximum: 0.99 with required affordable housing Maximum: 1.35 with 20% affordable housing	Incentives can be used to increase FAR to 0.80.
Maximum lot coverage by structures	55%	
Maximum impervious surface area	70%	1. Incentives can be used to increase to 75%. 2. Non-pollution-generating impervious surfaces shall be infiltrated.
Minimum landscaping	40%	Ecological score of 30 or greater required.
Truck traffic	See RMC 10.76.050	
Drive-through	Drive-through facilities are prohibited except	

	where expressly permitted elsewhere in this section.	
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E. Use Standards for MDD2

#	Use	Parking ratio: Unit of Measure (Min. required, Max. allowed)	Special Regulations
	General sales or service	1,000 sq ft gross floor area (4.0, 5.0)	<ol style="list-style-type: none"> 1. Limited to 20% of site gross floor area 2. Gas stations prohibited 3. For pet and animal sales and service: <ol style="list-style-type: none"> a. Boarding and training facilities must be located inside of a structure. 4. For animal kennel/shelter: <ol style="list-style-type: none"> a. Boarding facilities must be located inside of a structure. b. Outdoor runs or yards are allowed for the purpose of exercising animals. Runs/yards must be enclosed by eight-foot-high walls of sound-attenuating fencing or material such as masonry or concrete. c. The planned maximum number of animals to be sheltered shall be indicated on the application. The maximum may be reduced if the applicant cannot demonstrate that the development has adequate lot size and facility design to accommodate the planned number of animals in a way that ensures neighboring residential properties will not be impacted with noise or odor problems. 5. South of NE 67th St., operating hours are limited to the following: <ol style="list-style-type: none"> a. Sun: 7am-9pm b. Mon-Thu: 6am-9pm c. Fri-Sat: 7am-11pm
	Manufacturing and wholesale trade	1,000 sq ft gross floor area (2.0, 3.0)	
	Transportation, communication, information, and utilities		Heliports, float plane facilities, solid waste transfer and recycling, and all hazardous waste treatment and storage uses are prohibited.

#	Use	Parking ratio: Unit of Measure (Min. required, Max. allowed)	Special Regulations
	Arts, entertainment and recreation	Adequate to accommodate peak use	
	Education, public administration, health care, and other institutions		<ol style="list-style-type: none"> 1. All uses except educational services and religious institutions are limited to 20% of site gross floor area. 2. For religious institutions: <ol style="list-style-type: none"> a. A seat is one fixed seat, or 18 inches on a pew or bench, or seven square feet in the general assembly area, including aisle space, but excluding stage, podium, lobby, and space for musical instruments. b. Storage locations of buses/vans over 10,000 gvw shall be shown on a plan and screened from neighboring properties or right-of-way. c. Decorative fencing or decorative walls and landscaping on side or back lots are required when necessary to prevent visual impacts on neighboring properties and public shoreline areas. d. Off-site parking in residential zones is allowed only with a shared parking agreement with an existing institutional use, such as a school. e. A traffic mitigation plan is required. The plan shall address traffic control, parking management (including mitigation of overflow parking into adjoining residential areas), and traffic movement to the arterial street system. f. Steeples, bell towers, crosses or other symbolic religious icons mounted on the rooftop may exceed the maximum shoreline building height by 15 feet. (SMP) g. Maximum height for separate structures on-site, such as bell towers, crosses, statuary, or other symbolic religious icons, is 60 feet. h. Institutions with a seating capacity greater than 750 seats shall: require a traffic study or other documentation deemed suitable by the Technical Committee that demonstrates that

#	Use	Parking ratio: Unit of Measure (Min. required, Max. allowed)	Special Regulations
			<p>there will be no significant adverse impacts to traffic operations on the adjacent street system; have a maximum building height of five stories; be setback five additional feet for every one foot in building height over 45 feet exclusive of rooftop symbolic icons; not contain accessory or stand-alone parking facilities; not contain primary or secondary schools; and shall require a conditional use permit. See RZC 21.76.070.K, Conditional Use Permit.</p> <ul style="list-style-type: none"> i. Institutions with a seating capacity greater than 7,500 seats shall be located adjacent to at least one collector, minor, or principal arterial. <p>3. Secure community transition facilities require a conditional use permit.</p>
	Construction related businesses	1,000 sq ft gross floor area (2.0, 3.0)	
	Crop production		
	Roadside produce stand	N/A	
	Kiosk		<ul style="list-style-type: none"> 1. Shall not locate in required parking, landscaping, or drive aisle area, or any area that would impede emergency access. 2. Shall not reduce or interfere with functional use of walkway or plaza to below standards of Americans with Disabilities Act. 3. Structures shall be secured to prevent tipping and endangering public safety. 4. Maximum size is six feet wide by ten feet long. 5. Administrative design review required for structures.
	Vending Cart		

#	Use	Parking ratio: Unit of Measure (Min. required, Max. allowed)	Special Regulations
	Drive-up stand		<ol style="list-style-type: none"> 1. Shall not locate in required parking, landscaping, or drive aisle area, or any area that would impede emergency access. 2. Shall not reduce or interfere with functional use of walkway or plaza to below standards of Americans with Disabilities Act. 3. Structures shall be secured to prevent tipping and endangering public safety. 4. Maximum size is six feet wide by ten feet long. 5. Administrative design review required for structures. 6. Must submit circulation plan addressing queuing.

F. Performance Standards for MDD2

	Standard	Notes
Minimum setbacks	Front: 10 feet Side/interior: 5 feet Side street: 10 feet Rear: 10 feet Alley: 4 feet	
Maximum height	2 stories	Incentives can be used to increase to 5 stories.
Maximum FAR	0.5	TDRs or GBP can be used to increase FAR to 1.0
Maximum lot coverage by structures	55%	
Maximum impervious surface area	70%	<ol style="list-style-type: none"> 1. Incentives can be used to increase to 75%. 2. Non-pollution-generating impervious surfaces shall be infiltrated.
Minimum landscaping	30%	Ecological score of 30 or greater required.
Truck traffic	See RMC 10.76.050	
Drive-through	Drive-through facilities are prohibited except where expressly permitted elsewhere in this section.	

G. Design Standards

1. Purpose. The purpose of this section is to establish design criteria for properties in the Marymoor Design District that will guide development to be attractive in appearance and functionally integrated, take advantage of Marymoor Park as a visual and recreational amenity, and incorporate green spaces and green development into the district.
2. Development Along the Edge of Marymoor Park
 - a. Intent. The Marymoor Design District shares a border with Marymoor Park. Development on properties along this border should take advantage of Marymoor Park as a visual and recreational amenity, and should avoid creating or maintaining a wall between Marymoor Park and the Design District.
 - b. Design criteria.
 - i. No more than 50 percent of the park boundary shall be fronted with buildings so as not to create a wall of buildings along the park boundary.
 - ii. Buildings shall be designed to take advantage of the park as a visual amenity, such as by placing large windows onto the park.
 - iii. Developments shall provide connections to the park shown in the Southeast Redmond Neighborhood Connections Map in the Comprehensive Plan. These connections shall be landscaped in such a way that the landscaping draws attention to the existence of the connection and has the effect of creating a “green finger” from the park into the Design District.

H. Incentive Program

	Applicable Performance Areas	Maximum Incentive
Increase ecological score to 35 by incorporating items 7 and 19, or item 18	MDD1	1 additional story (to 5 stories)
Incorporate three community food gardens, each 4,000 square feet	MDD1	75% maximum impervious surface area (from 70%)
20% of homes affordable at 80% of area median income	MDD1	0.44 FAR increase (to 1.32, inclusive of all units and bonuses)
Reduce impervious surface area to 60%	All	1 additional story
Structured parking	MDD2	1 additional story
Arts (or in lieu contribution)	MDD2	75% maximum impervious surface area (from 70%)
Gathering place or park	MDD2	Up to 3 additional stories, depending on amount provided

Maximum height with incentives: 5 stories (MDD1 and MDD2)

Maximum FAR with incentives: 1.35 (MDD1), 1.0 (MDD2, achieved through TDR or GBP program)

Maximum impervious surface area with incentives: 75%

1. Features Explained.

- a. Increase ecological score to 35 by incorporating items 7 and 19, or item 18: incorporate vegetated walls and landscaped roofs, or incorporate green roofs according to the standards described in RZC 21.32.060, Ecological Score Requirements.
- b. Incorporate three community food gardens, each 4,000 square feet: provide land and planting-ready beds with appropriate soil for community food gardens open, at minimum, to residents of the development.
- c. 20% of homes affordable at 80% of area median income: start with the number of required affordable homes (10%) and double that number.
- d. Reduce impervious surface area to 60%: design the site to reduce total impervious surface area to 60%.
- e. Structured parking: provide at least half of all required parking in a parking structure rather than a surface lot.
- f. Arts (or in lieu contribution): commit 1% of total construction costs to art viewable by the public from the public way. Total construction cost is the sum of all construction costs shown on all building permits associated with the development. In lieu of providing public art a development using this bonus may contribute 1% of total construction costs to the Arts Activity Fund.
- g. Gathering place or park: dedicate at least one-half acre of land for a public park or gathering place accessible to the public from the public way. A development earns one additional allowed story for all buildings on the development site for each half-acre of land dedicated.

RZC 21.14.XXX, Northeast Design District

A. Purpose

The purpose of the Northeast Design District is to create a transition from single-family residential areas in the south to industrial areas in the north and to provide opportunities for businesses that require larger footprints. This will be accomplished by careful siting of buildings, vegetated buffers and park spaces, campus-like settings, and master planning. The performance zoning regulations in this chapter grant significant flexibility in terms of allowed uses while establishing district-specific design standards. Standards in this chapter emphasize transition from lower- to higher-intensity uses, creating neighborhood character and a sense of place through site and building design, and connecting this area to parks and regional trails located to the east. The Northeast Design District comprises three performances area called NDD1, NDD2 and NDD3.

B. Maximum Development Yield

		Base	Bonuses Available	Maximum	Illustrations
NDD1	FAR	0.50	Required affordable housing: 0.05	0.55	
	Height – residential	2 stories	Incentive program: 1 story	4 stories	
	Height – non-residential	2 stories	TDRs or GBP: 1 story Incentive program: 3 stories	5 stories	
NDD2 and NDD3	FAR	N/A	N/A	N/A	
	Height	2 stories	3 stories	5 stories	

C. Use Standards for NDD1

#	Use	Parking ratio: Unit of Measure (Min. required, Max. allowed)	Special Regulations
	Detached dwelling unit	Dwelling unit (2.0)	
	Size-limited dwelling		
	Cottage	Dwelling unit (1.5, 2.0)	See RZC 21.08.290, Cottage Housing Developments, for specific site development requirements and supplemental neighborhood regulations that may apply.
	Accessory dwelling unit (ADU)	ADU (1.0)	See RZC 21.08.220, Accessory Dwelling Units, for specific regulations that may apply.
	Attached dwelling unit, 2-4 units	Dwelling unit (2.0)	See RZC 21.08.260, Attached Dwelling Units, for specific regulations related to density, design, review and decision procedures, and affordable housing exceptions.
	Manufactured home		See RZC 21.08.320, Designated Manufactured Homes, Manufactured Homes and Mobile Homes, for specific regulations that may apply.
	Multifamily structure	Studio dwelling unit (1.0) 1-bedroom dwelling unit (1.25) 2-bedroom dwelling unit (1.5) 3+-bedroom dwelling unit (1.75)	

#	Use	Parking ratio: Unit of Measure (Min. required, Max. allowed)	Special Regulations
	Housing services for the elderly	See special regulations	<ol style="list-style-type: none"> 1. Retirement residences are permitted through a subdivision or binding site plan with a maximum of 16 retirement residence units per acre. Without a subdivision or binding site plan, a Conditional Use Permit is required. See RZC 21.08.360, Retirement Residences, for specific regulations which may apply. 2. A Conditional Use Permit is required for Residential Care Facilities. 3. A Conditional Use Permit is required for Long-Term Care Facilities. 4. Parking requirements are as follows: <ol style="list-style-type: none"> a. Multifamily housing for senior citizens: Unit (0.5, 2.0) b. Nursing home or long-term care facility: four patient beds (1.0, 1.0) c. Retirement residence with no skilled nursing facility: Unit (1.0, 1.0) d. Retirement residence with skilled nursing facility: Worker on largest shift (1.25, 1.25) 5. For Retirement Residences, a Traffic Mitigation Plan is required. See RZC 21.08.370.C.3.b.iii.
	Adult family home	Dwelling unit (2.0)	
	Community indoor recreation	Adequate to accommodate peak use	Includes noncommercial indoor recreation uses, such as community clubhouses, indoor swimming pools, and other similar facilities.
	Parks, open space, trails and gardens		Permitted if public or noncommercial. A Conditional Use Permit is required for commercial facilities.
	Local utilities		
	Regional utilities		A conditional use permit is required.
	Large satellite dish	N/A	See RZC 21.56 , Wireless Communication Facilities, for specific development requirements.
	Amateur radio tower		
	Family day care provider	Vehicle used	Family day care providers are permitted as home businesses. See RZC 21.08.340 , Home Business, for specific regulations which may apply.

#	Use	Parking ratio: Unit of Measure (Min. required, Max. allowed)	Special Regulations
	Home business	by the business (1.0).	See RZC 21.08.340, Home Business, for specific regulations that may apply.
	Roadside produce stand	N/A	

D. Performance Standards for NDD1

	Standard	Notes
Minimum setbacks – residential	<ol style="list-style-type: none"> 1. Front: 10 feet 2. Side / interior: 5 feet 3. Side street: 10 feet 4. Rear: 10 feet 5. Alley: 4 feet 	
Minimum setbacks – non-residential	<ol style="list-style-type: none"> 1. 15 feet from streets. 2. 15 feet from property lines abutting properties with residential uses. 3. All other setbacks determined through required master planning process 	
Maximum height – residential	3 stories	Up to 4 stories with incentives

	Standard	Notes
Maximum height – non-residential	2 stories	
Maximum FAR	0.50	Must achieve 140-170 units in NDD1 through master plan
Maximum impervious surface area	70 percent	Non-pollution-generating impervious surfaces shall be infiltrated to the extent feasible.
Minimum landscaping	40 percent	Ecological Score at least 30 required.
Groundwater intrusion	Permanent intrusion of parking garages, basements, elevator pits or similar structures into groundwater is prohibited. Note: foundations, auger cast piles, utilities, and similar structures that do not require dewatering are permitted.	
Truck traffic	See RZC 21.50.040 and RMC 10.76.050	
Drive-through	Drive-through facilities are prohibited except where expressly permitted elsewhere in this section.	

E. Use Standards for NDD2 and NDD3

#	Use	Parking ratio: Unit of Measure (Min. required, Max. allowed)	Special Regulations

	General sales or service	1,000 sq ft gfa (4.0, 5.0)	<ol style="list-style-type: none"> 1. Limited to 5% of site gross floor area 2. For pet and animal sales and service: <ol style="list-style-type: none"> a. Boarding and training facilities must be located inside of a structure. 3. For animal kennel/shelter: <ol style="list-style-type: none"> b. Boarding facilities must be located inside of a structure. c. Outdoor runs or yards are allowed for the purpose of exercising animals. Runs/yards must be enclosed by eight-foot-high walls of sound-attenuating fencing or material such as masonry or concrete. d. The planned maximum number of animals to be sheltered shall be indicated on the application. The maximum may be reduced if the applicant cannot demonstrate that the development has adequate lot size and facility design to accommodate the planned number of animals in a way that ensures neighboring residential properties will not be impacted with noise or odor problems.
	Manufacturing and wholesale trade	1,000 sq ft gfa (2.0, 3.0)	
	Transportation, communication, information, and utilities		<ol style="list-style-type: none"> 1. Heliports and float plane facilities prohibited. 2. Hazardous waste treatment and storage, primary: <ol style="list-style-type: none"> a. Prohibited within 1000 feet of NDD1 b. Otherwise, conditional use permit required
	Arts, entertainment and recreation	Adequate to accommodate peak use	<p>Limited to 15% of site gross floor area Prohibited within 500 feet of NDD1</p>
	Education, public administration, health care, and other institutions		<ol style="list-style-type: none"> 1. Limited to 5% of site gross floor area 2. For religious institutions: <ol style="list-style-type: none"> a. A seat is one fixed seat, or 18 inches on a pew or bench, or seven square feet in the general assembly area, including aisle space, but excluding stage, podium, lobby, and space for musical instruments. b. Storage locations of buses/vans over 10,000 gw shall be shown on a plan and screened from neighboring properties

			<p>or right-of-way.</p> <ul style="list-style-type: none"> c. Decorative fencing or decorative walls and landscaping on side or back lots are required when necessary to prevent visual impacts on neighboring properties and public shoreline areas. d. Off-site parking in residential zones is allowed only with a shared parking agreement with an existing institutional use, such as a school. e. A traffic mitigation plan is required. The plan shall address traffic control, parking management (including mitigation of overflow parking into adjoining residential areas), and traffic movement to the arterial street system. f. Steeples, bell towers, crosses or other symbolic religious icons mounted on the rooftop may exceed the maximum shoreline building height by 15 feet. (SMP) g. Maximum height for separate structures on-site, such as bell towers, crosses, statuary, or other symbolic religious icons, is 60 feet. h. Institutions with a seating capacity greater than 750 seats shall: require a traffic study or other documentation deemed suitable by the Technical Committee that demonstrates that there will be no significant adverse impacts to traffic operations on the adjacent street system; have a maximum building height of five stories; be setback five additional feet for every one foot in building height over 45 feet exclusive of rooftop symbolic icons; not contain accessory or stand-alone parking facilities; not contain primary or secondary schools; and shall require a conditional use permit. See RZC 21.76.070.K, Conditional Use Permit. i. Institutions with a seating capacity greater than 7,500 seats shall be located adjacent to at least one collector, minor, or principal arterial. <p>3. Secure community transition facilities require a conditional use permit.</p>
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Construction related businesses	1,000 sq ft gfa (2.0, 3.0)		
Crop production			
Roadside produce stand	N/A	<ol style="list-style-type: none"> 1. Shall not locate in required parking, landscaping, or drive aisle area, or any area that would impede emergency access. 2. Shall not reduce or interfere with functional use of walkway or plaza to below standards of Americans with Disabilities Act. 3. Structures shall be secured to prevent tipping and endangering public safety. 4. Maximum size is six feet wide by ten feet long. 5. Administrative design review required for structures. 	
Kiosk			
Vending cart			
Automobile parking facility			Automobile parking facilities prohibited within 500 feet of NDD1
Drive-up stand			<ol style="list-style-type: none"> 1. Shall not locate in required parking, landscaping, or drive aisle area, or any area that would impede emergency access. 2. Shall not reduce or interfere with functional use of walkway or plaza to below standards of Americans with Disabilities Act. 3. Structures shall be secured to prevent tipping and endangering public safety. 4. Maximum size is six feet wide by ten feet long. 5. Administrative design review required for structures. 6. Must submit circulation plan addressing queuing.

F. Performance Standards for NDD2 and NDD3

	Standard	Notes
Minimum setbacks	<ol style="list-style-type: none"> 1. 30 feet from arterials. 2. 250 feet from NDD1 boundary. 3. All other setbacks determined through required master planning process. 	
Maximum height	2 stories	Up to 5 stories with incentives
Maximum impervious surface area	70 percent	Non-pollution-generating impervious surfaces shall be infiltrated to the extent feasible.
Minimum landscaping	40 percent	

Groundwater intrusion	Permanent intrusion of parking garages, basements, elevator pits or similar structures into groundwater is prohibited. Note: foundations, auger cast piles, utilities, and similar structures that do not require dewatering are permitted.	
Truck traffic	See RZC 21.50.040 and RMC 10.76.050	
Noise	See RMC 6.36	NDD2 is a Class B zone NDD3 is a Class C zone
Drive-through	Drive-through facilities are prohibited except where expressly permitted elsewhere in this section.	

G. Design Standards

1. Purpose. The purpose of this section is to establish design criteria for properties in the Northeast Design District that will guide development to be attractive in appearance and functionally integrated, and to promote a transition from less intense to more intense uses moving from south to north.
2. Development Along East Edge of Design District
 - a. Intent. The east edge of the Design District is also the east edge of the city and or the urban growth area. It is where urban gives way to rural. Development in this area should emphasize that transition through site and building design.
 - b. Design criteria.
 - i. Common open spaces shall be oriented toward the city limit in order to provide views and opportunities to enjoy the rural area.
 - ii. No more than 50 percent of city limit line shall be fronted with a building so as not to create a wall of buildings along the city limit line.
 - iii. No building shall be taller than two stories within 30 feet of the city limit.
3. Development Along North Edge of Woodbridge
 - a. Intent. The north section of Woodbridge is a single-family area and development immediately to the north of Woodbridge should create a gradual transition to more intense uses.
 - b. Design criteria.
 - i. No building shall be taller than two stories within 100 feet of the north edge of Woodbridge.
4. Transition from Less Intensive to More Intensive Uses.

- a. Intent. The Northeast Design District is intended to facilitate a transition from less intensive to more intensive uses moving from south to north. Development in both NDD1 and NDD2 should avoid direct lines of sight between NDD1 and NDD2.
 - b. Design criteria.
 - i. Structures shall be designed to minimize views from NDD1 into NDD2 and instead orient views from NDD1 to the rural area, parks, internal to NDD1, or to other residential areas to the south.
 - ii. Landscaping shall be designed and located to minimize views between NDD1 and NDD2, with Type 1 plantings used to screen industrial or commercial uses within 1000 feet of NDD1. Landscaping shall be tiered and multistoried and shall have the effect of creating a visual buffer between NDD1 and NDD2.
 - iii. Internal circulation shall be designed to minimize views between NDD1 and NDD2. For example, avoid long, straight sections of roadway.
 - iv. In areas of NDD2 that are within 1000 feet of NDD1, internal vehicle circulation systems shall be screened with Type II plantings that are at least five feet wide
5. Create a Sense of Place and Neighborhood Character.
- a. Intent. Development in the Northeast Design District should create a sense of place and neighborhood character so that it is attractive to people who live or work there.
 - b. Design Criteria.
 - i. Developments shall primarily rely on pedestrian-scale lighting in areas of pedestrian circulation.
 - ii. Developments shall incorporate benches along internal circulation paths.
 - iii. Residential and non-residential developments that provide common open space or pocket parks shall design such spaces to be attractive for recreation for those of all ages.

H. Incentive Program

	Applicable Performance Areas	Maximum Incentive
Reduce impervious surface area to 60%	NDD1	1 additional story
Increase ecological score to 46 by incorporating items 18 or 19, 6 or 20, and 7, 13 and 17	NDD1	1 additional story
Additional buffering	NDD2	1 additional story
Structured parking	NDD2	1 additional story
Reduced impervious surface area to 60%	NDD2	1 additional story

Stormwater pond land dedication	NDD2	Up to 3 additional stories, depending on amount of land provided
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Maximum height with incentives: 3 stories (NDD1), 5 stories (NDD2)

1. Features Explained.

- a. Reduce impervious surface area to 60%: design the site to reduce total impervious surface area to 60%.
- b. Increase ecological score to 46 by incorporating items 18 or 19, 6 or 20, and 7, 13 and 17: incorporate green roofs or landscaped roofs, additional replacement trees or canopy-forming trees, and vegetated walls, land for community food gardens and pervious paving for non-pollution-generating impervious surfaces, according to the standards described in RZC 21.32.060, Ecological Score Requirements.
- c. Additional buffering: place buildings and any parking structures at least 300 feet from the NDD1 boundary.
- d. Structured parking: provide at least half of all required parking in a parking structure rather than a surface lot.
- e. Stormwater pond land dedication: dedicate at least one-half acre of land for a stormwater pond. A development earns one additional allowed story for all buildings on the development site for each half-acre of land dedicated. The dedicated land shall be consistent with the needs identified in the Southeast Redmond Regional Stormwater Facilities Plan or its successor.

I. Master Planning

- 1. Applicability. Master Plans are required for all developments encompassing at least three acres. Master Plans are optional for all other sites. When a Master Plan has been approved by the City, site requirements and other development standards and regulations shall be administered on the basis of the area controlled by the approved Master Plan (the “Plan Area”) rather than on a site-by-site basis, provided the approved Master Plan demonstrates the ability to comply with the requirement in question. For example, in the case of a development application for a site that is part of an area controlled by an approved master plan, if the plan designates the maximum lot coverage of structures and such areas are sufficient to meet maximum lot coverage requirements applied to the entire Plan Area, then an individual site plan need not demonstrate compliance with maximum lot coverage requirements.
- 2. Master Plans must demonstrate all of the following:
 - a. Ability to comply with all standards of this and other applicable chapters of the Redmond Zoning Code.
 - b. If the scope of an applicant’s proposal includes some but not all of NDD1, the ability to site 140-170 homes in NDD1 as part of future development by the same or another applicant.
 - c. If the scope of an applicant’s proposal includes some but not all of NDD2, the ability to develop the balance of NDD2 in a manner that is consistent with Southeast Redmond Neighborhood policies and this chapter of the Redmond Zoning Code, such as by including a general land use and site design concept showing hypothetical future phases.

RZC 21.20 AFFORDABLE HOUSING

21.20.010 Purpose

The purpose of this chapter is to:

- A. Implement the responsibility of the City under the state Growth Management Act to provide for housing opportunities for all economic segments of the community.
- B. Help address the shortage of housing in the City for persons of low and moderate incomes, helping to provide opportunities for low- and moderate-income persons who work in the City to live here, rather than in locations distant from employment that contribute to increased length and number of vehicle trips.
- C. Help facilitate an adequate affordable housing supply in the City by offsetting the pressure on housing costs resulting from high job growth and construction of high-end housing.
- D. Preserve and create opportunities for affordable housing as the City continues to grow.
- E. Encourage the construction of housing that is affordable to senior citizens of Redmond.

21.20.020 Applicability

- A. This chapter applies to:
 1. All new residential and mixed-use developments within the Downtown, Overlake, Bear Creek, Willows/Rose Hill, Grass Lawn, North Redmond, Southeast Redmond and Education Hill Neighborhoods;
 2. All new senior housing developments and congregate care dwelling units, not including nursing homes.

No change to remainder of chapter

No changes to sections .010-.050

21.32.060 Ecological Score Requirements

A. The purpose of this section is to enhance the City’s ecological functions by promoting water conservation, restoring and preserving habitat, increasing energy efficiency, and creating value through significant economic, social, and environmental benefit. This requirement is designed to increase the quality and canopy of planted areas within the City while promoting flexibility in design of landscaped areas.

B. An applicant is required to comply with ecological score requirements below ~~when a required landscaped area exceeds 500 square feet:~~

~~1. With the exception of the Marymoor and Northeast Design Districts, when a required landscaped area exceeds 500 square feet, a~~ An applicant shall achieve an ecological score of 20 or greater, based on the techniques listed in the table below, in any combination.

~~1-2. In the Marymoor and Northeast Design Districts, an applicant shall achieve an ecological score of 30 or greater, based on the techniques listed in the table below, in any combination.~~

~~2-3.~~ Scoring of points are awarded on the basis of a technique’s overall ecological benefit.

~~3-4.~~ Techniques listed with an “*” can achieve an additional score of one point for every increase of 10 percent. For example, a technique that requires 40 percent of trees to be preserved, an additional point shall be awarded as follows:

Technique: 40 percent Tree Preservation

Additional Point: 10 percent of 40 = 44 percent tree preservation

4. Every landscape plan shall include a minimum of three different techniques to achieve the total score and any one technique cannot exceed a maximum score of 10 points.

5. Techniques incorporating stormwater solutions shall comply with RMC Chapter 15.24, *Clearing, Grading, and Stormwater Management*.

		Table 21.32.060 Ecological Score Requirements			
Technique	Points Awarded - Downtown	Points Awarded – Overlake Village	Points Awarded – MDD and NDD	Points Awarded – Other citywide zones	
1. 25 percent of the plants installed are Northwest adaptive and 25 percent of the plants installed are native*	5 points	5 points	5 points	5 points	
2. 40 percent of existing significant trees includes landmark are retained.	3 points	3 points	7 points	7 points	

	Table 21.32.060 Ecological Score Requirements			
3. Minimum of 25 percent of proposed trees are evergreens	3 points	3 points	<u>5 points</u>	5 points
4. Minimum of 25 percent of evergreen trees are greater than 10 feet high at installation	3 points	3 points	<u>5 points</u>	5 points
5. Minimum of 25 percent of deciduous trees are 3-inch caliper or greater at installation	3 points	3 points	<u>5 points</u>	5 points
6. 10 percent increase over the minimum number of required replacement trees, street trees or parking lot trees*	3 points	3 points	<u>7 points</u>	5 points
7. Vegetated walls (including trellis, green tower or similar features) that have a minimum area of 300 square feet. Additional points in increments of three shall be awarded for every 300 square feet of vegetated walls provided.	5 points	5 points	<u>5 points</u>	3 points
8. Proposed water features use recycled water	3 points	3 points	<u>3 points</u>	3 points
9. Minimum of 25 percent of landscaped areas are designed with long-term irrigation from harvested rainwater (such as rain barrels)*	3 points	3 points	<u>5 points</u>	5 points
10. Minimum of 25 percent of landscaped areas are designed with landscaping that does not require irrigation after a three-year period	3 points	3 points	<u>3 points</u>	3 points
11. Minimum of 50 percent of landscaped areas where native soils are preserved on-site	4 points	4 points	<u>7 points</u>	7 points
12. Minimum of 50 percent of required planting areas in disturbed soils are amended	3 points	3 points	<u>3 points</u>	3 points
13. 5 percent of common open space or 25 square feet per unit, is reserved as a food garden*	5 points	5 points	<u>7 points</u>	3 points
14. Use of rain gardens, bioretention swales, engineered swales and/or engineered wetlands that treats 25 percent of pollution generating impervious surfaces	N/A	5 points	<u>5 points</u>	5 points
15. Use of rain gardens, bioretention swales, engineered swales and/or engineered wetlands for 25 percent of non-pollution generating impervious surfaces*	5 points	5 points	<u>5 points</u>	5 points
16. Permeable paving of 25 percent of paved areas within a site of pollution generating impervious surfaces*	N/A	5 points		5 points
17. Use of Permeable paving for 25 percent of non-pollution generating paved areas within a site*	5 points	5 points	<u>5 points</u>	5 points
18. Green Roofs that provide 10 percent of roof coverage*	5 points	5 points	<u>7 points</u>	5 points
19. Landscape Roofs that provide 10 percent of roof coverage*	2 points	2 points	<u>5 points</u>	2 points
20. Installed trees that will attain an average 30-foot-spread canopy in 10 years within parking lots.	5 points	5 points	<u>7 points</u>	3 points
21. 10 percent of roof coverage dedicated to solar panel installation*	5 points	5 points	<u>5 points</u>	5 points

No changes to remainder of chapter.

RZC 21.38 OUTDOOR STORAGE, RETAIL DISPLAY, AND GARBAGE AND RECYCLING ENCLOSURES

Outdoor Storage User Guide

21.38.010 Outdoor Storage and Retail Display

A. **Purpose.** The purpose of this section is to:

1. Create an attractive and economically healthy community by allowing for **outdoor retail display** as an **accessory use** to a permitted use.
2. Provide economic opportunities for existing businesses while encouraging pedestrian activity in commercial areas.
3. Create safe and attractive walkways within Downtown, Overlake, General Commercial and Neighborhood Commercial zones, and control of storage or display of materials to allow the minimum amount necessary to encourage quality **development** and avoid creation of a nuisance. (Ord. 2416)
4. Provide protection for existing parking areas and walkways from impacts of **outdoor storage**.
5. Ensure that adequate opportunity is allowed for the outdoor storage of **vehicles** and materials in residential zones while not impacting the character and uses intended for those zones.

C. **Applicability.**

1. The provisions of this chapter apply to all outdoor storage and retail displays within the **City** with the exception of:
 - a. Parking **lots** covered by RZC 21.40.010, *Vehicle Parking*;
 - b. RV parking and storage covered by RZC 21.40.010.G, *Parking and Storage of Recreational, Utility, and Commercial Vehicles and Vessels in Residential Neighborhoods*;
 - c. Outdoor storage associated with emergency situations such as utility repairs; and items stored on a **site** during **construction**.

D. **Outdoor Storage Standards.** Outdoor Storage shall be allowed as provided in the table titled "Requirements for Outdoor Storage." Transition Overlay Standards relating to outdoor storage shall apply as provided for in RZC 21.50.030, *Use, Operations and Development Standards in a Transition Overlay*.

Table 21.38.010 Requirements for Outdoor Storage				
Zone	Type of Storage Permitted	Size and Height Requirements	Location Restrictions	Screening Requirements
Downtown, OV, NC-1, NC-2	None	N/A	N/A	N/A
GDD, GC, <u>MDD1</u>	Bulk and Non-Bulk	Maximum height of ten feet	Bulk Storage cannot be located between the building and the front street . Non-Bulk Storage shall be moved indoors during close of business	Screening shall be placed on all sides of storage areas other than where a building wall would act as a screen. Screening shall be adequate to provide a solid barrier at least six feet in height. It may include fences, walls, earth berms or vegetation.
UR, RA-5, BP, OBAT,	Bulk and Non-Bulk	Maximum height 20 feet		
<u>NDD3</u> , MP and I	Bulk and Non-Bulk	Maximum height 20 feet		
<u>MDD2</u> and <u>NDD2</u>	<u>Bulk and Non-Bulk</u>	<u>Maximum height 20 feet</u>	<u>Bulk Storage cannot be located between the building and the front street. Non-Bulk Storage shall be moved indoors during close of business</u>	<u>Screening shall be placed on all sides of storage areas other than where a building wall would act as a screen. Screening shall be adequate to provide a solid barrier at least six feet in height. It may include fences, walls, earth berms or vegetation.</u>
BCDD, <u>NDD1</u> and R	See Outdoor Storage in Residential Zones	See Outdoor Storage in Residential Zones	See Outdoor Storage in Residential Zones	See Outdoor Storage in Residential Zones

(Ord. 2416)

E. Prohibited Locations for Outdoor Storage. Outdoor storage is prohibited as follows:

1. In floodways;
2. Within shoreline **setbacks** indicated in RZC 21.68.060, *Shoreline Buffers*; and critical area **buffers** as identified in RZC 21.64.020.B, *Stream Buffers*; RZC 21.64.030.B, *Wetland Buffers*; and RZC 21.64.060.B, *Landslide Hazard Area Buffers*;
3. On **slopes** greater than 15 percent;
4. In industrial and business park areas ~~which that~~ adjoin residential districts;
5. In required parking stalls;
6. In areas where outdoor storage causes traffic, pedestrian circulation or safety problems as determined by the **Administrator**, or where a minimum five-foot width of walkway does not remain clear and free of obstructions;
7. Any materials that attract animals, birds or vermin; and
8. Within emergency fire lanes.

F. Covering and Containing Outdoor Storage. Hazardous materials or deleterious substances that have the potential to threaten stormwater, soil or groundwater quality shall be stored within secondary containment and under cover to prevent contact with precipitation and

stormwater. Compliance with WAC 173-218 Underground Injection Control (UIC) regulations shall be demonstrated for outdoor storage areas that drain to infiltration.

G. Outdoor Storage in Residential Zones.

1. Limitations. Outdoor storage is prohibited in all residential zones except when the items stored are customarily associated with and accessory to the use of the dwelling and comply with the requirements of this section. Outdoor storage in RA-5 zones shall comply with the standards listed above in the Requirements for Outdoor Storage table of this chapter.
2. Allowed Outdoor Storage. Items customarily associated with the residential use of a dwelling may be stored outside provided the following conditions are met:
 - a. Outdoor storage may only take place outside of the front yard setbacks and side yard setbacks.
 - b. Except for vehicles allowed under RZC 21.08.340, *Home Business*, or sections E.3 or E.4 below, outdoor storage shall not be visible from a public or private street. Fences, landscaping, or a building wall may be used to ensure that an outdoor storage area is not visible from the street.
 - c. Outdoor storage areas shall not prevent emergency **access** to the residence or any **accessory structure**.
 - d. Outdoor storage shall not cover more than 200 square feet of land area.
 - e. Except for motor vehicles allowed under RZC 21.08.340, *Home Business*, or sections E.3 or E.4 below, materials stored outdoors shall not be owned by or used in any business or industry including a **home business**.
 - f. Except for vehicles allowed under RZC 21.08.340, *Home Business*, or sections E.3 or E.4 below, materials stored outdoors shall not exceed a height of six feet nor shall they be stacked or stored higher than six feet.
3. **Recreational and Utility Vehicles**. See RZC 21.40.010.G, *Parking and Storage of Recreational, Utility, and Commercial Vehicles and Vessels in Residential Neighborhoods*.
4. Commercial Vehicles.
 - a. Allowed Commercial Vehicles.
 - i. Within a residential zone, no more than one **commercial vehicle** may be parked on a lot(s) occupied by a residence or on a street(s) adjoining the residence. Where a lot includes more than one residence, one commercial vehicle may be parked on the lot(s) or an adjoining street for each residence. Notwithstanding this provision, where an accessory dwelling and a primary dwelling occupy one

or more lots, only one commercial vehicle may be parked on the lot(s) occupied by the residences or on the street(s) adjoining the residences.

- ii. The commercial vehicle shall be operable.
 - iii. Other than cleaning the commercial vehicle, maintenance and repairs shall not be performed on the commercial vehicle within a residential zone except on the premises of a home business that meets the requirements of RZC 21.08.340, Home Business.
 - iv. The commercial vehicle shall not be parked or stored on a lawn or in any landscaped area.
- b. Prohibited Commercial Vehicles. Except as provided in section E.4.c below, and RZC 21.08.340, Home Business, and except as to school buses parked or stored on the property of a school or religious institution, truck tractors, truck tractor trailers, vehicles over 10,000 pounds gross weight, and commercial vehicles that do not comply with section E.4 shall not be parked or stored within a residential zone.
- c. Vehicles used in a business may be parked in a residential zone when making pickups or deliveries or being used in conjunction with the performance of a service on property within a residential zone.

5. Storage, Shipping, or Moving Container.

Administrative Interpretation: Storage, Shipping, or Moving Container

- a. A rented, leased, purchased, or assembled storage, moving, or shipping container, when associated with the construction of a home, or homes, in a [subdivision](#), may be located anywhere on a property within the R-1 to R-30 residential land use zones. Any rented, leased, purchased, or assembled storage, moving, or shipping container associated with construction permits must be removed no later than 60 days after the issuance of a certificate of occupancy or final inspection approval for the construction.
- b. Rented, leased, purchased, or assembled storage, moving, or shipping containers within the R-1 to R-30 residential zones that are not associated with construction permits may be placed temporarily on a [driveway](#) and/or hard surface only, providing that:
 - i. Any and all containers are visible from a public [right-of-way](#);
 - ii. Any and all containers fit entirely on the driveway and/or hard surface;
 - iii. Containers are not stacked;

- iv. Any and all containers do not protrude onto any part of any sidewalk or public right-of-way without the owner or agent of the property having first obtained a Street Use Permit;
- v. Any and all containers are not located in a site distance triangle; and
- vi. Any and all containers remain on the property for no more than 60 calendar days in any 365 calendar day period. The 365 calendar day period commences the first day that the container is located on-site.

H. Outdoor Retail Display Standards. Outdoor Retail Display shall comply with the following criteria:

1. The outdoor retail display shall be accessory to a permitted retail use;
2. The total space allowed for outdoor retail display shall not exceed 50 percent of the length of the storefront, provided that a minimum area of 32 square feet shall be allowed in any event;
3. Retail items must be displayed in a neat and orderly manner, and remain in the area specified for its display;
4. Retail display shall not be located within required fire lanes or required parking stalls;
5. Retail display shall not be located within the public right-of-way without required permits and shall maintain a clear zone of a minimum of 44 inches in width to accommodate pedestrian access along sidewalks;
6. Safe ingress and egress to the site, visibility for [transportation](#), and pedestrian access shall be maintained;
7. The location of the retail display shall be established as a condition of approval of any applicable permits.

Effective on:9/17/2011

21.38.020 Garbage and Recycling Enclosures

- A. **Purpose.** The purpose of these standards is to ensure that new [construction](#) incorporates the space required for on-site garbage and recycling containers including food waste collection by:
1. Establishing mandatory minimum storage space requirements for multifamily residential and nonresidential [buildings](#); and
 2. Providing location and design guidelines that will assist the [applicant](#) in the [development](#) of such spaces.
- B. **Exemptions.** The following are exempt from the requirements of this division:
1. Multifamily dwellings that will be served by curbside collection at each unit.

2. Detached single-family dwelling units, parks and construction-sites.
3. Structural alterations to existing uses that do not increase gross floor area.

C. **Disposal System and Storage Space Requirements.** The following minimum space requirements for recycling and garbage enclosures shall be incorporated into the design of all multifamily and nonresidential buildings:

Table 21.38.020 Disposal System and Storage Space Requirements	
Land Use	Disposal System and Storage Requirement
Multifamily residential and hotels	1.5 square feet per dwelling unit or room and accommodate one garbage and one recycling container
Office, educational and Institutional developments	2 square feet for 1,000 square feet of gross floor area; maximum of 1,000 square feet
Manufacturing and general commercial developments	3 square feet for 1,000 square feet of gross floor area; maximum of 1,000 square feet
Retail development	3 square feet for 1,000 square feet of gross floor area;

D. **Location and Accessibility.**

1. Enclosures shall not interfere with the primary use of the site and shall:
 - a. Minimize exposure of residents or employees to waste-related noise and odor.
 - b. Minimize conflicts between collection trucks and pedestrian or vehicular traffic.
 - c. Not be located in required yards, critical area buffers, utility corridors, or easements.
 - d. Not be located within 12 feet of a fire hydrant, unless approved by the Fire Department.
2. The enclosures for multifamily residential developments shall be located in collection points as follows:
 - a. There shall be one collection point for every 30 dwelling units.
 - b. No dwelling unit within the development shall be more than 200 feet from a collection point.
3. Enclosures for nonresidential development may be centrally located.

E. **General Standards.** The following provisions shall apply to all garbage and recycling enclosures that contain receptacles served by commercial collection equipment.

1. Enclosures shall be incorporated into the designs for multifamily and nonresidential buildings.
2. Drainage. The drainage area of uncovered storage enclosures must be restricted to 200 square feet. Storage areas that exceed 200 square feet must be covered. Please refer to the

City of Redmond's *Clearing, Grading, and Stormwater Management Technical Notebook* for additional requirements relating to drainage of storage enclosures.

3. Weather Protection. Storage containers shall be protected from weather damage by using containers that are largely weatherproof or by covering the storage area. Roofed storage areas must be accessible to haulers and collection equipment.
 4. Gate Openings. Enclosures for outdoor collection points and buildings used primarily to contain a collection point shall have gate openings at least 12 feet wide for haulers. In addition, the gate opening for any building or other roofed **structure** used primarily as a collection point shall have a vertical clearance of at least 12 feet. Gate materials shall be consistent with the materials and design of the primary structure(s).
- F. **Review and Approval.** The proposed site plan and enclosure detail shall be submitted to the City and City's contracted solid waste hauler for review and approval. Before a **building permit** can be issued, a form from the City's contracted solid waste hauler accepting the relevant container location and enclosure detail must be submitted.

RZC 21.50 TRANSITION OVERLAY AREAS

21.50.010 Purpose

The purpose of this division is to:

- A. Set performance standards and create transition areas as a means of implementing the policies of the Redmond [Comprehensive Plan](#) promoting land use compatibility;
- B. Use of techniques such as citywide [development](#) and performance standards in order to minimize potential conflicts between abutting higher and lower intensity zones; and
- C. Regulate [site](#) design, [construction](#), uses, and site operations in transition areas within higher intensity zones in order to protect the character of abutting lower density zones.

Effective on:4/16/2011

21.50.020 Transition Overlay Areas

- A. Transition Overlay Areas.
 1. Transition Overlay regulations shall apply to those portions of “complying zones” within the Transition Overlay area, as designated in RZC 21.50.020.A.5, *Protected and Complying Zones*.
 2. The [Administrator](#) may waive some or all of the regulations of this division where a proposed [development](#) in a complying zone consists of uses and activities whose [noise](#), [glare](#), light trespass, [outdoor storage](#), and other similar [site](#) and [building](#) impacts are equal to or less than what is allowed for development in the abutting protected zone. Where such a waiver is granted, the proposed development in the complying zone must:
 - a. Comply with site requirements for the abutting protected zone as shown in the Allowed Uses and Basic Development Standards table in the zone chapter;
 - b. Provide a site plan and perimeter landscaping plan that protects development in the abutting protected zone from adverse impacts resulting from the proposed development; and
 - c. Restrict the uses and activities of the proposed development to those on which the waiver was based.

Notwithstanding the above, the [City](#) may impose select requirements of this division where it determines that doing so would mitigate adverse impacts resulting from the proposed development

A.

3. Transition Overlay development standards shall apply in addition to the development standards applicable in the underlying zone. Where there is a conflict between the standards, the most restrictive shall apply.
4. Property in complying zones rezoned to a protected zone on or after April 14, 2007, shall not be designated “protected,” as designated in subsection A.5 below, nor shall property in zones that would otherwise be designated “complying” as a result of that rezone be designated as such. Instead, the **developer** of the property rezoned to a complying zone shall have the responsibility of providing within the property’s own boundaries protections that would otherwise be accorded to “protected” zones in this chapter. Those protections would **buffer** uses in protected zones from, and mitigate the impacts associated with, uses typical of complying zones, which may include, but are not limited to, various manufacturing, assembly, warehouse, entertainment, and other uses that operate both at day and at night, at noise levels consistent with existing regulations governing complying zones. The mitigation may be achieved through visual and audio screening, increased **setbacks**, building placement, open space, landscaping, architectural screening, berms, fences, topographical separation, or other methods that meet the intent of this provision as determined by the Administrator.
5. Protected and Complying Zones.

A.

6. Except as otherwise provided in this division, the Transition Overlay shall be located within the complying zone.

Table 21.50.020 Protected Zones					
	Protected Zones (Protected by Transition Overlay Regulations)				
		UR, RA-5	R-1, R-3	R-4 – R-8, ORS1	R-12 – R-30, NDD1 , MDD1 , ORM2
Complying Zones (Required to Comply with Transition Overlay Regulations)	R-12, R-18, NDD1	Δ	O	O	
	R-20, R-30, NC-1, NC-2, MDD1	Δ	◆	O	
	GC, BP, OBAT, MP, I, MDD2, NDD2, NDD3	Δ	◆	◆	O
	GDD	Δ	◆	◆	Δ

Table 21.50.020 Protected Zones				
Protected Zones (Protected by Transition Overlay Regulations)				
	UR, RA-5	R-1, R-3	R-4 – R-8, ORS1	R-12 – R-30, <u>NDD1</u> , <u>MDD1</u> , ORM2
<p>TABLE NOTES:</p> <p>◆ A 300-foot-wide Transition Overlay shall apply.</p> <p>○ A 150-foot-wide Transition Overlay shall apply.</p> <p>Δ Where the Administrator determines that, because of potential adverse impacts resulting from allowed uses and intensities in the complying zone, the transition overlay regulations shall apply, the Administrator shall assign a Transition Overlay width up to 300 feet. The specific width of the Transition Overlay shall be the minimum width deemed necessary by the Administrator to be sufficient to protect development in the protected zone from the potential adverse impacts of the allowed uses and intensities in the complying zone.</p> <p>1 ORS (Outside Redmond Single-Family) refers to primarily single-family residential zones in neighboring jurisdictions that have an allowed density of eight or fewer housing units per <u>gross acre</u>.</p> <p>2 ORM (Outside Redmond Multifamily) refers to primarily small <u>lot</u> single-family residential zones and primarily multifamily residential zones in neighboring jurisdictions that have an allowed density greater than eight units per gross acre. (Ord. 2614)</p>				

A.

7. The Transition Overlay shall be measured at right angles along the boundary of the complying zone, except as described in subsection A.8 of this section.
8. The Transition Overlay shall include the following areas within the protected zone:
 - a. Street and railroad rights-of-way;
 - b. The Sammamish River, parks or easements for park and open space uses owned by a public agency, and trail rights-of-way or corridors where a public agency owns or leases the right-of-way or corridor or has an easement or equitable servitude for the right-of-way or corridor;
 - c. Native Growth Protection Areas and easements or equitable servitudes with similar purposes; and
 - d. Undeveloped critical areas and their buffers where structure construction is generally prohibited through RZC 21.64, Critical Areas Regulations, provided that the Technical Committee determines that the area is unlikely to ever be developed based on the nature and extent of the critical area.

Effective on:9/17/2011

21.50.030 Use, Operations, and Development Standards in a Transition Overlay

A. Use and Operations Standards and Limitations in a Transition Overlay.

1. Outdoor nonemergency maintenance and testing activities may only take place from 7:00 a.m. to 7:00 p.m., Monday through Friday, and from 9:00 a.m. to 6:00 p.m. Saturday. No

maintenance and testing activities may take place on legal holidays. Emergency maintenance activities may take place at any time and on any day.

2. All exterior work activities ordinarily allowed by the underlying zone shall be prohibited except for the following uses: restaurants, plant nurseries, entertainment and recreation uses that are commonly performed outside, sales of motor **vehicle** fuels, car washes, parking **lots**, outdoor markets, and sidewalk sales. This regulation does not apply in transition overlay areas in the Industry zone.
3. Except for in the Industry (I) zone, no exterior storage of goods or materials shall be allowed within the Transition Overlay. Exterior storage of **construction** materials and construction equipment during temporary construction activities is allowed.

B. Site and Building Design Standards in a Transition Overlay.

1. Site Design Standards.
 - a. Uses shall be located on a **site** so as to minimize adverse impacts on protected zones.
 - b. **Streets, driveways**, parking, parking **structures**, and other vehicle use areas shall be designed, located, constructed, and maintained to minimize the impacts on protected zones of **noise**, and direct and reflected light trespass. Joint use driveways shall be used where possible. Parking structures shall be designed and constructed to minimize light from cars and lighting within the structure spilling over or intruding into protected zones.
 - c. New driveways, curb cuts, and streets shall be located, designed, and constructed to minimize cut-through traffic in protected zones. The **Technical Committee** may authorize an exception to this requirement when compliance would create an undue hardship on the **applicant**.
 - d. Surface parking lots, loading and refuse collection areas shall be located away from bordering protected zones and screened from street level views. This screening shall be done by placing the areas behind **buildings** or by the use of berms, hedges, walls, or equivalent or better methods. The Technical Committee may authorize an exception to this requirement when compliance would create an undue hardship on the applicant.
 - e. Loading and refuse collection areas shall not be located within a front **setback** or within the Overlake Business and Advanced Technology Zone Height and FAR Limit Overlay described in RZC 21.12.210.C, *Building Height*.
 - f. Nearby topography, vegetation, street patterns, parking configuration, building massing, and building and site design should be considered in order to result in a compatible fit between the proposed **development** and existing residential developments.

2. Building Design Standards.

- a. Building surfaces and design shall minimize light reflecting into protected zones and allowing light from inside the building to intrude into residential zones. Glass curtain walls, metallic wall or roof coverings, or similar materials shall not face residential zones.
- b. Building **facades** visible from protected zones shall be stepped back or projected forward at intervals to provide a minimum of 40 percent facade **modulation**. The minimum depth of modulation shall be one foot and the minimum width shall be five feet.

C. **Signs in a Transition Overlay.**

- 1. Only freestanding and **wall signs** shall be permitted. Freestanding signs shall be affixed directly to the ground and be no higher than five feet from the finished **grade**.
- 2. Signs shall only be indirectly lighted.

D. **Building Setbacks in a Transition Overlay.**

- 1. The following table shows the required setback from **property lines** that border a protected zone or property lines adjacent to a street that borders a protected zone.

Table 21.50.030A Required Setbacks from Property Lines	
Height of Building or Portion of Building	Minimum setback in complying zones: R-12 – R-30, NC-1, NC-2, GC, BP, MP, I, GDD, MDD, NDD
Up to 30 feet	20 feet
31 to 40 feet	25 feet
41 to 50 feet	30 feet
More than 51 feet	35 feet

(Ord. 2614)

A.

- 1.
 - a. This setback shall not apply to property lines bordering a multimodal corridor as designated in the Transportation **Master Plan**, or an arterial street served by all-day or peak-hour transit service.
 - b. This setback shall not apply to property lines bordering the SR 520 **right-of-way**.

E. **Maximum Height of Structures in a Transition Overlay.**

- 1. Maximum Height of Structures (in feet) with and without Bonuses in a Transition Overlay.

Table 21.50.030B Maximum Height of Structures					
		Maximum structure height in complying Zones (maximum height with bonuses shown in parenthesis)			
		R-12, R-18, <u>NDD1</u>	R-20, R-30, <u>MDD1, I</u>	BP	MP, GDD, <u>MDD2, NDD2, NDD3</u>
Protected Zones	RA-5, R-1 – R-8, and ORS	40 (45)	40 (50)	35(45)	40 (45)
	R-12, R-18, NC-1, NC-2, <u>MDD1, MDD2,</u> and ORM		50 (60)	45 (55)	

(Ord. 2614)

A.

2. The maximum height of structures may be increased to the height shown in subsection E.1 of this section if one or more of the following features are provided and if the potential adverse impacts of the height increase on properties in the protected zone are mitigated. In no case shall the maximum height of structures set in subsection E.2 of this section be exceeded through this subsection.

Table 21.50.030C Features	
Feature	
	At least one-quarter of the on-site parking is provided in underground parking structures <u>(not permitted in MDD)</u> .
	At least one-quarter of the on-site parking is located in parking structure(s) outside the transition zone.
	The building has a pitched roof.
	No mechanical equipment is located on the roof.
	The <u>existing grade</u> under the proposed building pad is at least 10 feet below the grade at the property lines of all properties in the protected zone that border the development site or are across the street from the development site.
	A landscape <u>buffer</u> at least 100 feet wide is provided along at least one public street bordering a protected zone.
	TDRs are used to increase building height.

F.

3. The maximum height of structures set in subsections E.1 and E.2 of this section may be increased by the Design Review Board if all of the following conditions are met:
 - a. The modified building height does not exceed the maximum height, without bonuses, permitted by the underlying zone for properties outside the transition overlay.
 - b. The proposal, with the height modification, will provide an equivalent or better transition to the protected properties as the maximum height of structures without bonuses in subsection E.1 of this section.

G. Landscaping and Buffers in a Transition Overlay.

3. Required Buffers.

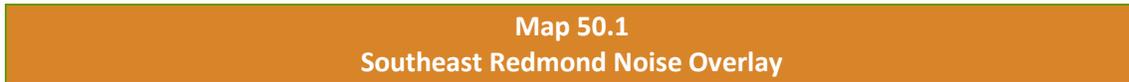
- a. Landscape buffers at least 20 feet wide shall be provided in the following locations, unless otherwise provided in the Zoning Code:
 - i. Along property lines that border a protected zone.
 - ii. Along **street frontages** where any portion of the street bordering the development site borders an R-1 through R-8, ORS, or ORM zone.
 - iii. Where the development site is zoned GC, MP, or I, along the street frontages where any portion of the street bordering the development site borders an R-1 through R-30, ORS or ORM zone.
4. In addition to the requirements of **RZC 21.32**, Landscaping, the following planting requirements shall apply in all setbacks, **open spaces**, and buffers:
 - a. All **significant trees** within 15 feet of the property line where a required transition overlay buffer or setback must be provided shall be retained unless the **removal** is necessary for streets, sidewalks, or utilities.
 - b. Where a GC, MP, or I zone borders a residential zone on an interior property line, an evergreen hedge a minimum of three feet in height at the time it is planted and capable of achieving a continuous visual screen with a height of four feet within three years shall be planted, or a combination of shrubs and a fence shall be added within the required **planting area**, to achieve the effect of a hedge.
5. Except as otherwise provided in this section, no structures shall be placed in required buffers.
 - a. Up to 20 percent of the buffer area may be used for streets, driveways, utility crossings, trails, or ground level features such as patios.
 - b. Patios shall not be placed closer than 10 feet from the property line.
 - c. No existing structure, except as noted in subsections F.3.a and F.3.b of this section, shall be considered a legal nonconforming use.
6. Buffers may be counted towards required open space, required pervious surfaces, and other requirements that they meet. Except for trails, any **impervious surfaces** within the buffer shall not be counted towards fulfilling open space requirements.

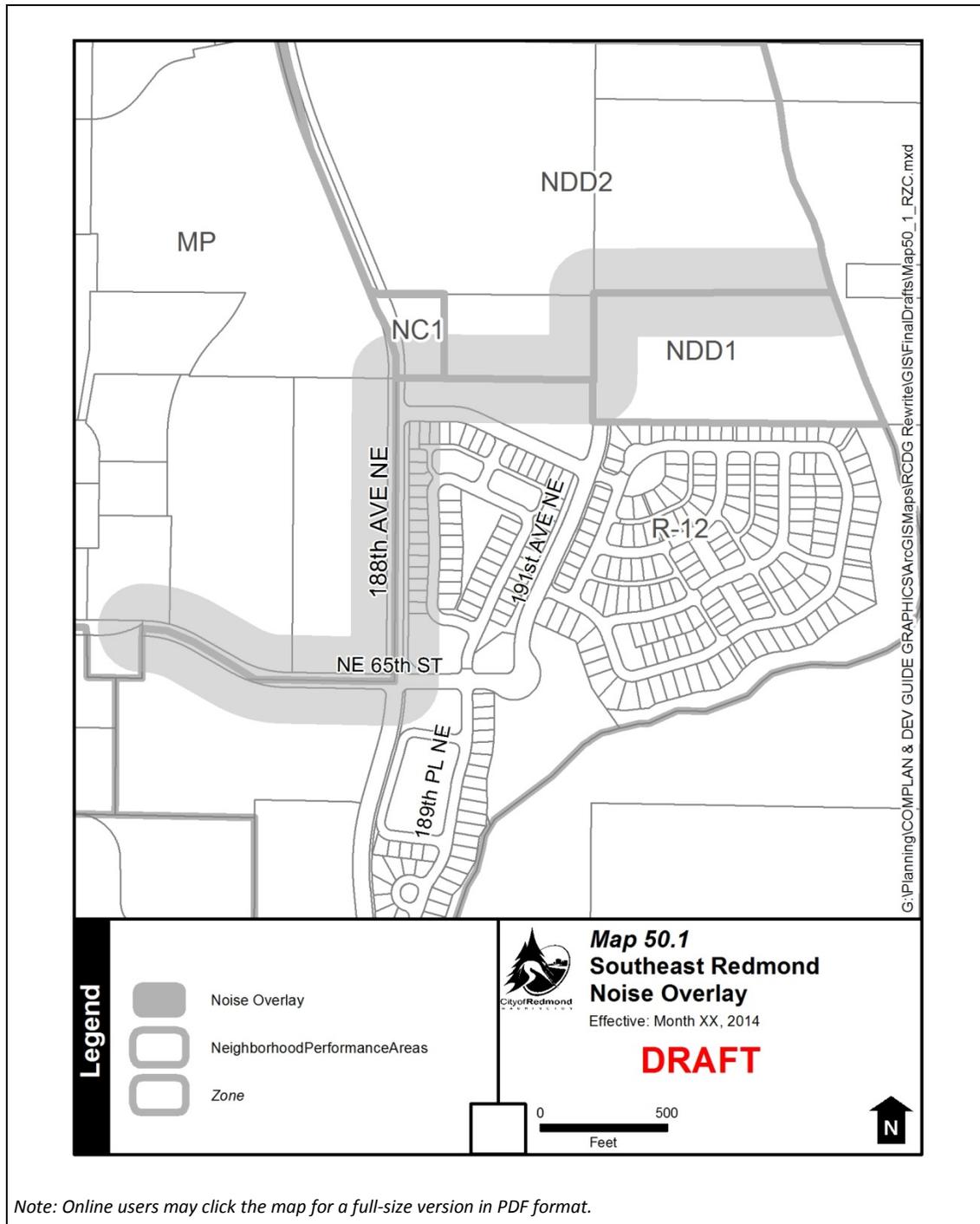
Effective on:9/17/2011

21.50.040 Southeast Redmond Noise Overlay Requirements

- A. **Purpose.** A **noise** overlay area where nonresidential uses will adjoin residential areas is established. The purpose is to protect new residential **development** from potential significant noise impacts from nonresidential uses.

- B. **Applicability.** The Southeast Redmond Noise Overlay area is located as shown on Map 50.1, *Southeast Redmond Noise Overlay*. The overlay shall be 350 feet wide. Where the overlay is shown over [right-of-way](#), the centerline of the right-of-way shall be the center point of the overlay. Where the overlay is shown over a zone boundary, the center point of the overlay shall be zone boundary.





1. Requirements. Development within the overlay area shall meet the following requirements:
 - a. New or expanding industrial, manufacturing, and business park uses shall provide appropriate noise restricting techniques such as earthen berms, locating the noisiest activities farthest from residential areas, and providing densely vegetated open space between residential and more intensive uses as listed in Policy N-SE-5 of the

~~Comprehensive Plan on their sites~~ to protect existing and future residential development from potential significant noise impacts.

- b. Operations and/or business activities ~~that which~~ generate significant noise impacts, such as heavy truck traffic at nighttime with loading and unloading, should be restricted from the noise overlay area.

No changes to sections .010-.030

21.60.040 Design Concepts

A. **Purpose.** The purpose of this section is to establish criteria for **building** design and review that addresses architectural concepts, building scale, details, materials, **colors**, blank wall treatment, pedestrian features, and personal safety.

B. **Buildings.**

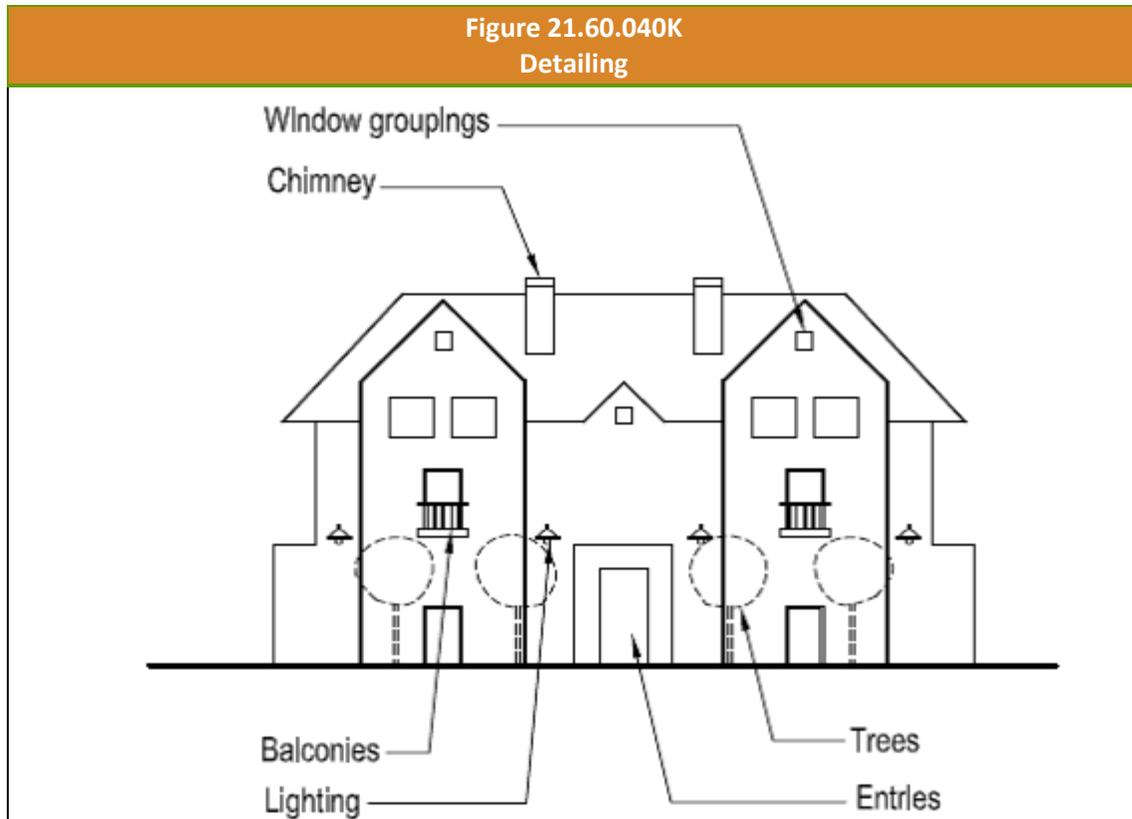
No changes to 21.60.040.B.1 through 21.60.040.B.3

4. Building Details, Materials, and Colors.

a. Intent. To provide visual interest, distinct design qualities, and a pleasing transition from the natural to the built environment, and to promote compatibility and improvement within surrounding neighborhoods and community development through architectural detailing and the use of sustainable and high-quality materials.

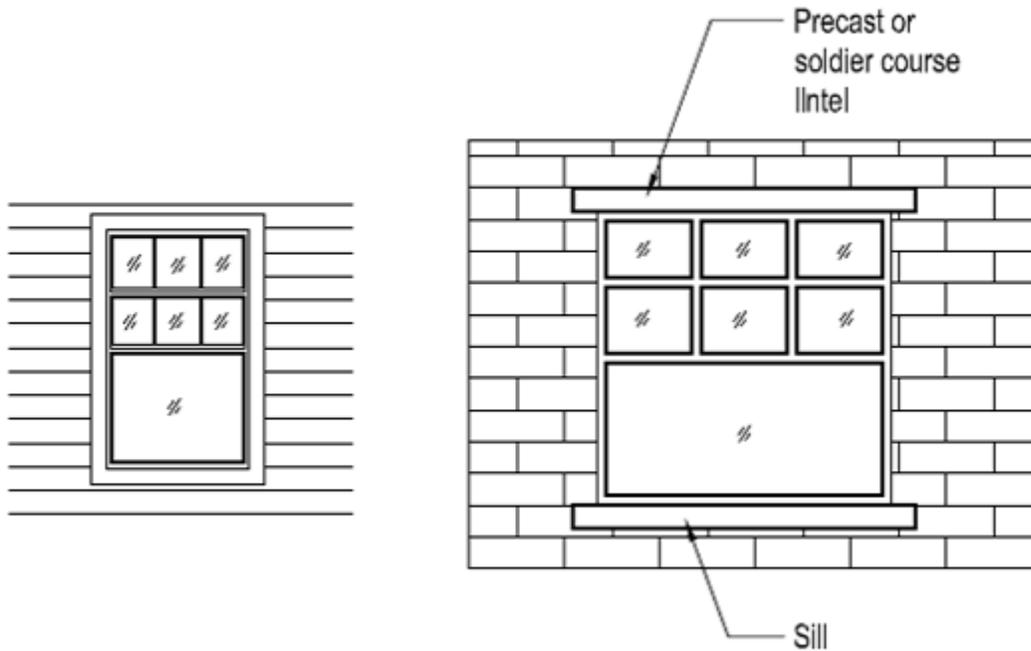
b. Design Criteria.

i. Use building materials of high durability and high quality. The use of brick is encouraged on walls or as accents on walls. Large areas of rough-cut wood, wide rough-cut lap siding, or large areas of T-111, plywood, or similar materials are prohibited. Vinyl siding is prohibited on the ground floor of commercial buildings. Wood-textured, cementitious fiberboard products should be considered in lieu of wood siding for commercial buildings.



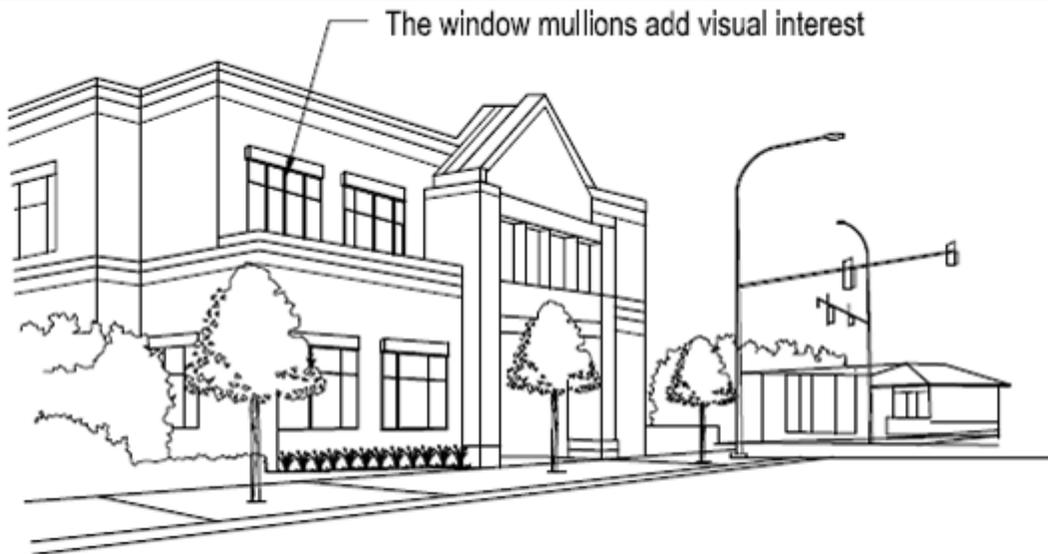
- ii. Enhance buildings with appropriate details. The following elements are examples of techniques used on buildings to provide detail.
 - a. Detailed Treatment of Windows and Doors. Examples include decorative lintels, sills, glazing, door design, molding, or framing details around all windows and doors located on facades facing or adjacent to public streets or parks.

Figure 21.60.040L
Windows



Multiple-paned windows also help to achieve human scale. Up to 50 percent of the window area can be composed of larger panes. Lintels and sills should be incorporated in masonry and stucco clad buildings. Wood trim should be incorporated into wood clad buildings.

Figure 21.60.040M
Window Mullions



Window Mullions Add Visual Interest

- b. Ornamentation. Examples include ornamental railings, grillwork, landscape guard, and trellises.
 - c. Distinctive Light Fixtures. Examples include lights with a decorative shade or mounting.
 - d. Varied Building Materials. Examples include patterned masonry, shingle, brick, or stone. Also, individualized patterns or continuous wood details, such as shingles in a geometric pattern, decorative moldings, brackets, wave trim or lattice work, ceramic tile, stone, glass block, carrera glass, or similar materials.
 - e. Artwork or Decorative Paving. The artwork may be freestanding or attached to the building, and may be in the form of mosaic mural, bas-relief sculpture, light sculpture, water sculpture, fountain, freestanding sculpture, art in pavement, or other similar artwork.
- iii. Avoid the use of building features or design elements that incorporate corporate themes, logos, or colors which do not reflect the neighborhood and community context.
 - iv. High-quality and natural materials and methods should be used to accent visible building features (i.e., wood, stone, brick, etc.). Building design should incorporate and display the natural grain or texture of materials. Wood-textured, cementitious fiber board is also a preferred alternative to wood products for commercial buildings.
 - v. Colors used on building exteriors should integrate a building's various design elements or features.
 - vi. Accent colors should use color combinations that complement each other.
 - vii. Softer, muted or earth-toned colors are preferred; however, brighter colors may be approved when contextually appropriate.
 - viii. Use accent colors in a way to enhance or highlight building design, and not in a manner that creates clutter or otherwise detracts from building design.
 - viii.ix. Use color and texture of building materials to provide a transition from natural features such as vegetation and streams to the built environment.

No changes to remainder of chapter

No changes to subsections A-O**P. Master Planned Development (MPD).**

1. Purpose. The purpose of this section is to provide a mechanism to allow the master planning of sites where development is proposed to occur in phases, where coordination of public facilities is needed, when a master plan is needed to determine how best to develop the area, when a master plan is needed to integrate various uses, or when multiple ownerships are to be coordinated into a unified development. The MPD process establishes conditions of approval for all concurrent and subsequent development applications; and thereby ensures that infrastructure, public services, and open space and recreation areas will be provided in a timely manner and be tailored to the MPD site. The MPD process also provides long-term guidance for a large area so that the continuity of development is maintained.
2. Applicability. MPDs are:
 - a. Allowed in all zones for projects encompassing at least three acres (for multifamily, commercial, and mixed use) or 50 dwelling units (for single-family);
 - b. Required in the Overlake Village Subarea and Northeast Design District for all projects encompassing at least three acres;
 - c. Optional in the Overlake Village Subarea and in Downtown zones for projects encompassing less than three acres;
 - d. Required in the East Sammamish Valley area pursuant to RZC 21.08.190.B, East Sammamish Valley Master Plan Requirement; and
 - ~~e. Required in the Southeast Redmond neighborhood pursuant to RZC 21.08.200.B.3, Master Planning Requirement.~~
3. Scope of Approval. The MPD approval shall constitute a limitation on the use and design of the site.
 - a. MPD Term. Development plans may include multiple phases to be developed successively over a period of no more than five years (10 years for MPDs located in Overlake Village and the Northeast Design District and MPDs greater than 10 acres in Downtown). If after this time period uncompleted phases remain, the applicant may request of the Technical Committee one extension of no more than five years. The Technical Committee may grant the extension if the applicant demonstrates economic hardship, change of ownership, unanticipated construction and/or site design problems, or other circumstances beyond his/her control determined acceptable by the Technical Committee. The MPD approval shall expire no more than 10 years from the original approval (15 years for MPDs located in Overlake Village and the Northeast Design District and MPDS greater than 10 acres in Downtown). If

an MPD is accompanied by a development agreement, the applicant shall have the option of having the term of the MPD coincide with that of the development agreement, even if the term of the development agreement exceeds the ordinarily allowable MPD timeframe.

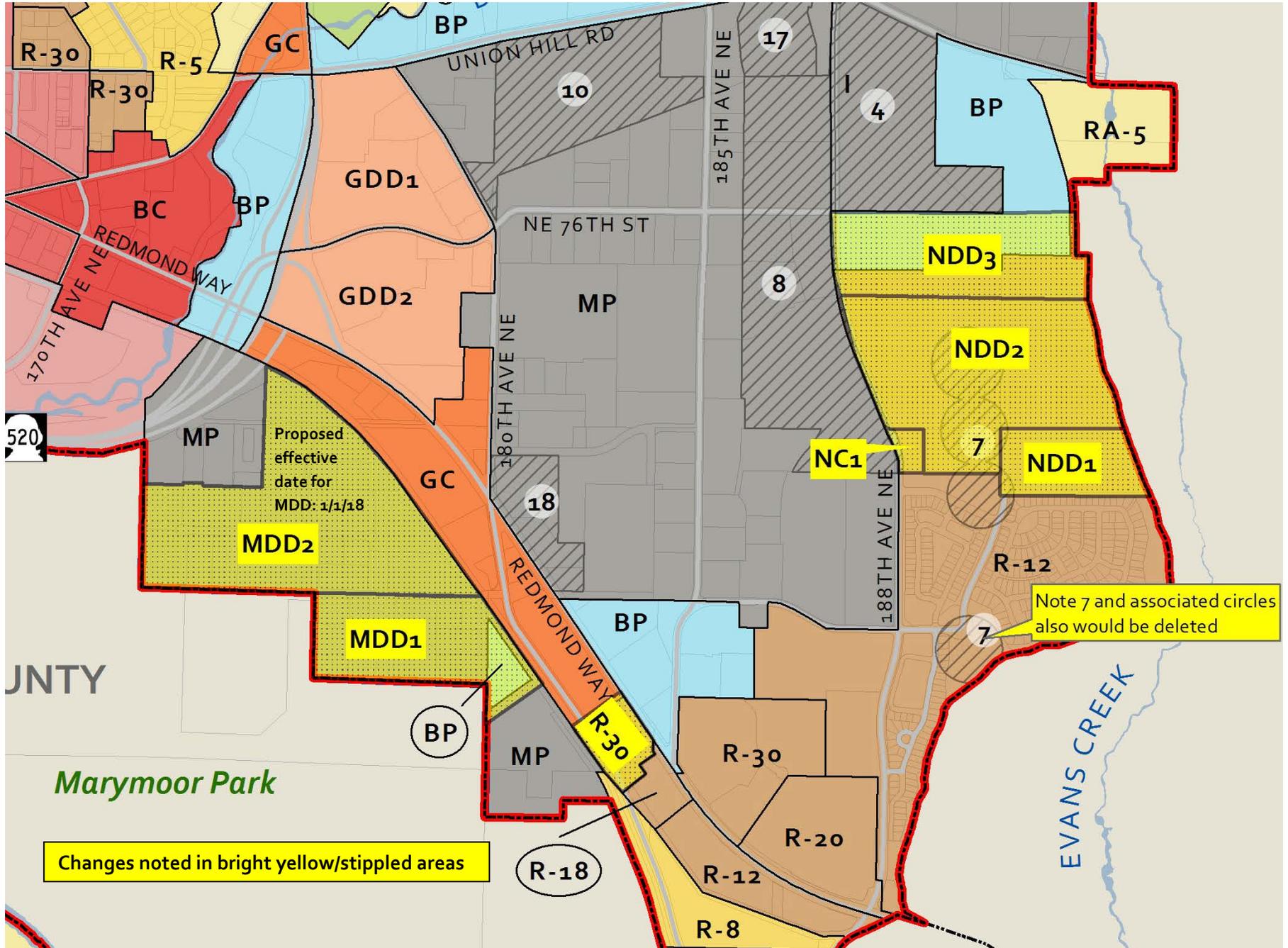
- b. MPD and Subdivision. An MPD that requires platting shall not receive final plat approval until the City has granted an MPD approval.
 - c. Approval Process. The approval process includes the City's review and consideration of the general project concept, including its intensity and overall design. Each land use permit associated with the MPD would then relate to specific site and development requirements as defined by the approval and the RZC.
4. Procedures. MPDs shall be processed using the following procedures:
- a. MPDs in the Overlake Village Subarea and the Northeast Design District that are larger than three acres in size and MPDs in the Downtown that are larger than ten acres in size shall follow a Type V process as set forth in RZC 21.76.050.J.
 - i. A recommendation from the Design Review Board shall be required.
 - ii. The applicant shall host two neighborhood meetings: one early in the PREP process if PREP is used, and a second pursuant to RZC 21.76.060.C. The second neighborhood meeting shall be held no later than 60 days before the public hearing.
 - iii. MPD approval extensions and MPD amendments that meet the criteria for administrative modifications shall be reviewed under RZC 21.76.090, Post-Approval Actions.
 - b. All other master planned developments shall follow the process that is followed for the underlying land use permit. For example, an MPD that accompanies a site plan entitlement would follow a Type II process.
 - i. A neighborhood meeting to gather public input shall be held prior to the applicant making a formal application for the underlying land use permit.
 - ii. MPD approval extensions and MPD amendments that meet the criteria for administrative modifications shall be reviewed under RZC 21.76.090.D, Administrative Modifications.
 - c. A Master Plan shall be completed prior to approval of any subdivision, binding site plan, or issuance of land use permit approval for any development. The following actions are exempt from this requirement:
 - i. Alterations to a building that qualify for review as an Administrative Modification under RZC 21.76.090.D.

- ii. Public projects, such as parks, utility, and street improvements, including subdivision of property for land acquisition, or acquisition of other property rights required for such projects.
 - iii. Actions exempt from subdivision requirements as listed in RZC 21.74.010.B.2.
 - iv. Relocation of structures displaced by public projects.
5. Decision Criteria. Master Planned Developments shall meet the following criteria:
- a. All elements of the MPD shall support and be consistent with the RZC and all applicable Comprehensive Plan policies.
 - b. MPDs proposed in the Overlake Village Subarea shall be consistent with the Overlake Master Plan and Implementation Strategy, and shall include the items listed in 5.c below in addition to the following:
 - i. A height and bulk study that demonstrates how building mass, height, and scale relate to open spaces, pedestrian pathways, streets, and other buildings;
 - ii. An analysis of shading effects of taller buildings (for sites smaller than three acres, only required if the Technical Committee or Design Review Board determine based upon the height and bulk study that analysis of shading effects is needed); and
 - iii. Phasing plan for bonus features and affordable housing component showing that the completion of improvements of bonus features and affordable housing shall be commensurate with the progress on the construction of the development (for sites smaller than three acres, only required if the Technical Committee determines necessary).
 - c. All MPDs shall include the items listed below:
 - i. A design concept that is in conformance with all applicable Comprehensive Plan policies and development regulations;
 - ii. Conceptual site plan indicating all proposed land uses (architectural design, exact building shapes, locations, and other detailed information required in a site plan shall not be required);
 - iii. Transportation and circulation plan indicating the layout and conceptual design of all streets, pedestrian pathways, parking, and location of transit facilities (as available), in plan view and cross section for streets (cross sections only required for projects in the Downtown);
 - iv. Location of proposed space for parks, open space, and any cultural facilities;

- v. Phasing plan describing anticipated time frames for development and showing that completion of affordable housing shall be commensurate with the progress on the construction of the development;
 - vi. Location of any environmentally critical areas;
 - vii. Landscape and tree retention concepts, including consideration of the health and structural stability of retained trees, as determined by an arborist report;
 - viii. Preliminary plan indicating required connections to adjacent properties for transportation and open space systems;
 - ix. Overall approach to sustainable design, including consideration of the use of environmentally sustainable materials such as permeable pavement, where possible; and
 - x. Preliminary plan for other major infrastructure improvements (may be waived by the Technical Committee for sites in Overlake smaller than three acres).
- d. The Master Plan must comply with all site requirements or design guidelines that would ordinarily apply to projects developed in the underlying zone.
- e. Property included in an MPD must be under the same ownership, or there must be a signed agreement establishing control over multiple ownerships.
6. Vesting. Where MPDs are required, they must be completed in conjunction with a development agreement, as described in RCW Chapter 36.70B, in order to vest to development regulations in place at the time of the agreement. Where MPDs are optional, applicants wishing to vest may pursue a development agreement as described in RCW Chapter 36.70B.
7. Nothing in this section shall preclude the acquisition of land prior to application or approval of a master plan.

No changes to remainder of chapter

Exhibit B: Recommended Zoning Map



Recommended Southeast Redmond Neighborhood Plan Policies

Reflects Planning Commission discussion through March 19, 2014 and limited staff-proposed changes

Neighborhood Vision

Southeast Redmond is a neighborhood in which people enjoy living, working, recreating, shopping and more. Its diverse land uses coexist for the benefit of residents and employees, and long-term investments in transportation, parks, and the environment have improved the quality of life for everyone.

With features such as gateways and places that celebrate the neighborhood, Southeast Redmond has maintained a strong sense of identity and inclusiveness. The neighborhood is vibrant and provides a variety of choices for living, working, shopping and more. Southeast Redmond has attracted a variety of new employers and is economically significant and at the same time is strongly connected to the natural environment given its location at the edge of the urban growth area and its proximity to Bear Creek, Evans Creek, Lake Sammamish, and Redmond's drinking water aquifer.

Investment in public infrastructure has established a network of efficient connections to help people travel between transit centers, residential areas, entertainment, employment areas and other neighborhood destinations.

Overall, the Southeast Redmond neighborhood:

- Provides a high quality place for people to live and work in close proximity to Downtown with convenient access to transit;
- Offers a variety of opportunities to live, learn, invest and work as a complement to Redmond's Downtown and Overlake urban centers;
- Includes many modes of safe and convenient transportation from light rail to long-haul transport to walking and bicycling;
- Maintains its unique identity and neighborhood character through sound investments in the built environment, innovative and sustainable infrastructure, and natural areas;
- Provides a variety of opportunities for people to engage and connect, including through stewardship projects and at neighborhood gathering places; and
- Preserves long-standing business opportunities as well as a spirit of entrepreneurship through which new businesses can locate, adapt and grow within the neighborhood.

Neighborhood Character

Southeast Redmond is located at Redmond's eastern edge, which is also the edge of King County's urban growth area. The incorporated part of the neighborhood is generally bounded by Bear Creek and Evans Creek on the north, the City limit on the east, Lake Sammamish and Marymoor Park on the southwest, and SR 520 on the west. Marymoor Park is unincorporated though is part of the neighborhood for planning purposes.

The neighborhood includes six distinct subareas as shown in Map N-SE-2.

1. The **Marymoor Subarea**, located southwest of Redmond Way and adjoining Marymoor Park, today features an eclectic mix of manufacturing, education, distribution, research and development, and religious uses. This is the future home of the Southeast Redmond Light Rail Station and so this subarea will serve a significant number of regional commuters in addition to local travel.
2. The **Regional Retail Subarea**, located between SR 520 and 178th/180th Ave NE and north of Redmond Way, is characterized by large regional retail uses and research and development. While most people access these services by car, a number of people around the neighborhood access them on foot despite the limited quality of the pedestrian environment. Efficient vehicular travel along 178th/180th Ave NE and NE 76th St is important to help support local businesses.
3. The **Redmond Way Subarea** features a variety of residential, goods and services, and employment-based land uses and in combination with East Lake Sammamish Parkway serves as a significant gateway to the neighborhood and City. Today, Redmond Way divides the neighborhood for pedestrians so this plan focuses on creating a variety of effective and safe travel choices that help people move comfortably through the neighborhood.
4. The **Central Subarea** represents the neighborhood's manufacturing, warehousing, and distribution core. Between Redmond Way on the south and Bear Creek on the north, this subarea provides the bulk of the neighborhood's employment opportunities in settings ranging from office parks to large-scale manufacturing and distribution facilities. Freight traffic is significant in this subarea and is expected to continue to be important throughout the life of this plan.
5. The **Northeast Subarea** is Redmond's industrial core. Careful siting of buildings, vegetated buffers and parks, and campus settings will help support a transition from single-family residential areas in the south to heavy industrial areas in the north. This subarea includes Southeast Redmond's neighborhood park which over time will provide a variety of amenities to help meet the needs of neighborhood residents and those that work in the vicinity. Non-motorized connections and trails will also help people connect with parks and regional trails located to the east of the neighborhood.

6. The **Evans Creek Subarea** is predominantly residential. It includes the largest concentration of single-family homes in Southeast Redmond – Woodbridge – as well as a number of large multi-family homes closer to Redmond Way. This is also the most recently-developed portion of Southeast Redmond, having been developed in the early 2000s. The east edge of this subarea abuts the rural area and, though nearby to manufacturing and industrial uses, is noticeably quieter.

CC-1	Use entryway elements to foster Southeast Redmond’s neighborhood character and welcome people to Redmond. For example, consider an entryway element at Redmond Way and East Lake Sammamish Parkway.
CC-2	Plan for and provide opportunities for art throughout the neighborhood. For example, ensure opportunities in the planning process for including art as part of the future light rail station and park and ride facility, consider placing art at City and neighborhood entryways, promote the installation of art at private entryways such as for businesses and commercial uses, and consider opportunities for display of rotating and local art.
CC-3	Maintain opportunities for continued diversity of residential, employment, shopping, recreation, and other activities.
CC-4	Strengthen the neighborhood’s connection to the natural environment such as by incorporating sustainable stormwater solutions.
CC-5	Plant and maintain trees and vegetation along the bluffs above the Red Brick Road to buffer views of urban development from the road and adjoining rural uses.
CC-6	Protect the historic significance of the Red Brick Road (196 th Ave NE) by minimizing new vehicle access points.
CC-7	Promote neighborhood communication and inclusiveness such as through neighborhood meetings, checking-in regarding neighborhood priorities identified in the neighborhood plan’s “Third Document,” and other opportunities for people to remain informed and share feedback regarding City and neighborhood topics.

Natural Environment

Southeast Redmond is located adjacent to Bear Creek, Evans Creek and Lake Sammamish, and is situated over Redmond’s drinking water aquifer. Because of Southeast Redmond’s proximity to the aquifer it is especially important here to focus on retaining the aquifer’s high quality and volume. Redmond has citywide policies and regulations in place to protect and enhance the quality of these natural assets. This Neighborhood Plan emphasizes how people who live, own businesses and work in the neighborhood can partner with each other and with the City to contribute to a clean and healthy local environment. For example, policies address innovative approaches to development that implement Redmond’s sustainability principles.

NE-1	Continue to work cooperatively with neighborhood businesses to develop mutually beneficial approaches to environmental issues such as stormwater management.
NE-2	Require new development and redevelopment to include techniques that support clean aquifer recharge, as feasible.
NE-3	Retain aquifer recharge capacity and protect, manage and encourage vegetated areas to

	facilitate groundwater recharge.
NE-4	Encourage the use of less-hazardous chemicals by businesses and homeowners to reduce impacts to stormwater, groundwater, and streams. <u>Employ targeted outreach to urge the use of less-hazardous chemicals by businesses and homeowners to reduce impacts to stormwater, groundwater, and streams.</u>

Bear Creek and Evans Creek are important salmon habitat and also provide beauty and recreation for residents and employees. Planning for salmon recovery happens mainly at the regional level, but local actions can still improve salmon habitat.

NE-5	Support City and regional actions to protect and restore Bear and Evans Creek for habitat, natural beauty and peaceful recreation.
NE-6	Provide opportunities for neighborhood stewardship of the natural environment through a variety of strategies such as creek side planting and home and business best management practices.

Much of Southeast Redmond developed without modern stormwater infrastructure. In addition, large parts of the neighborhood are low lying, causing frequent flooding during the wet season. Large-scale ponds and vaults in low-lying areas with high water tables can be difficult to develop, making low-impact techniques attractive.

NE-7	Reduce impacts from stormwater runoff by, for example, limiting impervious surface area, using site-appropriate low impact development practices, or using other retrofit techniques. <u>Especially in the Marymoor Subarea, shallow groundwater and lack of a stormwater outfall will require creative designs on the part of developers. To facilitate that, design regulations to allow developers to work within the natural constraints of the land.</u>
NE-8	Apply natural drainage (low impact development) or other techniques for new right-of-way where appropriate, such as in low-lying areas near Marymoor Park, to reduce the need for centralized stormwater management systems where such systems would be challenging to develop.

Having residential and industrial uses in close proximity in the eastern part of the neighborhood has caused friction over time. Residents and business representatives have often developed solutions themselves. This plan calls for continued cooperation as well as zoning standards that reduce friction points.

NE-9	Facilitate collaboration between businesses and residents to address dust and odor issues.
NE-10	Implement green infrastructure, such as habitat areas and urban green spaces, that helps mitigate issues regarding dust and odors. Use neighborhood stewards to help achieve this.

Land Use

The Southeast Redmond neighborhood is Redmond’s most diverse neighborhood in terms of land use, and will likely carry that distinction for many years to come. There are six subareas, all described earlier: Marymoor, Regional Retail Subarea, Redmond Way, Central, Northeast, and Evans Creek. These areas combined constitute a neighborhood with a high quality of life with places to live, work, shop, and recreate. This Neighborhood Plan continues that direction while planning for significant change in the Northeast and Marymoor subareas.

Southeast Redmond will continue to be Redmond’s manufacturing center, and will also welcome additional retail and service businesses, especially in existing commercial centers and near the light rail station. Walkable destinations such as small-scale commercial businesses will help create additional employment opportunities, strengthen the local economy, and offer places for community gathering and culture.

Unlike Downtown and Overlake, Southeast Redmond is not one of Redmond’s urban centers. Among other things, that means that the plan for Southeast Redmond accommodates uses at lower levels of intensity and continues to support more-intense manufacturing and industrial uses. Even so, this plan incorporates transit-oriented development to take advantage of the future light rail station in Southeast Redmond.

LU-1	Grow and support Southeast Redmond’s variety of activities and land uses. Through this, support Redmond’s sustainability pillars of strong employment, a healthy environment, and social equity.
LU-2	Maintain land use and zoning that, in addition to residential areas, supports a variety of business types and employment opportunities, fostering neighborhood resiliency particularly in the context of economic changes.
LU-3	Complement Redmond’s urban centers by planning for mid-density residences, regional retail, neighborhood retail, manufacturing and business parks, and industrial uses.

Housing

Few opportunities remain for new single-family development in Southeast Redmond. Still, as opportunities become available, innovative housing types should be encouraged to provide additional housing choices. There is considerable opportunity for additional townhome- and apartment-style living in the neighborhood, such as near the future light rail station, and it is important to the neighborhood to ensure that these opportunities are available to people at a range of income levels.

LU-4	Encourage alternative single-family housing types, such as cottages, small-lot short plats, and single-family attached homes to add to the variety of single-family housing types in Southeast Redmond.
LU-5	Ensure housing opportunities for those at a variety of income levels by requiring that a minimum of 10 percent of the units in all new developments of 10 units or more be affordable to individuals or families earning 80 percent or less of the King County median income. Provide at least one bonus market rate unit for each affordable unit.

	As part of any rezone that increases residential capacity, require an additional portion of the units to be affordable to low- and moderate-income households.
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Economic Vitality

Southeast Redmond is Redmond’s principal location for manufacturing and the only part of Redmond that allows industrial uses. As the nature of manufacturing evolves over time, it will be important for Southeast Redmond to be an attractive destination for business types that may not exist today. To that end this plan focuses on flexibility in zoning and making land available for these kinds of uses.

LU-6	Foster new opportunities for businesses by zoning land in the Northeast Subarea for employment uses.
LU-7	Use performance zoning in the Northeast Subarea and Marymoor Subarea to attract a variety of new businesses and employment activities such as business campuses, and reduce negative impacts to nearby residents.
LU-8	Retain opportunities for industrial businesses in the northeast corner of the neighborhood.

Subarea Land Use Policies

Regional Retail Subarea

The Regional Retail Subarea is located at a major transportation crossroads and so is highly visible. Because of its size, the nature of surrounding development and proximity to transportation corridors, the Regional Retail Subarea is an appropriate location for a mix of business and commercial uses, including regional retail/wholesale uses; general, professional and mixed offices; business park uses; hotel/motels; corporate headquarters offices; and support services.

LU-9	Allow high technology research and development facilities; associated light assembly and warehousing; other manufacturing uses with similar character, intensity and impact; support services; regional retail/wholesale uses; office uses, including corporate headquarters and regional offices; and hotels/motels.
LU-10	Encourage development that is sensitive to natural features and that will enhance the entryway to the city.
LU-11	Restrict uses that tend to locate in smaller commercial spaces; encourage those uses to locate in other commercial zones or in Downtown or Overlake mixed-use zones.
LU-12	Continue to preserve the tree stand at the south end of the Regional Retail Subarea adjacent to Redmond Way.
LU-13	Use building materials that provide a pleasing transition from trees and other natural features through the use of color and texture.
LU-14	Minimize views of large areas of asphalt by using landscaping, berms, building placement or other effective techniques.
LU-15	Maintain one or more significant architectural entry features to serve as landmarks, including an area for public art and a feature recognizing the Snoqualmie Tribe’s historic use of the property.

LU-16	Provide for safe and comfortable pedestrian and bicycle circulation within the subarea and to and from the subarea.
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Marymoor Subarea

The Marymoor Subarea is home to a wide variety of uses including high-tech manufacturing, graphics production, schools and colleges, video game research and development, catering, breweries, fitness centers, and religious facilities. It also is a low lying area close to Bear Creek and Lake Sammamish, creating opportunities to embrace the natural environment and redevelopment challenges due to high groundwater.

The area's proximity to Marymoor Park combined with the extension of light rail will create pressure for change. The light rail will bring increased local and regional transportation options and opportunities for transit-oriented development. Land use changes will create opportunities for people to live, work and shop near transit. Recognizing that, this plan emphasizes supporting businesses as they grow and change, including space for artists, and creating opportunities for new housing near Marymoor Park. It also recognizes the need for multidisciplinary infrastructure planning in the near future to support the vision for this area. The vision is for a walkable, denser subarea that features opportunities for living, employment, community gathering, education, shopping, and commuting to other Redmond and central Puget Sound destinations.

LU-28	Provide funding for and complete an interdisciplinary infrastructure plan for this subarea given the change in land use that is expected over time. Include utilities, parks, transportation and parking strategies, and land use standards such as minimum density as part of the plan. Also, ensure that the subarea supports adjacent natural areas, including Bear Creek, the aquifer, and the Sammamish River. Complete this study prior to having updated zoning regulations go into effect for the Marymoor Subarea.
LU-17	Support the extension of light rail to Southeast Redmond as shown in Map TR-1. Leverage the investment in light rail to create a walkable subarea with ample connections to Marymoor Park, local and regional transit, and the rest of the neighborhood.
LU-18	Use zoning regulations and public investments to facilitate opportunities for housing, employment, community gathering, education, and small-scale shopping in this subarea.
LU-19	Support business growth and adaptation in this subarea by implementing zoning that emphasizes performance standards over use standards. Cap general retail and service uses to encourage the location and growth of businesses in primary industries.
LU-20	Focus employment growth nearest the light rail station. Focus residential growth near Marymoor Park. Accommodate at least 700 new homes in the subarea or other parts of Redmond to offset reductions in residential capacity in the Northeast Subarea.
LU-21	Incorporate housing into the Marymoor Subarea that is walkable to the station. Focus on the areas closest to Marymoor Park. Provide sufficient zoning capacity to make up for lost capacity in the Northeast Subarea.
LU-22	Prepare station area plans in cooperation with Sound Transit and other stakeholders to guide updates to policies and implementation measures. Key opportunities include creating opportunities for transit-oriented development and developing a multimodal transportation

	system.
LU-23	Design new structures adjacent to Marymoor Park to take advantage of the Park as an amenity, such as by creating connections to the park, placing common areas near the park, or facing windows onto the park.
LU-24	Soften the transition between the Marymoor Subarea and Marymoor Park while maintaining views from the subarea into the park.
LU-25	Improve wayfinding to key nearby destinations such as Marymoor Park, the light rail station, East Lake Sammamish Trail, and the Redmond Central Connector.
LU-26	Improve subarea connectivity and light rail station access by planning a grid of public, complete streets and pathways as shown in Map N-SE-3. Ensure that blocks are generally 400 feet or less in length, recognizing that Map N-SE-3 shows street and pathway connections in conceptual locations. Use the public realm as an opportunity to introduce additional natural character and green infrastructure to the subarea.
LU-27	Plan for a grade-separated crossing over Redmond Way near the station area to provide better non-motorized access to the station from the northeast.

Redmond Way Subarea

The Redmond Way Subarea is the commercial corridor that extends along the south side of Redmond Way from the SR 520 interchange to just beyond the intersection with 180th Ave NE. A portion of the commercial corridor extends to the north side of the street around 180th Ave NE. This corridor is characterized by a wide variety of commercial uses that are largely dependent on vehicle traffic. These uses serve both local neighborhood traffic and regional traffic passing through Southeast Redmond.

The need for auto-oriented commercial uses here is expected to continue as the area around it continues to grow. This plan continues the focus for general retail uses along Redmond Way and discourages expansion of retail uses into the Central Subarea so as not to erode Redmond's land base for manufacturing and related uses.

LU-29	Maintain zoning that allows for general retail uses in the Redmond Way Subarea to serve both local and regional users.
LU-30	Restrict land uses, such as major office and walkup retail, which are more appropriate for Redmond's urban centers.
LU-31	Reserve land for manufacturing and related uses in the Central Subarea.

Central Subarea

The Central Subarea is Redmond's manufacturing and distribution hub, and is an important jobs location. Although natural features are not immediately obvious, the aquifer under this subarea is particularly productive. The subarea extends east from the Regional Retail Subarea to 188th Ave NE and north from Redmond Way to Bear Creek and Evans Creek. Most of the subarea is zoned Manufacturing Park, with some portions zoned Business Park. Among other things, businesses in this subarea manufacture industrial equipment, distribute parcels, cater events, develop new software products, manage food service operations, and manufacture aircraft equipment.

LU-32	Continue to encourage manufacturing, distribution, and industrial uses in the Central Subarea.
LU-33	Support the Central Subarea as a significant jobs location in the following ways: <ul style="list-style-type: none"> • Support citywide partnerships with business and community interests such as OneRedmond • Monitor changes in the economy and adjust zoning regulations as needed • Restrict incompatible uses in this area, such as standalone housing, general retail, and mixed-use developments

Northeast Subarea

The Northeast Subarea features land use and zoning that transitions from single-family residential in the south to manufacturing and industry in the north in order to provide opportunities for land-intensive employment uses and buffer residential areas from industrial areas. Over time this area will demonstrate techniques for successfully locating living and working opportunities in close proximity of one another.

LU-34	Maintain a Northeast Design District performance zone as shown in Map LU-1 that fosters opportunities to live, work and recreate in close proximity and in a manner that appropriately transitions between land uses of different intensities. For the Northeast Design District maintain development regulations to achieve the following: <ul style="list-style-type: none"> • Accommodate 140-170 dwellings east of Southeast Redmond Neighborhood Park and south of the 7100 block at a density of approximately 12-18 units per acre. In this same area allow recreation uses and medium-intensity business park uses. • Accommodate a broad range of business park activities in the balance of the Northeast Design District, including those business park activities allowed in the above bullet as well as manufacturing and outdoor storage.
LU-35	Require master planning for new development in the Northeast Design District.
LU-36	Maintain Industry zoning in the northern part of this subarea.
LU-37	Provide a variety of mobility choices and connections within this subarea including non-motorized connections to the Bear-Evans Creek Trail system and multi-modal routes to the north and west to provide a grid-based travel network. Ensure that pedestrian and bicycle connections, which may be part of multi-modal connections, are generally no more than 500 feet apart. Complete the connections generally as shown on Map N-SE-3, recognizing that locations shown on the map are conceptual.
LU-38	Establish vegetated street and non-motorized connections in the Northeast Design District to provide for additional buffering between land uses of different intensities.
LU-39	Design north-south streets to avoid creating direct visual corridors from low-intensity to high-intensity areas in the Northeast Design District.
LU-40	Plan for and design the 192 nd Avenue NE corridor between NE 68th Street and Union Hill Road as follows, moving from south to north: <ul style="list-style-type: none"> • Residential green corridor: where 192nd Ave NE is adjacent to residential development in the southern portion of the design district, incorporate high-comfort pedestrian and bicycle facilities, broad street-side planting strips, canopy-forming and large-

	<p>species vegetation, and narrow vehicular travel lanes</p> <ul style="list-style-type: none"> • Design the intersection of 192nd Ave NE and the first east-west street north of Woodbridge to encourage through traffic to turn west toward 188th Ave NE instead of entering Woodbridge. • Campus green corridor: where the street is adjacent to moderately-intense uses, incorporate supportive, multi-modal infrastructure including comfortable facilities for pedestrians and bicyclists, moderately-wide street-side planting strips, a variety of vegetation including canopy-forming and large-species vegetation that increases in density from north to south, a vegetated median, and vehicular travel lanes. • Industrial corridor: where the street is adjacent to higher-intensity uses, support efficient travel for large and heavy vehicles associated with industrial and manufacturing land uses through a design that meets citywide arterial street standards. <p>Emphasize east-west non-motorized and multimodal connections along the length of 192nd Avenue NE to promote walking and bicycling and to provide connections to regional trails and to high capacity transit services.</p>
LU-41	Design and site buildings, landscape features, or other elements to establish structural buffers between land uses with different intensities. Include dense, multistory, evergreen landscaping to enhance the buffering effect.
LU-42	Select and maintain species for landscaping in residential areas to provide vegetation that is tiered and multistory at maturity and will supplement buffers and transitional areas.
LU-43	Incorporate design elements and amenities that foster a sense of place and neighborhood character in new developments in the Northeast Design District. Include elements such as pedestrian-scale street lights, seating along sidewalks and trails, pocket parks and children’s play areas, small outdoor recreation areas such as basketball courts, open areas for impromptu recreation such as volleyball, artistic features, and landscaped entryways.
LU-44	Encourage development of community gardens that are in close proximity to residences.
LU-45	Maintain Neighborhood Commercial land use designation and zoning immediately north of Southeast Redmond Neighborhood Park.

Evans Creek Subarea

The Evans Creek Subarea is the residential portion of Southeast Redmond. It is generally located in the southeast corner of the neighborhood. Homes here – both single-family and multi-family – are located within a short commute to thousands of jobs. Most homes were built in the 1990s and 2000s, making homes in this neighborhood relatively new compared to other parts of Redmond. This subarea is desirable for residents for these and other reasons. At the same time, many homes are near manufacturing and industrial operations that have operated for decades. This plan emphasizes improving residential quality of life while retaining and attracting businesses to Southeast Redmond.

LU-46	Use building siting, street alignment, on-site landscaping, vegetated buffers and other techniques to improve buffers between residential and higher-intensity uses.
LU-47	Provide, where feasible, additional vegetation in street-side planter strips of 188 th Avenue NE from Redmond Way to NE 68 th Street.

LU-48	Encourage developers and builders to cluster residential development away from existing manufacturing and industrial uses.
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Transportation

Southeast Redmond is at the confluence of four regional commute routes: SR 520, Avondale Road, Union Hill Road, and Redmond Way. In addition it is a hub for parcel distribution activity. During peak hours these routes can be severely congested.

Light rail will reorient how thousands of regional commuters, Southeast Redmond residents, and Southeast Redmond employees use the transportation system in Southeast Redmond. This plan leverages the light rail investment and focuses on improving the travel choices, consistent with citywide transportation policy and the Transportation Master Plan.

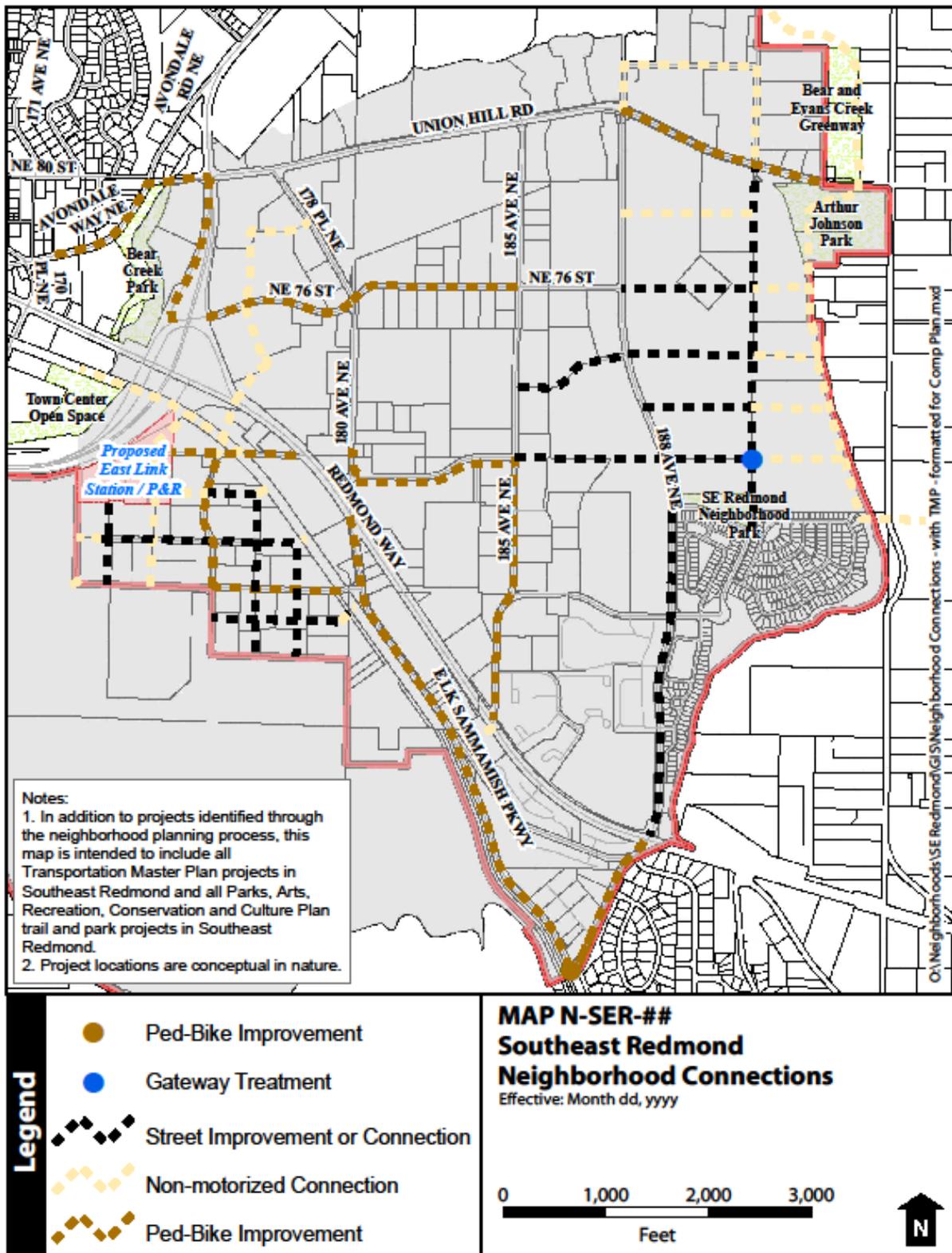
TR-1	Work with transit agencies and other partners to improve transit service as an alternative to driving alone, especially in the eastern part of the neighborhood where there is less transit service. Pursue improvements to local routes, access to regional routes, and park and ride access, and explore alternative transit service options.
TR-2	Explore and implement creative ways to reduce conflicts between vehicles and non-motorized street users.
TR-3	Find ways to enable pedestrians and bicyclists to safely cross Redmond Way to access homes, transit, jobs, services, and Marymoor Park. For example, encourage pedestrians to use shorter crossings, such as at NE 70 th St, or install grade-separated crossings.
TR-4	Manage congestion by, for example, using technology and making efficient use of existing infrastructure, so that the travel network overall continues to function.
TR-5	Ensure that arterials provide convenient and safe pedestrian crossing opportunities.
TR-6	Support long-term improvements to mitigate congestion at the end of SR 520, including at the interchange with Redmond Way and the intersection at Union Hill Road.

The lack of connectivity is a significant mobility issue for people regardless of their mode of travel. Creating new connections will help distribute traffic and give people more options for getting to where they want to go. It will be especially important to improve east-west connectivity to/from the light rail station since it will attract local and regional traffic. Creating a grid network of streets in the Marymoor subarea will help achieve this.

TR-7	Improve connectivity in the neighborhood by planning for and constructing multi-modal improvements as shown in Map N-SE-3. Prioritize improvements that improve the quality of connections from the Central Subarea to the Redmond Way and Marymoor Subarea for commuting and mid-day trips.
TR-8	Work with Sound Transit and others to encourage regional commuters to use the planned Southeast Redmond Park and Ride to access transit.
TR-9	Partner with Sound Transit and King County Metro to ensure high-quality multi-modal access to the station area, especially considering commuters who will need access to the regional street network.

As an industrial and distribution hub, Southeast Redmond experiences significant truck traffic. It will continue to be important to support the movement of freight while minimizing impacts on residential areas of the neighborhood. Pedestrians, bicyclists and others use Southeast Redmond streets and pathways for commuting and recreation. Without proper planning and design this can lead to conflict points. This plan envisions a neighborhood where all travel safely in the neighborhood.

TR-10	Promote through, general vehicular and truck travel on principal and other high capacity arterials, which have higher vehicular volume and speed.
TR-11	Look for ways to increase the attractiveness of 185 th Ave NE and NE 76 th Street for trucks as an alternative to using 188 th Ave NE. Recognize that 188 th Ave NE is a truck access street, but that it abuts residential areas on two sides, and so consider design treatments that reduce negative impacts on residents while maintaining access for trucks.
TR-12	Condition future commercial development that relies upon heavy vehicles to use alternatives to 188 th Ave NE between Redmond Way and the 6800 block.
TR-13	Plan and implement multi-modal and dedicated north-south and east-west non-motorized corridors that provide separation between trucks and pedestrians and bicyclists. Ensure that crossings also provide safe separation for all users.
TR-14	Identify corridors with safety concerns and design solutions that safely accommodate all modes of travel. Consider as options redesigning the corridor or providing alternative routes for some modes.



Parks and Recreation

Since Southeast Redmond developed first as a manufacturing and industrial center it does not have a collection of neighborhood parks. Even so, Southeast Redmond is ringed on the north and east by a network of existing parks and trails and future trail connections that when complete will connect Southeast Redmond to a regional network of trails and open spaces.

Another unusual feature is that Southeast Redmond – for planning purposes – includes Marymoor Park in unincorporated King County, which is almost as big as the rest of the neighborhood. Even though the incorporated portion of the neighborhood shares a long boundary with Marymoor, it is not easily accessible, with only two entry points: one road and one trail entrance.

Given that context, this plan focuses on completing existing parks and trails and creating new opportunities to access Marymoor Park.

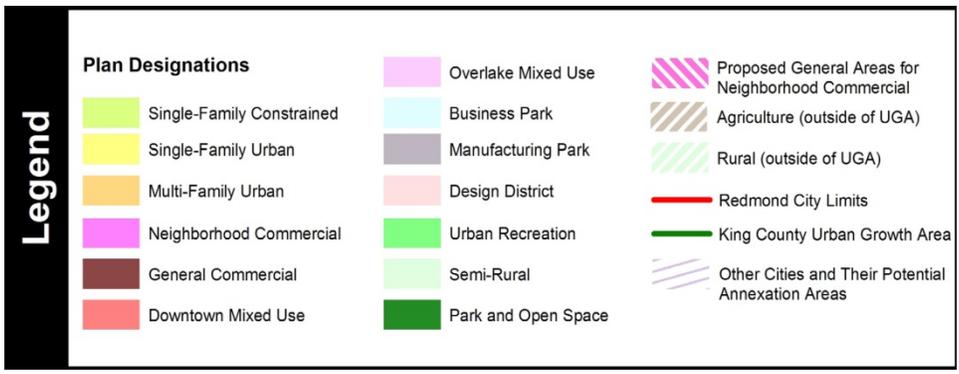
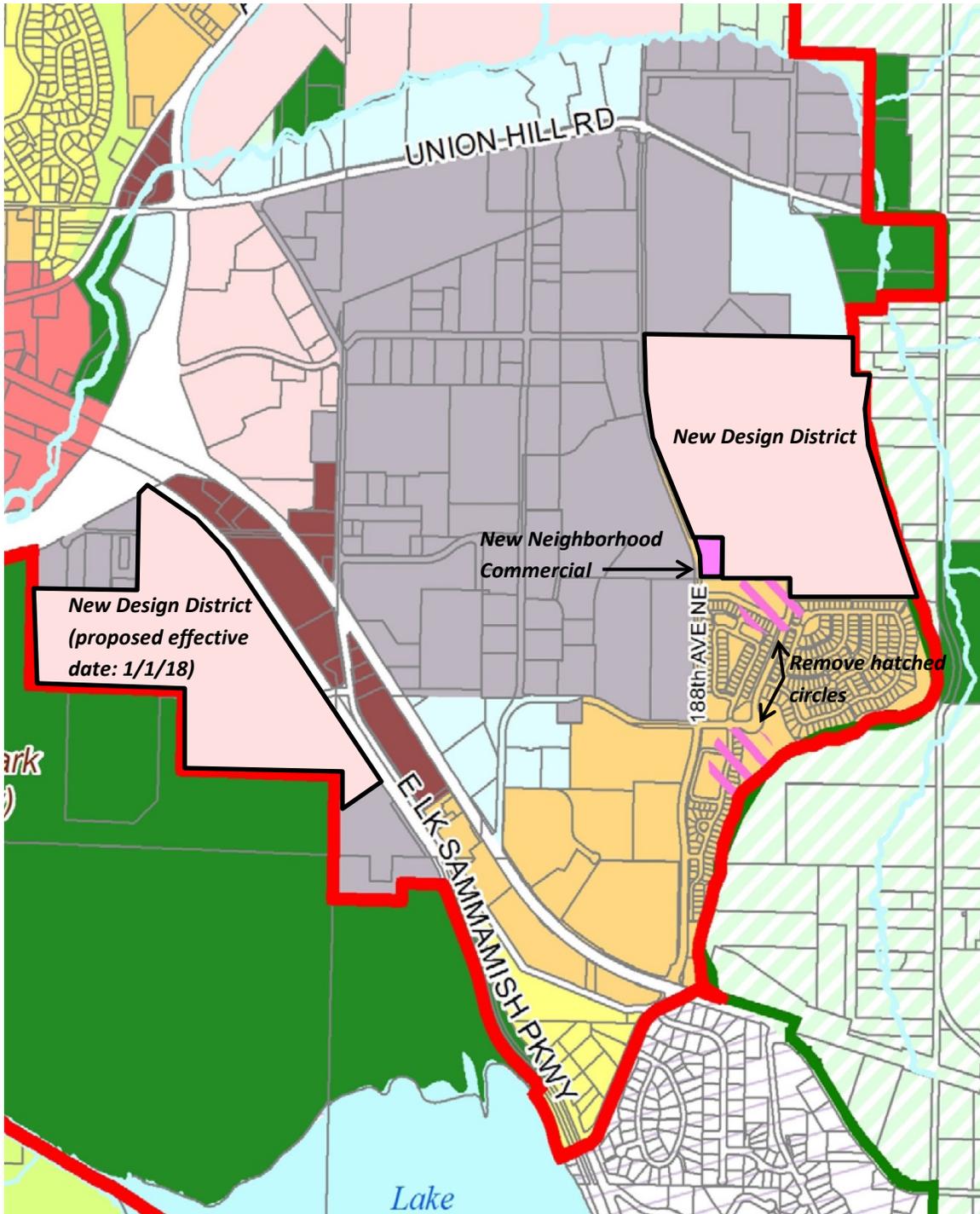
PR-1	Prioritize the development of Southeast Redmond Neighborhood Park. Expand the park if additional residential development warrants such expansion.
PR-2	Use Southeast Redmond Neighborhood Park and other existing and planned parks, trails, and community spaces to support opportunities for passive and active recreation for people of all ages and abilities. Include opportunities for community gardens and community gathering.
PR-3	Use parks and other open spaces to add green to the neighborhood, and to enhance stream health, the tree canopy, and other natural assets.
PR-4	Support planned trail connections to nearby parks, such as Arthur Johnson Park and Marymoor Park.
PR-5	Complete planned local and regional trail connections and create new connections between the planned trail network on the eastern edge of the neighborhood and the Northeast Subarea, as shown in Map N-SE-3.
PR-6	Complete the planned grade-separated connection across SR 520 between the north end of the East Lake Sammamish Trail and the Redmond Central Connector, recognizing its regional recreation and transportation significance. <u>Pursue an interim connection if needed. If a permanent connection is not complete prior to the extension of light rail to Downtown Redmond, ensure that the connection’s design and construction are coordinated and done in conjunction with the extension of light rail to Downtown Redmond.</u>

Neighborhood Gathering Spaces

People in Southeast Redmond value connections with one another. However, there are currently few places for people to gather outside their home or office. Additional gathering places such as plazas and pocket parks would provide a variety of opportunities for those who live, work, and visit Southeast Redmond to meet and to engage while building a strong sense of community.

CG-1	Encourage property owners and developers to integrate gathering places into their developments, such as a plaza outside coffee stores or pocket park as part of a new development
CG-2	Provide incentives for development of privately operated indoor/outdoor recreational facilities.

Exhibit D: Recommended Comprehensive Plan Land Use Map (excerpt)





STATE ENVIRONMENTAL POLICY ACT (SEPA) DETERMINATION OF NON-SIGNIFICANCE

Exhibit F

For more information about this project visit www.redmond.gov/landuseapps

PROJECT INFORMATION

PROJECT NAME: SE Redmond Neighborhood Plan Update

SEPA FILE NUMBER: SEPA-2014-00956

PROJECT DESCRIPTION:
Southeast Redmond Neighborhood Plan Update

PROJECT LOCATION: Southeast Redmond

SITE ADDRESS:

APPLICANT: Jeff Churchill
Kim Dietz

LEAD AGENCY: City of Redmond

The lead agency for this proposal has determined that the requirements of environmental analysis, protection, and mitigation measures have been adequately addressed through the City's regulations and Comprehensive Plan together with applicable State and Federal laws.

Additionally, the lead agency has determined that the proposal does not have a probable significant adverse impact on the environment as described under SEPA.

An Environmental Impact Statement (EIS) is not required under RCW 43.21C.030(2)(c). This decision was made after review of a completed environmental checklist and other information on file with the lead agency. **This information is available to the public on request.**

CITY CONTACT INFORMATION

PROJECT PLANNER NAME: Cathy Beam

PHONE NUMBER: 425-556-2429

EMAIL: cbeam@redmond.gov

IMPORTANT DATES

COMMENT PERIOD

Depending upon the proposal, a comment period may not be required. An "**X**" is placed next to the applicable comment period provision.

There is no comment period for this DNS. Please see below for appeal provisions.

'X' This DNS is issued under WAC 197-11-340(2), and the lead agency will not make a decision on this proposal for 14 days from the date below. Comments can be submitted to the Project Planner, via phone, fax (425)556-2400, email or in person at the Development Services Center located at 15670 NE 85th Street, Redmond, WA 98052. **Comments must be submitted by 06/25/2014.**

APPEAL PERIOD

You may appeal this determination to the City of Redmond Planning Department, Redmond City Hall, 15670 NE 85th Street, P.O. Box 97010, Redmond, WA 98073-9710, **no later than 5:00 p.m. on 07/10/2014**, by submitting a completed City of Redmond Appeal Application Form available on the City's website at www.redmond.gov or at City Hall. You should be prepared to make specific factual objections.

DATE OF DNS ISSUANCE: June 11, 2014

For more information about the project or SEPA procedures, please contact the project planner.

RESPONSIBLE OFFICIAL: Robert G. Odle
Planning Director

SIGNATURE: 

RESPONSIBLE OFFICIAL: Linda E. De Boldt
Public Works Director

SIGNATURE: 

Address: 15670 NE 85th Street Redmond, WA 98052

Exhibit G: Public Hearing Notice for July 9, 2014 Hearing

NOTICE OF PUBLIC HEARING CITY OF REDMOND

Southeast Redmond Neighborhood Plan Update, LAND-2014-00055

The City of Redmond **Planning Commission** will hold a Public Hearing in the **City Hall Council Chambers, 15670 NE 85th Street, Redmond, Washington** on **July 9, 2014 at 7:00 p.m.** or as soon thereafter as possible, on:

SUBJECT:

Proposed amendments to the Neighborhoods Element of the Redmond Comprehensive Plan and several chapters of the Redmond Zoning Code (RZC) in support of the Southeast Redmond Neighborhood Plan Update. The proposal includes amendments to the following chapters of the RZC:

- 21.04, General Provisions
- 21.08, Residential Regulations
- 21.14, Commercial Regulations
- 21.20, Affordable Housing
- 21.32, Landscaping
- 21.38, Outdoor Storage, Retail Display, and Garbage and Recycling Enclosures
- 21.50, Transition Overlay Areas
- 21.60, Citywide Design Standards
- 21.76, Review Procedures

The purpose of this update is to amend the policies and regulations that constitute the Southeast Redmond Neighborhood Plan. The update covers topics such as land use, natural features and the environment, neighborhood character and design, housing, transportation, and parks and open space. Among the main components of the recommended action are:

- Retaining land for manufacturing and industry;
- Recognizing the importance of Redmond's ground water and surface water and supporting citywide plans and policy to protect them;
- Planning for new residential and employment opportunities in the area north of Woodbridge;
- Planning for new residential and employment opportunities in the Marymoor subarea, leveraging the arrival of light rail;
- Improving mobility by all transportation modes by planning for new motorized and non-motorized connections;
- Prioritizing transportation corridors for efficient heavy-duty truck travel and freight transit;
- Promoting manufacturing and distribution related transportation on routes away from single-family residential areas;
- Supporting housing affordability and choice, similar to the approach in other neighborhood plans within Redmond, such as by allowing cottage and attached homes and by extending affordable housing regulations to Southeast Redmond;
- Completing planned trail connections and developing existing parks;
- Enhancing a sense of place and community through opportunities for gathering places, particularly as part of private development and design of the public realm;

Exhibit G: Public Hearing Notice for July 9, 2014 Hearing

- Renaming the Gateway Design District to the Regional Retail subarea while maintaining the same land use intent in neighborhood plan policies;
- Maintaining current zoning along Redmond Way, preserving the limits of the Manufacturing Park Overlay;
- Northeast Subarea: This area is proposed to be rezoned primarily for employment uses while retaining some capacity for additional residential development near Woodbridge. The plan calls for performance zoning to help in transitioning between different intensities in the employment area while emphasizing business park activities in the center of this subarea;
- Marymoor Subarea: The Marymoor subarea includes the future light rail station and park-and-ride facility. The proposed plan amendments call for rezoning this area to allow multi-family housing near Marymoor Park and potentially near the future station, and to support adaptive manufacturing and business spaces nearer the station and SR 520; the effective date of these changes to zoning and other regulations is proposed to be delayed until January 1, 2018;
- Redmond Way Rezone: about 6.0 acres zoned General Commercial and R-12 are proposed to be rezoned to R-30 on the southwest side of the intersection of Redmond Way and 185th Ave NE as part of this plan update, adding capacity for about 60 more homes than under current zoning; and
- Growth Potential Compared to Current Plan: The proposed plan adds about 48 acres of capacity for employment uses compared to the existing plan, and retains capacity for about 850 additional homes. Nearly all of this change is proposed in the Marymoor and Northeast Subareas as described above.
- Renaming Southeast Redmond neighborhood based on neighborhood input;

REQUESTED ACTION: Planning Commission recommendation on the proposed amendments. All persons are invited to comment in person at the hearing, or in writing prior to the hearing, to the Planning Department at City Hall, P.O. Box 97010, Redmond, Washington, 98073-9710. Comments may also be provided by e-mail to planningcommission@redmond.gov or by fax to (425) 556-4242. **Contact Kimberly Dietz at (425) 556-2415 or kdietz@redmond.gov and Jeff Churchill at (425) 556-2492 or jchurchill@redmond.gov for more information.**

A copy of the proposal is available from the Planning Department, 4th Floor of City Hall and also on the web at www.redmond.gov/planningcommission. If you are hearing or visually impaired, please notify the Planning Department at (425) 556-2440 one week in advance of the hearing in order to be provided assistance.

LEGAL NOTICE: JUNE 18, 2014